



A G E N D A



**CITY OF IMPERIAL BEACH
CITY COUNCIL
PLANNING COMMISSION
PUBLIC FINANCING AUTHORITY
HOUSING AUTHORITY**

IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY

SEPTEMBER 17, 2014

**Council Chambers
825 Imperial Beach Boulevard
Imperial Beach, CA 91932**

REGULAR MEETING – 6:00 P.M.

THE CITY COUNCIL ALSO SITS AS THE CITY OF IMPERIAL BEACH PLANNING COMMISSION, PUBLIC FINANCING AUTHORITY, HOUSING AUTHORITY AND IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY

The City of Imperial Beach is endeavoring to be in total compliance with the Americans with Disabilities Act (ADA). If you require assistance or auxiliary aids in order to participate at City Council meetings, please contact the City Clerk's Office at (619) 423-8301, as far in advance of the meeting as possible.

REGULAR MEETING CALL TO ORDER

ROLL CALL BY CITY CLERK

PLEDGE OF ALLEGIANCE

AGENDA CHANGES

MAYOR/COUNCIL REIMBURSEMENT DISCLOSURE/COMMUNITY ANNOUNCEMENTS/REPORTS ON ASSIGNMENTS AND COMMITTEES

COMMUNICATIONS FROM CITY STAFF

PUBLIC COMMENT- *Each person wishing to address the City Council regarding items not on the posted agenda may do so at this time. In accordance with State law, Council may not take action on an item not scheduled on the agenda. If appropriate, the item will be referred to the City Manager or placed on a future agenda.*

PRESENTATIONS (1.1-1.2)

1.1* PROMOTION OF TYRA MOE TO ENGINEER/PARAMEDIC BADGE CEREMONY. (0410-30)

* No staff report.

1.2 REPORT OF MAJOR CITY SUPPORTED EVENTS, 4TH OF JULY FIREWORKS AND THE SUN & SEA FESTIVAL. (0410-30)

Recommendation: Receive report and provide input as desired.

Any writings or documents provided to a majority of the City Council/Planning Commission/Public Financing Authority/Housing Authority/I.B. Redevelopment Agency Successor Agency regarding any item on this agenda will be made available for public inspection in the office of the City Clerk located at 825 Imperial Beach Blvd., Imperial Beach, CA 91932 during normal business hours.

CONSENT CALENDAR (2.1-2.6)-All matters listed under Consent Calendar are considered to be routine by the City Council and will be enacted by one motion. There will be no separate discussion of these items, unless a Councilmember or member of the public requests that particular item(s) be removed from the Consent Calendar and considered separately. Those items removed from the Consent Calendar will be discussed at the end of the Agenda.

2.1 MINUTES.

Recommendation: Approve the Regular City Council Meeting minutes of July 16, 2014 and August 20, 2014.

2.2 RATIFICATION OF WARRANT REGISTER. (0300-25)

Recommendation: Ratify the following registers: Accounts Payable Numbers 85152 through 85280 with a subtotal amount of \$1,514,247.46 and Payroll Checks/Direct Deposits 46102 through 46164 for a subtotal amount of \$332,975.16 for a total amount of \$1,847,222.62.

2.3 CONSULTANT SELECTION FOR CLIMATE READY SEA LEVEL RISE STUDY. MF 1025. (0620-77)

Recommendation: Approve the selection of Revell Coastal/ USC Sea Grant as the Climate Ready sea level rise consultant.

2.4 RESOLUTION NO. 2014-7513 AWARDDING A PUBLIC WORKS CONTRACT; TO WIT: ANNUAL MAIN LINE AND MANHOLE REPAIRS FY 11/12 CIP # W12-201. (0830-10)

Recommendation:

1. Receive report;
2. Adopt Resolution No. 2014-7515 awarding a contract to the lowest responsive bidder;
3. Authorize the City Manager to sign the construction contract with the lowest responsive bidder; and
4. Authorize the City Manager to approve a purchase order for the amount of the bid price.

2.5 MAYOR PROCLAMATION FOR TIJUANA RIVER ACTION MONTH FOR OCTOBER. (0230-70 & 0410-30)

Recommendation: Mayor to provide a proclamation for Tijuana River Action Month.

2.6 RESOLUTION NO. 2014-7515 APPROPRIATING AN ADDITIONAL \$33,000 SEWER ENTERPRISE CIP FUNDS TO THE SEWER SYSTEM CIP PROJECT TO WIT – TELEWISE SEWER MAINLINES PROJECT NO. W15-202. (0830-10)

Recommendation:

1. Receive report and
2. Adopt Resolution No. 2014-7515 appropriating an additional \$33,000 from the Sewer Enterprise Fund to the CIP Project W14-202.

ORDINANCES – INTRODUCTION/FIRST READING (3.1)

3.1 INTRODUCTION AND FIRST READING OF ORDINANCE NO. 2014-1147 OF THE CITY OF IMPERIAL BEACH, CALIFORNIA AMENDING SECTION 10.36.160 AND ADDING SECTION 10.36.161 OF THE IMPERIAL BEACH MUNICIPAL CODE RELATED TO PARKING REGULATIONS ON STREET-ENDS WEST OF SEACOAST DRIVE. (0920-95)

Recommendation: That the City Council consider public input, introduce and hold the first reading of Ordinance No. 2014-1147 by title only, waive further reading in full, and schedule the second reading and adoption of the Ordinance on October 1, 2014.

PUBLIC HEARINGS (4)

None.

REPORTS (5.1-5.3)

5.1 ADOPTION OF RESOLUTION NO. 2014-7516 APPROVING AND ADOPTING SIDELETTERS OF AGREEMENT TO THE MEMORANDUMS OF UNDERSTANDING BETWEEN THE CITY AND THE IMPERIAL BEACH FIREFIGHTERS' ASSOCIATION, LOCAL 4692, AND BETWEEN THE CITY AND SERVICE EMPLOYEES INTERNATIONAL UNION, LOCAL 221, AND APPROVING HEALTH BENEFIT CHANGES FOR UNREPRESENTED APPOINTIVE MANAGEMENT AND CONFIDENTIAL EMPLOYEES, AND AMENDING THE FISCAL YEAR 2014/2015 CITY BUDGET APPROPRIATING THE NECESSARY FUNDS FOR HEALTH BENEFIT CHANGES. (0520-50)**

**Staff report to be provided at or prior to the City Council meeting.

5.2 NAVAL BASE CORONADO (NBC) COASTAL CAMPUS ENVIRONMENTAL IMPACT STATEMENT (EIS) COMMENTS. (0620-80)

Recommendation: That the City Council reviews the draft comment letter regarding the NBC Coastal Campus EIS and Provide Any Additional Comments.

5.3 SADBERRY-PALM, LLC (APPLICANT); REVIEW DESIGN/FAÇADE AND COMPREHENSIVE SIGN PLAN REVISIONS FOR THE PROPOSED BREAKWATER DEVELOPMENT LOCATED ON THE SOUTHERN PORTION OF PALM AVENUE BETWEEN 7TH AND 9TH STREETS (ALSO KNOWN AS 9TH & PALM). MF 1062. (0600-20)

Recommendation:

1. Receive report and entertain public testimony;
2. Consider the design of the project; and
3. That the City Council approve the design and façade modifications and determine that the revised plans substantially conform with the approved plans for Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028); Resolution 2011-7131.

I.B. REDEVELOPMENT AGENCY SUCCESSOR AGENCY REPORTS (6)

None.

ITEMS PULLED FROM THE CONSENT CALENDAR (IF ANY)

ADJOURN REGULAR MEETING

The Imperial Beach City Council welcomes you and encourages your continued interest and involvement in the City's decision-making process.

FOR YOUR CONVENIENCE, A COPY OF THE AGENDA AND COUNCIL MEETING PACKET MAY BE VIEWED IN THE OFFICE OF THE CITY CLERK AT CITY HALL OR ON OUR WEBSITE AT www.imperialbeachca.gov

/s/
Jacqueline M. Hald, MMC
City Clerk



STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *ah*
MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: PUBLIC SAFETY *ah*
SUBJECT: REPORT OF MAJOR CITY SUPPORTED EVENTS, 4TH OF JULY
FIREWORKS AND THE SUN & SEA FESTIVAL

EXECUTIVE SUMMARY:

This is an information only report on the outcomes of two major events held in Imperial Beach, the 4th of July Fireworks Show, and the Sun & Sea Festival. No action is required of City Council.

Background

The City of Imperial Beach was heavily involved in two major events that were held during the month of July. City Council approved a 4th of July Fireworks Show fueled by a strong community effort to raise funds for the event, and once again Imperial Beach fireworks were launched from the Imperial Beach Pier to celebrate Independence Day. Between 80,000 and 100,000 people witnessed a perfectly executed, spectacular fireworks display which lasted approximately 18 minutes and synchronized to a simulcast of patriotic music on KYXY Radio.

The Sun & Sea Festival was held on July 18th and this time it included the return of a professional sandcastle building competition, and the Sun & Sea Parade. The Port of San Diego handed off the management of the event to a paid event manager and a committee of dedicated volunteers. Julia Simms did an excellent job of managing and promoting the event. Media coverage was considerable, very positive, and promoted the family-friendly theme for the event. All of the coverage promoted the Sun & Sea Festival and the City of Imperial Beach in the best light. Sun & Sea was attended by approximately 70,000 people over the course of the day, and peaked at about 35,000 from 1:30 to 4:00pm.

Both of these events were very successful, in terms of beach and community activation, and in terms of the logistics and execution of all of the related activities.

4th of July Fireworks:

Imperial Beach residents and businesses contributed funds through a variety of means, including donations made in decorated paint cans placed on merchant counters throughout the City, by holding special events held at local businesses, and through sponsorships, which all combined, funded \$17,300 of the \$25,000 cost of the fireworks show. City Council approved funding for the remaining costs, including the gap in the cost of the fireworks, and the costs of required City services. City Staff immediately began investigating alternatives to provide a show with little

Upon City Council approval for the show, City staff immediately began investigating alternatives to provide a show with little advance planning time, including joining the Big Bay Boom, joining the inland shows, or contracting for an independent show.

At the eleventh hour, the City was able to work out the show with Pyro Spectaculars, Inc. to join the inland shows. This did require considerable staff time to apply for and process permits for pier access and clean water, which isolated the event from the other ocean / bay events, and it exposed the show to additional scrutiny and potential challenges because it was a lone show. Also, since it was the last show the added to the group, and permits had already been pulled, considerable coordination had to be done at the last minute to provide shooters and explosives. Staff had to arrange for all of the necessary permits, and coordinated fire and public safety services to ensure a safe event for the large crowd that attended.

If the City is directed to produce its own show again, it would require issuing an RFP, going through the appropriate review and selection process, then based on the contracted services, commence the process to obtain all of the necessary permits. This would be substantially more work than even this year's event, and would involve considerable staff time and extra expenses. Finally, should this be the direction, it would need to be determined as soon as possible to allow time for all of the necessary steps to be taken to successfully contract for and produce such a show.

Producing independent fireworks shows is expensive and time-consuming, therefore it is recommended that the City join the Big Bay Boom in the future, and early in the process to ensure that our event would be processed with the other locations. In past years the City has successfully participated in the Big Bay Boom sponsored by the Port of San Diego and participating member cities. Some advantages of participating in the Big Bay Boom are:

- It fits the beach activation priority of the Port, and therefore would connect us to their multimedia marketing for the event
- The fireworks promoter coordinates the handling of all of the permitting for the entire event, which would include the Imperial Beach display.
- The matching fireworks displays spread around the bay and down to the Imperial Beach Pier is very impressive, and makes this marquis show even more impressive from a regional perspective.

Sun & Sea Festival:

The major sponsors of the Sun & Sea Festival were the Port of San Diego, The City of Imperial Beach, and California American Water. In place of outside vendors, the Farmer's Market was expanded to 80 booths, including sponsors and non-profits, with priority given to local vendors. There were three bands providing entertainment at Pier Plaza. The Kids 'N Kastles competition was held south of the pier. This year, Seaside Candy engaged a band to perform in the lot next to their property, which drew many people to enjoy the music and check out the candy store and other businesses on the northern end of Seacoast and along Old Palm. Seacoast was closed between Date Avenue and Ebony Avenue, and booths were contained on the west side of the street. This allowed the event to open up to bicycles, skateboards and leashed dogs in the event area.

There were minor problems including some unlicensed vendors, a few unlicensed street performers, and then late in the day, three groups arrived who were not getting along with each other. These issues were quickly addressed by event staff, Public Safety staff, and law enforcement personnel. The other problem was the composition of the new sand, which is not conducive to this kind of competition. The City will work with the Sandcastle Sub-committee to explore solutions to this issue.

The two major recommendations for next year's Sun & Sea event are designed to get better control of the event space and increase the appeal for families. Dunes Park will be converted into a Kid's Zone and the Kids 'N Kastles competition will be located on the beach along Dunes Park and northward. The park will be entirely dedicated to Children's activities and attractions. Similar opportunities for entertainment features will be explored along other Seacoast street-ends in concert with businesses in the area. Other minor adjustments have been discussed with the Sun & Sea Steering Committee, and will be worked into next year's plan. The traffic and parking plan worked very well, with positive feedback, received from residents near the event area.

The City agreed to sponsor the event at \$10,000 which covered all non-law enforcement costs for City services. Actual City expenses were \$9,973.19. Law Enforcement costs were estimated at \$13,988 and came in under budget by approximately \$500. The total cost of City services to Sun & Sea was \$23,504. The committee paid a deposit of \$14,979 based on early law enforcement estimates, which will result in a refund. The success of this event should result in additional opportunities to build upon those elements that will attract families and enhance the sandcastle competition in the future.

Volunteers

Each of these events were made possible by the substantial efforts of several volunteers, not all of whom can be mentioned here. The following individuals however, served in leadership capacities, and spent countless hours making the events come off as well as they did, and are to be commended.

4th of July Fund Raising

Thanks to Candy Unger and Tim O'Neal for their extraordinary efforts in raising approximately \$17,000 largely in small amounts from several hundred donors in IB, making this a truly grass roots, patriotic effort to bring back Independence Day Fireworks to our community.

Sun and Sea Festival

Thanks to the IB Sun and Sea Festival Steering Committee for their extraordinary efforts in bringing back IB's most iconic event. Their efforts began in September 2013 and continued through July 2014, and included 18 meetings, plus 8 subcommittees and their meetings and other work, for a total of approximately 200 volunteer hours for each of the Steering Committee members, including:

Shirley Nakawatase and Leah Goodwin, Valerie Acevez, Sha-Ron Cobb, Jazmine Decker, Leonard Gonzales, Nellie Hazel, Stephanie Kinnamon, Erika Lowery, Kim Paris, Richard Schaumburg, Rico Toscano, Candy Unger, Jean Villard and Jaimy Wilkinson

Thanks for the commitment and hard work by the Auxiliary Committee Members:
Luis Aceves, Dan Gutowski, Douglas Jones, Christine Lapausky, Kim Miller, Tim O'Neal, Mike Osborne, Katie Patton, Diane Rose, Paul Spear, Audra White, and William Zidbeck

RECOMMENDATION:

Receive this report, and provide input as desired.

DRAFT

**CITY OF IMPERIAL BEACH
CITY COUNCIL
PLANNING COMMISSION
PUBLIC FINANCING AUTHORITY
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IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY**

ITEM 2.1

JULY 16, 2014

**Council Chambers
825 Imperial Beach Boulevard
Imperial Beach, CA 91932**

REGULAR MEETING – 6:00 P.M.

REGULAR MEETING CALL TO ORDER

MAYOR JANNEY called the City Council meeting to order at 6:00 p.m.

ROLL CALL BY CITY CLERK

Councilmembers present:	Bilbray (arrived at 6:03 p.m.), Spriggs, Bragg
Councilmembers absent:	None
Mayor Present:	Janney
Mayor Pro Tem Present:	Patton
Staff Present:	City Manager Hall, City Attorney Lyon, City Clerk Hald, Assistant City Manager Wade, Public Works Director Levien; Administrative Services Director Bradley

PLEDGE OF ALLEGIANCE

EAMMON GARLAND, Eagle Scout Candidate, lead everyone in the Pledge of Allegiance.

AGENDA CHANGES

MAYOR JANNEY recommended taking Item No. 5.1 immediately after the Consent Calendar.

COUNCILMEMBER SPRIGGS requested discussion on Item Nos. 2.4 and 2.6.

MOTION BY JANNEY, SECOND BY PATTON, TO TAKE ITEM NO. 5.1 IMMEDIATELY AFTER THE CONSENT CALENDAR AND TO TAKE ITEM NOS. 2.4 AND 2.6 OFF THE CONSENT CALENDAR FOR DISCUSSION AT THE END OF THE AGENDA. MOTION CARRIED UNANIMOUSLY.

MAYOR/COUNCIL REIMBURSEMENT DISCLOSURE/COMMUNITY ANNOUNCEMENTS/REPORTS ON ASSIGNMENTS AND COMMITTEES

COUNCILMEMBER BRAGG reported that she and Mayor Janney attended the Chamber of Commerce meeting where the new Chamber of Commerce Board Members were sworn into office.

COUNCILMEMBER SPRIGGS reported on the successful 4th of July Fireworks show and he commended the community volunteers who raised funds for the event and City staff for assisting with the event.

MAYOR PRO TEM PATTON announced the following upcoming weekend events: the Sandcastle Ball on Friday night, the Pancake Breakfast and parade on Saturday morning and the Sun and Sea Festival on Saturday. He reported on his attendance at the Dog Surfing Contest and thanked the volunteers who helped make the event happen.

MAYOR JANNEY also reported on his attendance at the Dog Surfing Contest and he thanked the Kiwanis Club for recently hosting the Annual City Appreciation Day Luncheon for employees. Both Councilmember Bilbray and Former Mayor Bilbray attended the luncheon.

COMMUNICATIONS FROM CITY STAFF

CITY MANAGER HALL also thanked the Kiwanis Club for hosting the Annual City Appreciation Day Luncheon for employees.

PUBLIC COMMENT

CHARLES QUISENBERRY asked City Council to think in real world terms when considering a reduction in parking and narrowing lanes.

DAVE ELMORE asked City Council to continue to ban short term rentals in the R-1500 Zone. He also asked City Council to step up enforcement, to close the loopholes in enforcement and to increase penalties. He expressed concern about property owners who continue to: violate the City's ordinance against illegal short-term rentals, not pay Transient Occupancy Taxes and steal customers from the new hotel.

PRESENTATIONS (1)

None.

CONSENT CALENDAR (2.1-2.3 & 2.5)

MOTION BY BILBRAY, SECOND BY SPRIGGS, TO APPROVE CONSENT CALENDAR ITEM NOS. 2.1-2.3 AND 2.5. MOTION CARRIED UNANIMOUSLY.

2.1 MINUTES.

Approved the Regular and Special Meeting minutes of the June 18, 2014 City Council meetings.

2.2 RATIFICATION OF WARRANT REGISTER. (0300-25)

Ratified the following registers: Accounts Payable Numbers 84747 through 84894 (84834 voided) with a subtotal amount of \$857,253.56 and Payroll Checks/Direct Deposits 45979 through 46036 for a subtotal amount of \$333,855.29 for a total amount of \$1,191,108.85.

2.3 RESOLUTION NO. 2014-7503 APPROVING A TWENTY-FOUR (24) MONTH EXTENSION FOR TRAFFIC ENGINEER SERVICES PER THE AGREEMENT WITH KOA CORPORATION DATED 6 OCTOBER 2011. (0750-05)

1. Received report;
2. Adopted Resolution No. 2014-7503 authorizing the Public Works Director to provide a written notification to KOA Corporation of the twenty-four (24) month extension, with an Agreement as extended to expire on October 5, 2016 and directing staff to prepare and execute an addendum to the Contract between the City of Imperial Beach and KOA Corporation for the Traffic Engineering Services extending the Agreement for twenty-four (24) months.

2.5 ADOPTION OF RESOLUTION NO. 2014-7505 AUTHORIZING THE CITY MANAGER TO EXTEND THE AGREEMENT WITH AMERICAN MEDICAL RESPONSE AMBULANCE SERVICE, INC. TO PROVIDE AMBULANCE SERVICES IN THE CITY OF IMPERIAL BEACH IN CONJUNCTION WITH THE EXCLUSIVE OPERATING AREA THAT INCLUDES THE CITY OF CHULA VISTA, THE CITY OF IMPERIAL BEACH, AND THE BONITA/SUNNYSIDE FIRE PROTECTION DISTRICT. (0250-20)

Adopted resolution.

ORDINANCES – INTRODUCTION/FIRST READING/PUBLIC HEARING (3.1)

3.1 INTRODUCTION AND FIRST READING OF ORDINANCE NO. 2014-1146: ZONING IMPLEMENTATION OF THE 2013-2021 IMPERIAL BEACH HOUSING ELEMENT (GPA 100060). MF 1060

MAYOR JANNEY declared the public meeting open.

CITY MANAGER HALL reported on the item. He noted that the the City adopted a Housing Element as required by State law and the Housing Element was later adopted and approved by the State. The changes under consideration this evening would make the Zoning Ordinance consistent with the Housing Element.

ASSISTANT CITY MANAGER WADE gave a PowerPoint presentation on the item. He noted that staff is recommending removal of Housing Program 11 (Amend §19.42.070 by eliminating the lot combining restriction for development in the multiple-unit residential zones and including incentives to encourage the owner-initiated merger or consolidation of contiguous properties) because a lot consolidation restriction was specifically included in a City-wide initiative that was approved by the City voters in 1992, the input/comment letters received from the public and because the amendment would have a minor impact on the overall objective of identifying housing sites for all income levels. He then reviewed the zoning provisions that staff recommended for City Council's consideration.

CITY PLANNER NAKAGAWA gave the definitions of manufactured homes, mobile homes and prefabricated/modular homes. He also explained that transitional supportive housing is to be treated as regular residences.

CITY MANAGER HALL explained that what is before City Council this evening does not change the regulations we have on the books for assisted living.

In response to questions of Councilmember Spriggs, ASSISTANT CITY MANAGER WADE stated that on corridors where new development occurs, City Council has discretion. Occupancy of an existing single family home with a supportive housing facility would be difficult to operate economically without increasing density. He also noted that this type of development would be subject to review by the City Council. The types of development that the proposed definitions apply to would likely be subject to City Council's discretion.

CHUCK QUISENBERRY spoke in opposition to the ordinance.

GENE HILGER spoke in support for staff's recommendation to eliminate the lot consolidation provision. He encouraged City Council to vote against the changes that are not mandated by the State or Federal Governments and he spoke in opposition to higher density.

FRIEL ALLEN spoke in opposition to changing density.

DANTE PAMINTUAN encouraged City Council to consider staff's recommendation and he spoke in support for maintaining the City as it is because it has a good balance.

MARIE JOYCE spoke in support for a classic beach community and for being on the conservative side for meeting State compliance rules.

NICHOLE HAZELTON said that creating higher density, combining lots and allowing lower income housing would put a halt to the increasing values of the housing market in Imperial Beach.

MAYOR JANNEY expressed appreciation for City staff looking into the lot consolidation issue and for determining that there was no need to include that element in the ordinance. He noted that the other elements must be considered as required by the State.

MAYOR PRO TEM PATTON expressed appreciation for the comments made by the public and he spoke in support for staff's recommendation to remove lot consolidation.

COUNCILMEMBER SPRIGGS noted that the community spoke very clearly when it came to adopting the current zoning plan. He spoke about the importance of staying true to the existing zoning and development plans and he noted that new development or substantial renovation would be subject to regulatory processes and City Council's oversight. He suggested paying close attention development projects as the State required zoning changes are implemented.

COUNCILMEMBER BRAGG spoke in support for moving the item forward and removing Housing Program 11.

CITY CLERK HALD read the title of Ordinance No. 2014-1146 "An Ordinance of the City Council of the City of Imperial Beach amending Title 19 (Zoning) of the Imperial Beach Municipal Code by implementing Programs 12 and 13 of the 2013-2021 (5th Cycle) Housing Element. MF 1060."

MAYOR JANNEY closed the public hearing.

MOTION BY BRAGG, SECOND BY SPRIGGS, TO WAIVE FURTHER READING IN FULL AND SET THE MATTER FOR ITS SECOND READING AND ADOPTION AT THE NEXT REGULARLY SCHEDULED CITY COUNCIL MEETING ON AUGUST 6, 2014.

ASSISTANT CITY MANAGER WADE commented that a project can be denied under discretionary review.

VOTES WERE NOW CAST ON ORIGINAL MOTION BY BRAGG, SECOND BY SPRIGGS, TO WAIVE FURTHER READING IN FULL AND SET THE MATTER FOR ITS SECOND READING AND ADOPTION AT THE NEXT REGULARLY SCHEDULED CITY COUNCIL MEETING ON AUGUST 6, 2014. MOTION CARRIED UNANIMOUSLY.

REPORTS (5.1)

5.1 PROPOSED BSA EAGLE PROJECT PRESENTATION. (0920-40 & 0940-10)

PUBLIC WORKS DIRECTOR LEVIEN introduced the item.

EAMMON GARLAND, BSA Eagle Scout Candidate, gave a PowerPoint presentation on the item which is to landscape the nearly 3000 square foot dirt area of Sports Park where the previous 4th Street exit left Sports Park. He noted that I.B. Beautiful has yet to approve funding for the project.

MAYOR PRO TEM PATTON suggested that Mr. Garland revisit I.B. Beautiful to obtain funding.

COUNCILMEMBERS SPRIGGS spoke in support for the project.

CONSENSUS OF CITY COUNCIL TO AUTHORIZE THE CITY MANAGER TO SIGN THE EAGLE PROJECT PLAN FOR EAMMON GARLAND TO CONTINUE THE PROJECT DEVELOPMENT AND CONSTRUCTION AS APPROVED BY CITY COUNCIL AND CITY STAFF.

MAYOR JANNEY announced that the I.B. Women's Club donated a bench that was installed at Veterans Park and he thanked Councilmember Bragg for spearheading the project.

PUBLIC HEARINGS (4.1)

4.1 AT&T (APPLICANT); REGULAR COASTAL PERMIT (CP 140002), CONDITIONAL USE PERMIT (CUP 140003), DESIGN REVIEW CASE (DRC 140004), AND SITE PLAN REVIEW (SPR 140005) TO CONSTRUCT A NEW WIRELESS TELECOMMUNICATIONS FACILITY AND EQUIPMENT SHELTER AT 800 SEACOAST DRIVE (APN 626-262-02-00) IN THE C/MU-2 (SEACOAST COMMERCIAL & MIXED-USE) ZONE. MF 1140; RESOLUTION 2014-7502. (0600-20 & 0800-50)

MAYOR JANNEY declared the public hearing open.

CITY MANAGER HALL introduced the item.

SENIOR PLANNER FOLTZ gave a PowerPoint presentation on the item.

ED KRAVITZ questioned who receives the revenue generated from the cell phone tower.

ASSISTANT CITY MANAGER WADE responded that the Successor Agency to the Redevelopment Agency is in a contractual relationship as part of the agreement for the development of the property. Under that agreement, the developer is entitled to engage in leases (or permitted transfers) with businesses such as the restaurant, the spa and AT&T. He also stated that the developer, Imperial Coast LP, is the sole recipient of revenue from the AT&T facility.

CATHY LIM, representing AT&T Mobility, stated that requests for benefits to the City should go through Christine Moore, External Affairs Representative.

MAYOR JANNEY closed the public hearing.

MOTION BY BRAGG, SECOND BY BILBRAY, TO ADOPT RESOLUTION 2014-7502 APPROVING REGULAR COASTAL PERMIT (ACP 140002), CONDITIONAL USE PERMIT (CUP 140003), DESIGN REVIEW CASE (DRC 140004), AND SITE PLAN REVIEW (SPR 140005), WHICH MAKES THE NECESSARY FINDINGS FOR A WIRELESS TELECOMMUNICATIONS FACILITY AND EQUIPMENT SHELTER CONCEALED WITHIN THE PIER SOUTH HOTEL AND PROVIDES CONDITIONS OF APPROVAL IN COMPLIANCE WITH LOCAL AND STATE REQUIREMENTS. MOTION CARRIED UNANIMOUSLY.

REPORTS (5.2 - 5.4)

5.2 COUNTY OF SAN DIEGO (APPLICANT); PRELIMINARY ALTERNATIVE PARKING REVIEW FOR THE PROPOSED NEW PUBLIC LIBRARY AT 810 IMPERIAL BEACH BOULEVARD (APN 626-400-54-00). MF 1067. (0150-20)

SENIOR PLANNER FOLTZ reported on the item. He noted that if a parking agreement between the County of San Diego and St. James Church is not reached, a parking alternative is needed so that the County of San Diego can approve sending out the Request for Proposals this month. He continued with a review of the parking alternative concept.

ASSISTANT CITY MANAGER WADE stated that the shared parking agreement with the City is an alternative to the agreement with the Church. The specifics of the shared parking agreement would later be brought forward for City Council's consideration.

MAYOR JANNEY spoke in support for pursuing negotiations with the Church and for "head-in" parking.

COUNCILMEMBER SPRIGGS was hopeful that negotiations with the Church are accomplished. He spoke about the importance of ensuring that the facility is completely accessible to everybody. He supported perpendicular parking to increase the amount of parking spaces, which could allow for more spaces designated for seniors or for people with disabilities.

COUNCILMEMBER BRAGG thanked the Boys and Girls Club for partnering with the City and she thanked staff for continuously looking for resolutions to issues that arise. She supported "head-in" parking.

MAYOR PRO TEM PATTON recommended that the parking spaces not be narrowed and be made wide enough to accommodate the width of a truck. He spoke in support for plenty of handicap spaces and for additional bike racks.

CONSENSUS OF CITY COUNCIL TO DIRECT STAFF TO NEGOTIATE A SHARED PARKING AGREEMENT BETWEEN THE CITY AND THE COUNTY TO PROVIDE APPROXIMATELY TWENTY-ONE (21) PARKING SPACES ON THE SOUTHERN PORTION OF THE BOYS AND GIRLS CLUB, IN THE EVENT A PARKING AGREEMENT CANNOT BE REACHED BETWEEN THE COUNTY AND THE CHURCH.

5.3 FISCAL YEAR 2014 FINANCIAL UPDATE. (0330-30)

ADMINISTRATIVE SERVICES DIRECTOR BRADLEY gave a PowerPoint presentation on the item.

COUNCILMEMBER SPRIGGS stated that he is not comfortable with the revenues and expenses having a close difference of \$60,000. He noted that it is great that the budget is positive but it is almost negligible. He stressed that it will be difficult to keep up with costs that escalate annually and he spoke about the importance of looking for opportunities to grow the sales tax and other sources of revenue and not cut it so fine in the future.

MAYOR PRO TEM PATTON stated that the City is not struggling and that it is solid. He spoke in support for looking at investments.

ADMINISTRATIVE SERVICES DIRECTOR BRADLEY reported on how some of the auditor's findings were responded to and resolved.

5.4 AUTHORIZATION TO NEGOTIATE AN AGREEMENT TO PROVIDE CITY FINANCIAL ASSISTANCE TO FACILITATE POTENTIAL DEVELOPMENT AT 1046 SEACOAST DRIVE. (0660-43)

ASSISTANT CITY MANAGER WADE gave a PowerPoint presentation on the item.

DANTE PAMINTUAN spoke in support for generating revenue but expressed concern about parking, increased density, and he said that there are basic City services and maintenance that haven't been completed. He said that there are people in the community who have ideas and suggested that the item be tabled so that a public workshop can be held to allow the public to provide comments on how the property should be developed.

CHUCK QUISENBERRY spoke in opposition to the item. He supported Mr. Pamintuan's suggestion of tabling the item and holding a public workshop.

BOB MILLER stated that the location is a world class site that deserves a world class design. He said that although there are other needs in the City, those needs can be satisfied with more revenue. He also added that this development is needed and it is the best we can do on Seacoast Drive.

CONGRESSMAN BRIAN BILBRAY stated that since the 1970's there have been numerous hearings, general plans, and encounter groups with the public where there has been general consensus that this location is an essential site for the community. He noted that in the City of Coronado there are great parks and sidewalks because they have hotels that generate a lot of money allowing the City Council the ability to give their citizens what they want and to be able to afford to buy those improvements in the long term. He stressed that this is an opportunity that the City has been trying to move forward on for a long time and that this is an opportunity to take Imperial Beach money and invest it into Imperial Beach's future.

COUNCILMEMBER SPRIGGS stated that the budget is barely balanced and questioned how the annual 5% increase in public safety contracts would be addressed in order to keep the community safe. He stressed the need for the City to have revenue growth and that there is a need to smartly use I.B.'s locational benefits (i.e. estuary, beach, and bay) to ensure the future and prosperity of the community. He spoke against having a condemned property in the heart of the beachfront. He supported a public discussion but he stressed that this is a private transaction where a private owner is having negotiations with another private owner. There is an option on this property that must be acted on and there is a limited amount of time. He spoke about the necessity for a revenue base that allows for the City to take care of its residents and not rely on only a \$60,000 surplus. He spoke in support for empowering City staff to be part of the discussions and to come back to City Council with the best arrangement for the City.

PAUL MARRA, Managing Principal with Keyser Martson Associates (KMA), stated that it is not uncommon for high-quality hotels in not yet established markets to require a subsidy. He said that in terms of revenue, the highest and best use for a city for a piece of property is a hotel because the city gets 10% of the room revenue. If directed to proceed, KMA would work with staff to bring back the best deal possible. He commented that the developer is highly qualified, their restaurant at Tower 23 is a major draw and revenue generator and that there is potential for spin-off benefits.

STEVE SMITH, Vice President of Operations for Eat.Drink.Sleep., spoke about the hotels and restaurants of Eat.Drink.Sleep and their desire to build a hotel in Imperial Beach.

ASSISTANT CITY MANAGER WADE spoke about the time constraint on the transaction between the development team and the property owner, the spinoff benefit of a restaurant across the street from the Pier South Hotel that experienced a 300% increase in revenue in the 4th quarter of 2012 to the 4th quarter of 2013, and the condition of the current property.

COUNCILMEMBER BRAGG spoke in support for the project.

MAYOR PRO TEM PATTON stated that the options are to fix up the current property, have a limited service hotel or have a high quality hotel by a proven developer. He noted that development of the property has been ongoing since 2002 with several public workshops held over the years. With regard to development of the property, he questioned the impacts during construction, if there would be building delays, how soon until completion and how long until the City receives revenues. He stressed that street end parking should be free and should have the same time restrictions as they are now, that the views of the surf should not be blocked and that there should be flexibility in aesthetics and not build a big box hotel. He noted that Pier South is a world class hotel and now there is a chance to have another one. This hotel would be a perfect bookend for Seacoast Drive and synergistically bring in revenues that can be put back into the community.

COUNCILMEMBER BILBRAY concurred with the comments made by his colleagues. He spoke in support for bringing this type of project to Imperial Beach and stated that this type of revenue generating project will ensure that there will be an Imperial Beach for his kids and grandkids.

MAYOR JANNEY also agreed with the comments of his colleagues. He spoke in support for the project and for directing staff to work on a deal that will work for Imperial Beach and the developer.

CITY MANAGER HALL stated that the City is trying to strike a balance with what the community wants which is a 40 foot hotel. This limits the ability to recoup the revenues necessary to get the funding for this type of a project. With regard to parking, it would be subterranean which is expensive. He noted that the identified funding is from outside of the monies that were set aside for other projects. He asked City Council to direct staff to accomplish the goal of investing one time monies that can increase base revenues and realize a higher rate of return than what is projected for other investments.

CONSENSUS OF CITY COUNCIL TO AUTHORIZE THE CITY MANAGER AND STAFF TO NEGOTIATE AN AGREEMENT BETWEEN THE CITY OF IMPERIAL BEACH AND PACIFICA ENTERPRISES AND EAT, DRINK AND SLEEP TO PROVIDE CITY FINANCIAL ASSISTANCE TO FACILITATE DEVELOPMENT ON THE PROPERTY AT THE NORTHWEST CORNER OF IMPERIAL BEACH BOULEVARD AND SEACOAST DRIVE (1046 SEACOAST DRIVE).

I.B. REDEVELOPMENT AGENCY SUCCESSOR AGENCY REPORTS (6)

None.

ITEMS PULLED FROM THE CONSENT CALENDAR (2.4 & 2.6)

2.4 RESOLUTION NO. 2014-7504 AWARDED A CONTRACT WITH NV5 TO UPDATE THE DESIGN AND CONSTRUCTION DOCUMENTS FOR FOURTEEN CITY ALLEY BLOCK IMPROVEMENTS. (0720-08)

CITY MANAGER HALL reported that the Engineer that worked for Atkins Engineering has left that firm and gone to a new firm. Since the plans for the alleys need to be updated, it is necessary to approve a contract with the new firm.

COUNCILMEMBER SPRIGGS understood that the City wants the same person to work on the project but questioned if the new firm is in the best interest of the City in terms of rates and overhead.

PUBLIC WORKS DIRECTOR LEVIEN responded that the schedule of wages that were negotiated with Atkins is the same as NV5 so there is no change in cost.

DANTE PAMINTUAN stated that 30 alleys were identified and only 14 will be improved. He questioned if the other alleys would be improved.

MAYOR JANNEY stated that there was a process to determine which alleys are priorities for improvement. He also stated that if there was more funding available, he was hopeful that this City Council or future City Councils would consider paving the other alleys.

COUNCILMEMBER BILBRAY stated that there is a policy in place should property owners contribute funds to have alleys improved.

MOTION BY SPRIGGS, SECOND BY PATTON TO APPROVE THE AWARD OF A CONTRACT WITH NV5 TO UPDATE THE PLANS AND SPECIFICATIONS FOR THE 14 ALLEY BLOCKS AS SHOWN IN THE STAFF REPORT, AUTHORIZE THE CITY MANAGER TO SIGN A CONTRACT WITH NV5 FOR THE SCOPE OF WORK NECESSARY TO UPDATE THE PLANS AND SPECIFICATIONS FOR THE 14 ALLEY BLOCKS AS SHOWN IN THE STAFF REPORT, AND AUTHORIZE THE CITY MANAGER TO APPROVE A PURCHASE ORDER WITH NV5 FOR THE UPDATE OF THE PLANS AND SPECIFICATIONS FOR THE 14 ALLEY BLOCKS AS SHOWN IN THE STAFF REPORT. MOTION CARRIED UNANIMOUSLY.

2.6 RESOLUTION NO. 2014-7506 AWARDED A PUBLIC WORKS CONTRACT; TO WIT: RTIP FY 11-12 (STREET IMPROVEMENTS) CIP # S12-104. (0250-20)

COUNCILMEMBER SPRIGGS stated that he pulled the item for discussion in an effort to be transparent because a substantial amount of funds is being awarded to a new contractor.

PUBLIC WORKS DIRECTOR WADE gave report on the item and explained that the original contractor pulled out of the project due to delays caused by SDG&E.

MOTION BY SPRIGGS, SECOND BY JANNEY, TO ADOPT RESOLUTION NO. 2014-7506 AWARDED A CONTRACT TO THE LOWEST RESPONSIVE BIDDER; AND AUTHORIZING THE CITY MANAGER TO SIGN THE CONSTRUCTION CONTRACT WITH THE LOWEST RESPONSIVE BIDDER. MOTION CARRIED UNANIMOUSLY.

ADJOURNMENT

MAYOR JANNEY adjourned the meeting at 9:03 p.m.

James C. Janney, Mayor

Jacqueline M. Hald, MMC
City Clerk

CITY OF IMPERIAL BEACH
CITY COUNCIL
PLANNING COMMISSION
PUBLIC FINANCING AUTHORITY
HOUSING AUTHORITY
IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY

AUGUST 20, 2014

Council Chambers
825 Imperial Beach Boulevard
Imperial Beach, CA 91932

*CLOSED SESSION MEETING – 5:00 P.M.
REGULAR MEETING – 6:00 P.M.*

CLOSED SESSION MEETING CALL TO ORDER

MAYOR JANNEY called the Closed Session meeting to order at 5:02 p.m.

ROLL CALL BY CITY CLERK

Councilmembers present: Spriggs, Bragg (arrived at 5:10 p.m.)
Councilmembers absent: Bilbray
Mayor Present: Janney
Mayor Pro Tem Present: Patton
Staff Present: City Manager Hall, City Attorney Lyon, City Clerk Hald

CLOSED SESSION (1-4)

MOTION BY PATTON, SECOND BY SPRIGGS, TO ADJOURN TO CLOSED SESSION UNDER:

- 1. CONFERENCE WITH LEGAL COUNSEL – EXISTING LITIGATION**
Pursuant to Government Code section 54956.9(d)(1)
Case No. 37-2014-00003054-CU-EI-CTL
- 2. CONFERENCE WITH LEGAL COUNSEL – EXISTING LITIGATION**
Pursuant to Government Code section 54956.9(d)(1)
Case No. 37-2013-00081555-CU-EI-CTL
- 3. CONFERENCE WITH LEGAL COUNSEL – ANTICIPATED LITIGATION**
Significant exposure to litigation pursuant to Govt. Code Section 54956.9(d)(2) with existing facts and circumstances per Govt. Code Section 54956.9(e)(3): 10 claims
- 4. CONFERENCE WITH LABOR NEGOTIATORS**
Pursuant to Government Code Section 54957.6:
Agency Designated Representative: City Manager
Employee Organizations: Imperial Beach Firefighters’ Association (IBFA), Local 4692
Service Employees International Union (SEIU), Local 221

MOTION CARRIED BY THE FOLLOWING VOTE:

AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BRAGG, BILBRAY

ADJOURN CLOSED SESSION MEETING

MAYOR JANNEY adjourned the meeting to closed session at 5:03 p.m. and he reconvened the meeting to Open Session at 6:01 p.m.

CITY ATTORNEY LYON reported that City Council met in Closed Session on the four items. On Items 1-3 Councilmember Spriggs recused himself due to a potential conflict of interest. The balance of the City Council provided direction and no reportable action was taken on Items 1-3.

With regard to Item 4, MAYOR JANNEY reported that the City Council directed staff to work with the employees to see if the City can address unexpected increases in our employee healthcare costs.

REGULAR MEETING CALL TO ORDER

MAYOR JANNEY called the Regular meeting to order at 6:02 p.m.

ROLL CALL BY CITY CLERK

Councilmembers present:	Spriggs, Bragg
Councilmembers absent:	Bilbray
Mayor Present:	Janney
Mayor Pro Tem Present:	Patton
Staff Present:	City Manager Hall, City Attorney Lyon, City Clerk Hald, Senior Accountant Shouse, Public Safety Director Clark, Public Works Director Levien, Assistant City Manager Wade

PLEDGE OF ALLEGIANCE

MAYOR JANNEY led everyone in the Pledge of Allegiance.

AGENDA CHANGES

COUNCILMEMBER SPRIGGS requested a discussion on Item No. 2.1.

MAYOR JANNEY announced that since the next item is a presentation by an Eagle Scout who has not yet arrived, he suggested that City Council go through the Consent Calendar at this time.

PRESENTATIONS (1)

None.

CONSENT CALENDAR (2.1-2.2)

2.1 RATIFICATION OF WARRANT REGISTER. (0300-25)

City staff responded to Councilmember Spriggs' questions regarding the largest expenditures listed on the Warrant Register.

**MOTION BY SPRIGGS, SECOND BY BRAGG TO RATIFY THE FOLLOWING REGISTERS:
ACCOUNTS PAYABLE NUMBERS 85062 THROUGH 85151 WITH A SUBTOTAL AMOUNT
OF \$782,718.65 AND PAYROLL CHECKS/DIRECT DEPOSITS 46073 THROUGH 46101 FOR
A SUBTOTAL AMOUNT OF \$171,493.94 FOR A TOTAL AMOUNT OF \$954,212.59.**

MOTION CARRIED UNANIMOUSLY BY THE FOLLOWING VOTE:

**AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS, BRAGG
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BILBRAY**

2.2 RESOLUTION NO. 2014-7509 AUTHORIZING THE PURCHASE OF A REPLACEMENT WOOD CHIPPER (EQUIPMENT #131) AND APPROPRIATING \$33,000 FROM THE VEHICLE REPLACEMENT INTERNAL SERVICES FUND FOR THIS PURCHASE. (0380-10)

MOTION BY JANNEY, SECOND BY BRAGG, TO ADOPT RESOLUTION NO. 2014-7509 AUTHORIZING THE PURCHASE OF A REPLACEMENT WOOD CHIPPER (EQUIPMENT #131) AND APPROPRIATING \$33,000 FROM THE VEHICLE REPLACEMENT INTERNAL SERVICES FUND FOR THIS PURCHASE. MOTION CARRIED BY THE FOLLOWING VOTE:

AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS, BRAGG

NOES: COUNCILMEMBERS: NONE

ABSENT: COUNCILMEMBERS: BILBRAY

ORDINANCES – INTRODUCTION/FIRST READING (3)

None.

PUBLIC HEARINGS (4)

None.

REPORTS (5.1-5.2)

5.1 PROPOSED BSA EAGLE PROJECT PRESENTATION – OCHOA. (0940-10)

PUBLIC WORKS DIRECTOR LEVIEN introduced the item.

ARMANDO OCHOA, BSA Eagle Scout Candidate, gave a PowerPoint presentation on his project which is to landscape the west entrance to the Public Works facility located at 495 10th Street.

CONSENSUS OF CITY COUNCIL TO AUTHORIZE THE CITY MANAGER TO SIGN THE EAGLE PROJECT PLAN FOR ARMANDO OCHOA TO CONTINUE THE PROJECT DEVELOPMENT AND CONSTRUCTION AS APPROVED BY CITY COUNCIL AND CITY STAFF.

MAYOR/COUNCIL REIMBURSEMENT DISCLOSURE/COMMUNITY ANNOUNCEMENTS/REPORTS ON ASSIGNMENTS AND COMMITTEES

COUNCILMEMBER BRAGG reported on her attendance at Chamber of Commerce Installation Dinner.

COUNCILMEMBER SPRIGGS also reported on his attendance at the Chamber of Commerce Installation Dinner.

COMMUNICATIONS FROM CITY STAFF

CITY MANAGER HALL announced that he was challenged by Blair King, City Manager for the City of Coronado, to participate in the ALS Ice Water Bucket Challenge. He stated that he is honored to participate in the challenge and announced that he would use salt water instead of drinking water due to the drought.

PUBLIC COMMENT

JIM MICKELSON, Member of the Board of Directors for the Chamber of Commerce and Chairman of the Chamber of Commerce Liaison Committee to the City Council, stated that the purpose of the Committee is to attend City Council meetings and take information back to the membership and the Board.

MAYOR JANNEY informed Mr. Michelson of the upcoming DRB meeting because the Palm and 9th Project will be considered.

JUNE ENGEL, Imperial Beach Branch Manager, introduced Kesia Estrada and she spoke about the programs that are being offered at the library. She also announced that the Kiwanis Club is having a SeptemberFest Event on September 13.

JIM JANNEY announced that the County Board of Supervisors moved forward with the next phase of the library project by going out to bid.

MAYOR PRO TEM PATTON announced that the Kiwanis Club is having a Fishing Derby on Saturday, August 23.

5.2 RESOLUTION NO. 2014-7512 APPROPRIATING \$100,000 GAS TAX REVENUE FOR THE DESIGN AND CONSTRUCTION DRAWINGS OF DELAWARE STREET SIDEWALK INFILL 600, 700 AND 800 BLOCKS AND AWARD OF DESIGN AND CONSTRUCTION DRAWING COMPLETION TO BDS ENGINEERING, INC. (CIP S15-102). (0720-50)

CITY MANAGER HALL reported on the item.

**MOTION BY JANNEY, SECOND BY PATTON, TO ADOPT RESOLUTION NO. 2014-7512 APPROPRIATING \$100,000 GAS TAX REVENUE FOR THE DESIGN AND CONSTRUCTION DRAWINGS OF DELAWARE STREET SIDEWALK INFILL 600, 700 AND 800 BLOCKS AND AWARD OF DESIGN AND CONSTRUCTION DRAWING COMPLETION TO BDS ENGINEERING, INC. (CIP S15-102). MOTION CARRIED BY THE FOLLOWING VOTE:
AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS, BRAGG
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BILBRAY**

I.B. REDEVELOPMENT AGENCY SUCCESSOR AGENCY REPORTS (6.1 - 6.2)

6.1 ADOPTION OF RESOLUTION NO. SA-14-44 OF THE IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY APPROVING THE ADMINISTRATIVE BUDGET FOR THE PERIOD OF JANUARY 1, 2015 THROUGH JUNE 30, 2015 AND RELATED ACTIONS. (0418-50)

ASSISTANT CITY MANAGER WADE reported on the item. In response to Councilmember Spriggs' question about the references to both a minimum and capped amount of \$250,000, he clarified that \$250,000 is the maximum amount of administrative cost allowance available.

MOTION BY PATTON, SECOND BY JANNEY, TO ADOPT RESOLUTION NO. SA-14-44 OF THE IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY APPROVING THE ADMINISTRATIVE BUDGET FOR THE PERIOD OF JANUARY 1, 2015 THROUGH JUNE 30, 2015 AND RELATED ACTIONS. MOTION CARRIED BY THE FOLLOWING VOTE:

**AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS, BRAGG
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BILBRAY**

6.2 ADOPTION OF RESOLUTION NO. SA-14-45 OF THE IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY APPROVING AND ADOPTING THE RECOGNIZED OBLIGATION PAYMENT SCHEDULE FOR THE PERIOD OF JANUARY 1, 2015 THROUGH JUNE 30, 2015 (ROPS 14-15B). (0418-50)

ASSISTANT CITY MANAGER WADE reported on the item. He noted that the only new item on the ROPS is the agreement for expenditure of excess bond proceeds that was approved by the Successor Agency on August 6, 2014. He also stated that following approval of this item by the Oversight Board, the resolution was forwarded to the Department of Finance (DOF) who then requested a review of that resolution. In response to questions of Councilmember Spriggs, he reviewed the \$3.7 million funding total and the associated projects. With regard to the City loan repayment, he expected payment within a two year period and he did not know of any legislation that would affect it.

MOTION BY BRAGG, SECOND BY SPRIGGS, TO ADOPT RESOLUTION NO. SA-14-45 OF THE IMPERIAL BEACH REDEVELOPMENT AGENCY SUCCESSOR AGENCY APPROVING AND ADOPTING THE RECOGNIZED OBLIGATION PAYMENT SCHEDULE FOR THE PERIOD OF JANUARY 1, 2015 THROUGH JUNE 30, 2015 (ROPS 14-15B). MOTION CARRIED BY THE FOLLOWING VOTE:

**AYES: COUNCILMEMBERS: JANNEY, PATTON, SPRIGGS, BRAGG
NOES: COUNCILMEMBERS: NONE
ABSENT: COUNCILMEMBERS: BILBRAY**

MAYOR JANNEY recognized staff for seeking grants to help cover costs of capital projects.

ASSISTANT CITY MANGER WADE gave an update on the Bikeway Village project. He noted that tenants are being pursued and that the implementation agreement is being finalized.

ITEMS PULLED FROM THE CONSENT CALENDAR

None.

ADJOURN REGULAR MEETING

MAYOR JANNEY adjourned the meeting at 6:47 p.m.

James C. Janney
Mayor

Jacqueline M. Hald, MMC
City Clerk



STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
 FROM: ANDY HALL, CITY MANAGER *AH*
 MEETING DATE: SEPTEMBER 17, 2014
 ORIGINATING DEPT.: ADMINISTRATIVE SERVICES *DSB*
 SUBJECT: RATIFICATION OF WARRANT REGISTER

EXECUTIVE SUMMARY:

Approval of the warrant register in the amount of \$1,514,247.46 and the payroll checks in the amount of \$332,975.16.

BACKGROUND:

None

ANALYSIS:

As of April 7, 2004 all large warrants above \$100,000 will be separately highlighted and explained on the staff report.

Vendor:	Check:	Amount:	Description:
City of San Diego	85207	\$ 615,531.00	Jul-Sep 2014 Metro Sewer
SD County Sheriff	85278	\$ 518,288.21	Jul 2014 Law Enf Services

The following registers are submitted for Council ratification:

<u>WARRANT #</u>	<u>DATE</u>	<u>AMOUNT</u>
<u>Accounts Payable</u>		
85152-85178	08/15/2014	\$ 121,956.24
85179-85193	08/21/2014	\$ 19,346.77
85162-void	08/15/2014	\$ (88.08)
85194-85265	08/29/2014	\$ 834,621.75
85266-85280	09/04/2014	\$ 538,410.78
	Sub-Total	\$1,514,247.46
<u>Payroll Checks/Direct Deposit</u>		
46102-46133	P.P.E. 8/07/14	\$ 165,240.47
46134-46164	P.P.E. 8/21/14	\$ 167,734.69
	Sub-Total	\$ 332,975.16
	TOTAL	\$1,847,222.62

ENVIRONMENTAL DETERMINATION:

Not a project as defined by CEQA.

FISCAL IMPACT:

Warrants are issued from budgeted funds and there is no additional impact on reserves.

RECOMMENDATION:

It is respectfully requested that the City Council ratify the warrant register.

Attachments:

1. Warrant Register Backup
2. Warrant Register

City of Imperial Beach

Warrant Register Back-up
Aug 15, 21, 29 & Sep 04

	Actual	Notes
TECHNICAL SERVICES		
CITY OF SAN DIEGO	\$ 616,314	FY15 Q1 Sewer Service
COUNTY OF SAN DIEGO	\$ 3,511	July 2014 Parking Ticket County Portion
ACACIA LANDSCAPE, CO.	\$ 3,325	July 2014 Landscaping
PARTNERSHIP WITH INDUSTRY	\$ 2,572	07/31 and -8/15 Invoice
MIRACLE BRAND DESIGN	\$ 1,345	Street Banners
RAPIDSCALE, INC	\$ 910	July 2014 Third Party Email Hosting Fee
VORTEX INDUSTRIES, INC.	\$ 887	Door Repairs
CLEAN HARBORS	\$ 747	
AMS AMERICA INC	\$ 440	
ASBURY ENVIRONMENTAL SERVICES	\$ 338	
GOOGLE, INC.	\$ 211	
COX COMMUNICATIONS	\$ 179	
DATAQUICK	\$ 174	
SOUTH WEST SIGNAL	\$ 160	
AFFORDABLE PRINTER CARE	\$ 85	
RELIABLE TIRES COMPANY	\$ 66	
PROFESSIONAL SERVICES		
SAN DIEGO COUNTY SHERIFF	\$ 520,397	July 2014 Sheriff Contract + July 4th Special Event Fee
ATKINS NORTH AMERICA, INC.	\$ 24,425	July 2014 Invoice Transportation Improvements; Slurry Seal; Plan Check
KANE, BALLMER & BERKMAN	\$ 20,819	July 2014 Attorney Services
KEYSER MARSTON ASSOC INC	\$ 10,056	July 2014 Financial Consulting Services
NINYO & MOORE	\$ 5,082	Pavement Evaluation
CHULA VISTA ANIMAL CARE FACILITY	\$ 2,337	July 2014 Animal Control Fee
U.S. BANK CORPORATE PAYMENT SYSTE	\$ 2,239	Portable Restroom Expense; Misc
HINDERLITER, DELLAMAS & ASSOCIATE	\$ 2,025	July - Sep 2014 Property Tax Analysis
BOCA RIO	\$ 1,427	T Shirt Inventory
MANAGED HEALTH NETWORK	\$ 795	
TREASURER, COUNTY OF S.D.	\$ 736	
DRUG TESTING NETWORK INC	\$ 414	
D.A.R. CONTRACTORS	\$ 347	
EAGLE NEWSPAPER	\$ 90	
H.T.E. MAINTENANCE	\$ 57,573	FY15 Accounting System Maintenance Fees (Sungard)
PAYROLL EXPENSE	\$ 35,376	Payroll Liabilities Payable
ATTORNEY SERVICES		
MCDUGAL LOVE ECKIS &	\$ 15,624	July 2014 Attorney Services
KANE, BALLMER & BERKMAN	\$ 13,349	July 2014 Attorney Services
KEYSER MARSTON ASSOC INC	\$ 357	July 2014 Financial Consulting Services
FEDERAL EXPRESS CORP.	\$ 31	
GAS & ELECTRIC (SDG&E)	\$ 20,840	
OPERATING SUPPLIES		
U.S. BANK CORPORATE PAYMENT SYSTE	\$ 9,833	Beach Wheelchair Parts; Misc Supplies
WAXIE SANITARY SUPPLY	\$ 1,867	Janitorial Supplies
BOUND TREE MEDICAL, LLC	\$ 1,276	Protective Gloves
PADRE JANITORIAL SUPPLIES	\$ 1,250	Janitorial Supplies
GRAINGER	\$ 778	
HANSON AGGREGATES PACIFIC	\$ 764	
ROBERTSON INDUSTRIES, INC.	\$ 614	
CALIFORNIA COMMERCIAL ASPHALT CC	\$ 607	
ATEL COMMUNICATIONS, INC.	\$ 600	
VALLEY INDUSTRIAL SPECIALTIES, INC	\$ 582	
TERRA BELLA NURSERY, INC.	\$ 470	
JIM SULLIVAN	\$ 360	
JOHN DEERE LANDSCAPES	\$ 303	
HUDSON SAFE-T LITE RENTALS	\$ 210	
ARROWHEAD MOUNTAIN SPRING WAT	\$ 92	
SPARKLETTS	\$ 81	
OFFICE DEPOT, INE	\$ 25	
PRO LINE PAINT COMPANY	\$ 23	

City of Imperial Beach

Warrant Register Back-up

Aug 15, 21, 29 & Sep 04

	Actual	Notes
FASTENAL	\$ 17	
GOLDEN STAR TECHNOLOGY, INC	\$ -	
VEHICLE OPERATE-FUEL/OIL	\$ 15,147	SKS, Inc. Fuel Expense
LIABILITIES-DEPOSITS MISCELLANEOUS DEP	\$ 14,616	Sheriff's Fee: Sun n Sea Event Paid by Event Promoter
TEMPORARY STAFFING	\$ 14,288	5 Temporary Staff
LIABILITIES-DEPOSITS BUILDING DEPOSITS	\$ 13,284	Developer Deposit Refunds
UTILITIES-WATER	\$ 12,133	
ATTORNEY SERVICES-OTHER		
MCDUGAL LOVE ECKIS &	\$ 8,227	July 2014 Attorney Services; Retainer
LIABILITIES-DEPOSITS DEVELOPER DEPOSIT	\$ 7,496	Developer Deposit Refunds
TRAVEL, TRAINING, MEETING	\$ 6,312	LG Marine Safety Training; Misc Training
RENT-UNIFORMS	\$ 6,167	
QUESYST	\$ 6,000	FY15 Maintenance Fee for Optiview Document Management System
EQUIPMENT	\$ 4,895	2014 Ford Utility AWC; Less Trade-in
UTILITIES-TELEPHONE	\$ 3,180	
MAINTENANCE & REPAIR	\$ 2,812	Beach Tower Repairs; Misc Repairs
COPIER LEASES	\$ 2,301	
OFFICE SUPPLIES	\$ 2,152	
TRAFFIC CONTROL	\$ 1,733	Safe-T Lite Rental; Signs; Misc.
UTILITIES-CELL PHONES	\$ 1,595	
OTHER SERVICES & CHARGES	\$ 1,504	Event Signs, Promotional Items, Community Items, Misc.
PRINTING SERVICES	\$ 1,428	
MEMBERSHIP DUES	\$ 1,031	
VEHICLE OPERATE-PARTS M&O	\$ 844	
BANKING/FIN SRVCS CHARGES	\$ 809	
CONTRACTS-ELECTIONS	\$ 802	
FEES & LICENSES	\$ 479	
SECURITY & ALARM	\$ 370	
PEST CONTROL SERVICE	\$ 325	
EMPLOYEE RECOGNITION AWRD	\$ 281	
SMALL TOOLS/NON-CAPITAL	\$ 274	
POSTAGE & FREIGHT	\$ 242	
SUBSCRIBE & PUBLICATIONS	\$ 218	
RENT-EQUIPMENT	\$ 207	
ACCOUNT RECEIVABLES	\$ 200	
BUSINESS IMPROVEMENT DIST BUSINESS I	\$ 100	
ADVERTISING	\$ 55	
MISCELLANEOUS PERMITS	\$ 36	
SB 1186 DISABILITY ACCESS	\$ (1)	
FINANCE ADMIN FEES	\$ (108)	
BUSINESS LICENSE	\$ (200)	
RES/FIRE INSPECTION FEES	\$ (230)	
VEHICLE IMPOUND FEE	\$ (2,108)	
Grand Total	\$ 1,514,247	

PREPARED 09/04/2014, 11:47:49
 PROGRAM: GM350L
 CITY OF IMPERIAL BEACH

A/P CHECKS BY PERIOD AND YEAR
 FROM 08/08/2014 TO 09/04/2014

PAGE 1

BANK CODE 00

CHECK DATE	CHECK NUMBER	VENDOR NAME	VENDOR #					CHECK AMOUNT
ACCOUNT #	TRN DATE	DESCRIPTION	INVOICE	PO #	PER/YEAR	TRN AMOUNT		
08/15/2014	85152	ACACIA LANDSCAPE, CO.	1942					3,325.00
101-6020-452.21-04	08/01/2014	JUL 2014	5025	150173	01/2015	3,325.00		
08/15/2014	85153	ARROWHEAD MOUNTAIN SPRING WATE	1340					92.11
101-5020-432.30-02	07/23/2014	JUL 2014	04G0026726646	150183	01/2015	51.08		
101-1010-411.30-02	07/23/2014	JUL 2014	04G0031149578	150175	01/2015	41.03		
08/15/2014	85154	ATEL COMMUNICATIONS, INC.	2355					600.00
101-1920-419.30-02	08/04/2014	SEP/OCT 2014	38077	150180	02/2015	300.00		
101-1920-419.30-02	07/07/2014	JUL/AUG 2014 AUTO ATTENDA	37730	150180	01/2015	300.00		
08/15/2014	85155	CALIFORNIA COMMERCIAL ASPHALT	590					606.75
101-5010-431.30-02	08/01/2014	CONCRETE ASPHALT	141812	150031	02/2015	606.75		
08/15/2014	85156	CVA SECURITY	797					185.00
101-1910-419.20-23	08/01/2014	AUG 2014 - 2089 CITY HALL	29835	150076	02/2015	30.00		
101-1910-419.20-23	08/01/2014	AUG 2014 - 2466 PW	29866	150076	02/2015	55.00		
101-1910-419.20-23	08/01/2014	AUG 2014 -2698 CITY HALL	29914	150076	02/2015	30.00		
101-1910-419.20-23	08/01/2014	AUG 2014 - 314 MVC	29929	150076	02/2015	30.00		
101-1910-419.20-23	08/01/2014	AUG 2014 - 314 PW	29938	150076	02/2015	40.00		
08/15/2014	85157	CLEAN HARBORS	913					747.00
101-5040-434.21-04	08/04/2014	JUL 2014	1000560030	150023	01/2015	747.00		
08/15/2014	85158	COX COMMUNICATIONS	1073					216.62
503-1923-419.29-04	08/01/2014	08/01-08/31 3110015533201	08-22-2014	150087	02/2015	37.62		
101-5050-435.21-04	08/04/2014	08/04-09/03 3110091187001	08-25-2014	150087	02/2015	179.00		
08/15/2014	85159	DATAQUICK	1134					173.50
101-1210-413.21-04	08/01/2014	JUL 2014 PROP DATA SEARCH	B1-2313188	150201	02/2015	14.50		
101-3020-422.21-04	08/01/2014	JUL 2014 PROP DATA SEARCH	B1-2313188	150201	02/2015	.50		
101-3040-424.21-04	08/01/2014	JUL 2014 PROP DATA SEARCH	B1-2313188	150201	02/2015	17.00		
101-3070-427.21-04	08/01/2014	JUL 2014 PROP DATA SEARCH	B1-2313188	150201	02/2015	141.50		
08/15/2014	85160	DRUG TESTING NETWORK INC	1195					352.90
101-1130-412.20-06	07/31/2014	RANDOM TESTING/CALL OUT	72429	150177	01/2015	352.90		
08/15/2014	85161	GO-STAFF, INC.	2031					2,523.00
101-1210-413.21-01	08/05/2014	W/E 08/07/14 FERGUSON, N	125358	150072	02/2015	978.12		
503-1923-419.21-01	08/05/2014	W/E 08/03/14 PIEDRA, M	128359	150108	02/2015	448.00		
101-1230-413.21-01	08/05/2014	W/E 08/03/14 LOPEZ, L	128356	150113	02/2015	1,096.88		
08/15/2014	85162	GST	10					.00
101-3020-422.30-02	08/04/2014	MOUNTING KIT FOR MDC UNIT	JAI109224	F15019	02/2015	88.08		
101-3020-422.30-02	08/29/2014	MOUNTING KIT FOR MDC UNIT	JAI109224		02/2015	88.08		
08/15/2014	85163	HUDSON SAFE-T LITE RENTALS	2382					316.40
101-3020-422.30-02	07/20/2014	RENT 50 NO PARKING SIGNS	00024678	F15017	01/2015	210.00		
101-5010-431.21-23	07/19/2014	BARRICADE RENTAL	00024699	150062	01/2015	106.40		

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08/15/2014	85165	IMPERIAL BEACH CHAMBER OF COMM	1505			60.00
101-1010-411.28-12	07/17/2014	JANNEY MEMBERSHIP DUES	7079	F15016	01/2015	60.00
08/15/2014	85166	JIM SULLIVAN	2004			360.00
101-3035-423.30-02	07/18/2014	REIMBURSE IBJG HANDPLANES	07-18-2014		01/2015	360.00
08/15/2014	85167	KANE, BALLMER & BERKMAN	1828			34,167.52
303-1250-413.20-01	06/03/2014		20259		11/2014	457.44
303-1250-413.20-01	08/07/2014		20451		01/2015	6,052.50
303-1250-413.20-06	08/07/2014		20452		01/2015	2,775.00
303-1250-413.20-06	08/07/2014		20454		01/2015	10,040.00
303-1250-413.20-06	08/07/2014		20455		01/2015	7,696.25
303-1250-413.20-01	08/07/2014		20456		01/2015	6,677.50
303-1250-413.20-01	08/07/2014		20459		01/2015	161.14
303-1250-413.20-06	08/07/2014		20460		01/2015	307.69
08/15/2014	85168	LLOYD PEST CONTROL	814			325.00
101-1910-419.20-22	07/10/2014	JUL 2014 -CITY HALL	4330836	150074	01/2015	36.00
101-1910-419.20-22	07/10/2014	JUL 2014 -FIRE DEPT	4330837	150074	01/2015	36.00
101-1910-419.20-22	07/10/2014	JUL 2014 -SHERIFF DEPT	4331000	150074	01/2015	36.00
101-1910-419.20-22	07/11/2014	JUL 2014 -MV CENTER	4331067	150074	01/2015	53.00
101-1910-419.20-22	07/24/2014	JUL 2014 -SPORTS PARK	4315488	150074	01/2015	51.00
101-1910-419.20-22	07/24/2014	JUL 2014 -PUBLIC WORKS	4317193	150074	01/2015	53.00
101-1910-419.20-22	07/24/2014	JUL 2014 -DEMPSEY CENTER	4317453	150074	01/2015	60.00
08/15/2014	85169	MANAGED HEALTH NETWORK	2432			795.20
101-1130-412.20-06	07/17/2014	AUG 2014	3200059969	150174	02/2015	397.60
101-1130-412.20-06	06/16/2014	JUL 2014	3200058992	150174	01/2015	397.60
08/15/2014	85170	MICHELLE SIROTA	2			36.00
101-0000-324.73-01	07/30/2014	REFUND SOUND PERMIT	752		01/2015	36.00
08/15/2014	85171	OFFICE DEPOT, INC	1262			1,123.99
101-1230-413.28-11	07/22/2014	BUSINESS CARDS/ WADE,G	720698221001	150000	01/2015	44.09
101-1230-413.30-01	07/23/2014	COPY PAPER/MISC SUPPLIES	724260409001	150000	01/2015	63.82
101-1130-412.30-01	07/23/2014	COPY PAPER/PENS	724271359001	150000	01/2015	56.78
101-5020-432.30-01	07/24/2014	DOCUMENT HOLDER	724458954001	150000	01/2015	13.39
101-5020-432.30-01	07/24/2014	CAMERA/SD CARD	724459492001	150000	01/2015	227.50
101-5020-432.30-01	07/24/2014	HP TONER	724537222001	150000	01/2015	144.05
101-1210-413.30-01	07/28/2014	POCKET FOLDERS	724608995001	150000	01/2015	114.89
101-1230-413.30-01	07/31/2014	FILE FOLDERS/BINDER CLIPS	722173437001	150000	01/2015	85.58
101-1210-413.30-01	07/31/2014	HP TONER	722250341001	150000	01/2015	123.44
101-1110-412.28-11	07/11/2014	VEA,E-BUSINESS CARDS	719210821001	150000	01/2015	44.09
101-1110-412.30-01	06/09/2014	PLANNER	715992099001	150000	01/2015	55.38
101-1110-412.30-01	06/11/2014	PROJECT PLANNER	716845754001	150000	01/2015	150.98
08/15/2014	85172	PARTNERSHIP WITH INDUSTRY	1302			1,413.07
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08/15/2014	85173	RICOH USA, INC.	2392			2,301.30	
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101-3020-422.20-17	08/04/2014	AUG 2014	92946838	150109	02/2015	328.76	
101-3030-423.20-17	08/04/2014	AUG 2014	92946838	150109	02/2015	328.76	
08/15/2014	85174	SAN DIEGO GAS & ELECTRIC	1399			20,839.63	
101-3020-422.27-01	08/06/2014	1008 786 9371 06/27-07/29	08-22-2014		01/2015	44.08	
101-1910-419.27-01	08/06/2014	1008 786 9371 06/27-07/29	08-22-2014		01/2015	160.36	
101-5010-431.27-01	08/06/2014	1008 860 4389 06/25-07/25	08-22-2014		01/2015	43.99	
101-3020-422.27-01	08/06/2014	1980 769 7764 06/26-07/28	08-22-2014		01/2015	4,753.31	
601-5060-436.27-01	08/06/2014	5263 521 9238 06/25-07/25	08-22-2014		01/2015	10.00	
101-6020-452.27-01	08/06/2014	5649 771 4749 06/30-07/30	08-22-2014		01/2015	7.76	
101-5010-431.27-01	08/06/2014	5649 771 4749 06/25-07/30	08-22-2014		01/2015	7,606.04	
101-5010-431.27-01	08/06/2014	8507 517 8464 06/30-07/30	08-22-2014		01/2015	129.38	
601-5060-436.27-01	08/06/2014	8507 517 8464 06/30-07/30	08-22-2014		01/2015	93.67	
101-6020-452.27-01	08/06/2014	8507 517 8464 06/30-07/30	08-22-2014		01/2015	928.46	
601-5060-436.27-01	08/06/2014	8541 770 1270 06/30-07/30	08-22-2014		01/2015	5,437.36	
101-5020-432.27-01	08/06/2014	9169 299 2261 06/24-07/24	08-22-2014		01/2015	1,625.22	
08/15/2014	85175	SKS INC.	412			7,436.59	
501-1921-419.28-15	08/07/2014	1395.1 GAL REG FUEL	1263989-IN	150041	02/2015	4,890.63	
501-1921-419.28-15	08/07/2014	693.4 GAL DIESEL FUEL	1263990-IN	150041	02/2015	2,545.96	
08/15/2014	85176	SOUTH WEST SIGNAL	488			160.00	
101-5010-431.21-04	07/31/2014	JUL 2014	51619	150032	01/2015	160.00	
08/15/2014	85177	SUNGARD PUBLIC SECTOR INC.	1370			43,341.60	
503-1923-419.20-25	05/28/2014	14/15 SOFTWARE MAINT	82579	150192	01/2015	43,341.60	
08/15/2014	85178	TRANSWORLD SYSTEMS INC.	2160			269.98	
101-1210-413.20-27	07/31/2014	JUL 2014 -COLLECTION FEES	956718		01/2015	808.73	
101-0000-321.72-10	07/31/2014	JUL 2014 -COLLECTION FEES	956718		01/2015	200.00-	
101-0000-323.71-03	07/31/2014	JUL 2014 -COLLECTION FEES	956718		01/2015	230.00-	
101-0000-344.76-03	07/31/2014	JUL 2014 -COLLECTION FEES	956718		01/2015	107.75-	
101-0000-371.83-09	07/31/2014	JUL 2014 -COLLECTION FEES	956718		01/2015	1.00-	
08/21/2014	85179	ASBURY ENVIRONMENTAL SERVICES	277			337.96	
101-5040-434.21-04	08/11/2014	ENVIRO WASTE PICKUP	130457850	150003	02/2015	337.96	
08/21/2014	85180	CALIFORNIA AMERICAN WATER	612			1,118.52	
101-3030-423.27-02	08/05/2014	1015-210019276868 JUL 14	08-27-2014		01/2015	178.28	
101-5010-431.27-02	08/05/2014	1015-210019278895 JUL 14	08-27-2014		01/2015	28.06	
101-5010-431.27-02	08/05/2014	1015-210019279782 JUL 14	08-27-2014		01/2015	22.23	
101-5010-431.27-02	08/05/2014	1015-210019357057 JUL 14	08-27-2014		01/2015	51.33	
101-5010-431.27-02	08/05/2014	1015-210019481684 JUL 14	08-27-2014		01/2015	22.23	
101-5010-431.27-02	08/05/2014	1015-210019278093 JUL 14	08-27-2014		01/2015	33.86	
101-5010-431.27-02	08/05/2014	1015-210019482014 JUL 14	08-27-2014		01/2015	250.33	

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101-5010-431.27-02		08/05/2014	1015-210019360534 JUL 14	08-27-2014		01/2015		33.86
08/21/2014	85181	FASTENAL	909					17.29
101-6040-454.30-02		08/01/2014	WHEELCHAIR HARDWARE	CACHU37340	150006	02/2015		17.29
08/21/2014	85182	GRAINGER	1051					738.95
601-5060-436.28-01		07/24/2014	WATER SURFACE SENSOR	9499417773	150007	01/2015		192.20
601-5060-436.28-01		07/28/2014	RUBBER GUM/RUBBER MAT	9502066161	150007	01/2015		546.75
08/21/2014	85183	HANSON AGGREGATES INC.	48					763.82
101-5010-431.30-02		07/30/2014	6CY CONCRETE	539822	150034	01/2015		763.82
08/21/2014	85184	I B FIREFIGHTERS ASSOCIATION	214					450.00
101-0000-209.01-08		08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015		450.00
08/21/2014	85185	ICMA RETIREMENT TRUST 457	242					5,659.02
101-0000-209.01-10		08/14/2014	PAYROLL AP PPE 8/07/14	101834826		02/2015		5,659.02
08/21/2014	85186	INTERSTATE BATTERY OF SAN	DIEG 388					134.07
501-1921-419.28-16		08/07/2014	#113 MTP-27	930010931	150008	02/2015		110.31
501-1921-419.28-16		08/11/2014	#602 REPLACEMENT BATTERY	930010957	150008	02/2015		23.76
08/21/2014	85187	PADRE JANITORIAL SUPPLIES	1430					473.01
101-1910-419.30-02		07/30/2014	JANITORIAL SUPPLIES	360143	150020	01/2015		124.60
101-3030-423.30-02		08/11/2014	JANITORIAL SUPPLIES	360453	150020	02/2015		348.41
08/21/2014	85188	PRO LINE PAINT COMPANY	52					22.66
101-1910-419.30-02		08/06/2014	RUST CONVRSN	3105-6	150016	02/2015		22.66
08/21/2014	85189	PRUDENTIAL OVERALL SUPPLY	72					248.08
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101-5020-432.25-03		08/06/2014	08/06/14 PW UNIFORMS	30436473	150077	02/2015		128.90
08/21/2014	85190	RAPID SCALE, INC	2591					910.35
503-1923-419.21-04		07/31/2014	JUL 2014 EMAIL HOST	2984	150191	01/2015		910.35
08/21/2014	85191	SEIU LOCAL 221	1821					1,635.88
101-0000-209.01-08		08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015		1,635.88
08/21/2014	85192	THE SURF HUT	1415					3,693.60
101-3030-423.25-03		07/31/2014	LG EYE PROTECTION GLASSES	8147	150185	01/2015		3,693.60
08/21/2014	85193	US BANK	2458					3,143.56
101-0000-209.01-20		08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015		3,143.56
08/29/2014	85194	ACE UNIFORMS & ACCESSORIES INC	1571					721.74
101-3020-422.25-03		07/01/2014	SANTOS, T UNIFORMS	218652	150196	01/2015		721.74
08/29/2014	85195	ADVANCED PROCESSING & IMAGING	2139					6,000.00
503-1923-419.20-26		06/06/2014	14/15 OPTIVIEW MAINT	34407	F15033	01/2015		6,000.00

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08/29/2014	85197	AFLAC	120	716.98
101-0000-209.01-13	08/14/2014	PAYROLL AP PPE 8/07/14	20140814	02/2015 358.49
101-0000-209.01-13	08/28/2014	PAYROLL AP PPE 8/21/14	362690	02/2015 358.49
08/29/2014	85198	SOUTHCOAST HEATING & A/C	1554	440.00
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08/29/2014	85199	AT&T	2430	3,179.96
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503-1923-419.27-04	08/20/2014	3393431504727	5680114	02/2015 178.97
503-1923-419.27-04	08/20/2014	3393439371447	5682783	02/2015 178.97
503-1923-419.27-04	08/20/2014	3393442323406	5683091	02/2015 178.97
101-1210-413.27-04	08/17/2014	6194235034	5672213	02/2015 17.98
101-3020-422.27-04	08/17/2014	6194237246664	5671436	02/2015 .70
101-5020-432.27-04	08/15/2014	6194238311966	5667811	02/2015 6.60
101-3030-423.27-04	08/15/2014	6194238322966	5667812	02/2015 3.80
503-1923-419.27-04	08/11/2014	6194243481712	5645476	02/2015 17.32
101-1230-413.27-04	08/17/2014	6196281356950	5671439	02/2015 8.12
101-1920-419.27-04	08/17/2014	6196282018442	5671445	02/2015 .10
601-5060-436.27-04	08/15/2014	C602221236777	5667803	02/2015 20.25
101-1920-419.27-04	08/15/2014	C602224829777	5668869	02/2015 108.19
101-1110-412.27-04	08/15/2014	C602224831777	5668871	02/2015 136.69
101-1020-411.27-04	08/15/2014	C602224832777	5668872	02/2015 63.50
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101-1130-412.27-04	08/15/2014	C602224834777	5668874	02/2015 48.43
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101-6010-451.27-04	08/15/2014	C602224837777	5668877	02/2015 14.37
101-3020-422.27-04	08/15/2014	C602224838777	5668878	02/2015 528.45
101-3030-423.27-04	08/15/2014	C602224839777	5668879	02/2015 216.94
101-5020-432.27-04	08/15/2014	C602224840777	5668880	02/2015 309.37
601-5060-436.27-04	08/15/2014	C602224841777	5668881	02/2015 163.41
08/29/2014	85200	ATKINS NORTH AMERICA, INC.	2455	31,870.90
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101-0000-221.01-02	07/08/2014	JUN 2014 PLAN CHECKS	1197761	01/2015 157.73
101-0000-221.01-02	07/08/2014	JUN 2014 PLAN CHECKS	1197761	01/2015 157.72
101-0000-221.01-02	06/30/2014	MAY 2014 PLAN CHECKS	1197255	01/2015 490.00
101-0000-221.01-02	06/30/2014	MAY 2014 PLAN CHECKS	1197255	01/2015 750.00
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201-5015-531.20-06	08/19/2014	JUL 2014 SLURRY SEAL PROG	1200667	150103	02/2015	1,440.00	
202-5016-531.20-06	08/19/2014	JUL 2014 TRANS IMPRVMENTS	1200668	150115	02/2015	22,347.50	
08/29/2014	85201	BOCA RIO	2610			1,427.25	
101-1110-412.20-06	07/31/2014	CITY OF IB T-SHIRTS	7962	150200	01/2015	1,541.43	
101-1110-412.20-06	07/31/2014	CITY OF IB T-SHIRTS-SALES	7962		01/2015	114.18-	
08/29/2014	85202	BOUND TREE MEDICAL, LLC	485			1,276.05	
101-6040-454.30-02	08/01/2014	PROTECTIVE GLOVES	81505171	150181	02/2015	1,276.05	
08/29/2014	85203	CALIFORNIA AMERICAN WATER	612			11,014.97	
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101-3030-423.27-02	08/07/2014	1015-210020153385 JUL 14	08-29-2014		01/2015	25.27	
101-6020-452.27-02	08/13/2014	1015-210020440898 JUL 14	09-04-2014		01/2015	884.61	
101-5010-431.27-02	08/08/2014	1015-210019179080 JUL 14	09-02-2014		01/2015	1,156.92	
101-6040-454.27-02	08/05/2014	1015-210019027905 JUL 14	08-27-2014		01/2015	774.04	
101-5020-432.27-02	08/08/2014	1015-210019058534 JUL 14	09-02-2014		01/2015	288.85	
101-5010-431.27-02	08/15/2014	1015-210020731235 JUL 14	09-08-2014		01/2015	6.56	
601-5060-436.27-02	08/18/2014	1015-210019401916 JUL 14	09-09-2014		01/2015	18.21	
601-5060-436.27-02	08/08/2014	1015-210018820255 JUL 14	09-02-2014		01/2015	18.21	
101-5020-432.27-02	08/18/2014	1015-210020277854 JUL 14	09-09-2014		01/2015	68.77	
101-6020-452.27-02	08/18/2014	1015-210021082448 JUL 14	09-09-2014		01/2015	213.20	
101-6020-452.27-02	08/08/2014	1015-210019176128 JUL 14	09-02-2014		01/2015	12.39	
101-6020-452.27-02	08/08/2014	1015-210019176067 JUL 14	09-02-2014		01/2015	541.27	
101-5010-431.27-02	08/07/2014	1015-210018811916 JUL 14	08-29-2014		01/2015	32.81	
601-5060-436.27-02	08/07/2014	1015-210019512885 JUL 14	08-29-2014		01/2015	222.07	
101-1910-419.27-02	08/07/2014	1015-210020154739 JUL 14	08-29-2014		01/2015	22.23	
101-6020-452.27-02	08/13/2014	1015-210019748332 JUL 14	09-04-2014		01/2015	6.56	
101-6020-452.27-02	08/12/2014	1015-210019749687 JUL 14	09-03-2014		01/2015	24.02	
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101-1910-419.27-02	08/14/2014	1015-210021068268 JUL 14	09-05-2014		01/2015	75.76	
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08/29/2014	85204	CALIFORNIA DENTAL	2480			758.76	
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101-0000-209.01-12	07/31/2014	PAYROLL AP PPE 7/24/14	20140731		01/2015	379.38	
08/29/2014	85205	CDW GOVERNMENT INC	725			17.55	
503-1923-419.30-22	08/07/2014	ACCESS POINT POWER SUPPLY	NP53777	150073	02/2015	17.55	
08/29/2014	85206	CHULA VISTA ANIMAL CARE FACILI	2599			2,337.30	
101-3050-425.20-06	08/12/2014	JUL 2014 A/C-OT/CALLBACK	08-12-2014	150214	01/2015	2,337.30	
08/29/2014	85207	CITY OF SAN DIEGO	896			615,531.00	
601-5060-436.21-04	07/28/2014	FY 2015 1ST QTR METRO SEW	1000110957	150189	01/2015	615,531.00	

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101-0000-209.01-13	08/28/2014	PAYROLL AP PPE 8/21/14	0802486		02/2015	50.09		50.09
08/29/2014	85209	COREY LOVE	4					1,673.00
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101-3010-421.21-04	07/31/2014	JUL 2014 PARKING PENALTY	07/14		01/2015	3,511.30		3,511.30
08/29/2014	85211	COUNTY RECORDER	1818					50.00
101-0000-221.01-02	08/05/2014	NOE 1111/1115 LOUDEN LN	MF 1152		02/2015	50.00		50.00
08/29/2014	85212	D.A.R. CONTRACTORS	1122					347.00
101-3050-425.20-06	08/01/2014	JUL 2014	071401229	150195	02/2015	347.00		347.00
08/29/2014	85213	DRUG TESTING NETWORK INC	1195					60.95
101-1130-412.20-06	08/15/2014	JUL 2014 RANDOM TESTING	72717	150177	02/2015	60.95		60.95
08/29/2014	85214	EAGLE NEWSPAPER	1204					210.00
101-1020-411.21-06	07/02/2014	NOTICE OF ELECTION	83516	F15026	01/2015	25.00		25.00
101-1230-413.29-04	07/02/2014	JUL 2014 LEGAL ADVERTISIN	83516	150017	01/2015	95.00		95.00
601-5060-536.20-06	07/30/2014	JUL 2014 LEGAL ADVERTISIN	83898	150017	01/2015	90.00		90.00
08/29/2014	85215	FEDERAL EXPRESS CORP.	911					30.99
303-1250-413.20-01	08/15/2014	08/06/14 KBB OVERNIGHT	2-750-49047	150114	02/2015	30.99		30.99
08/29/2014	85216	FIDELITY SECURITY LIFE INSURAN	2476					246.55
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08/29/2014	85217	GEORGE MELLON	4					11,611.00
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08/29/2014	85218	GO-STAFF, INC.	2031					7,229.09
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101-3020-422.21-01	08/05/2014	W/E 08/03/14 MEDLEY,A	128357	150168	02/2015	788.19		788.19
101-3020-422.21-01	08/12/2014	W/E 08/10/14 MEDLEY,A	128713	150168	02/2015	525.46		525.46
101-1210-413.21-01	08/19/2014	W/E 08/17/14, FERGUSON,N	129085	150072	02/2015	978.12		978.12
503-1923-419.21-01	08/19/2014	W/E 08/17/2014 PIEDRA,M	129086	150108	02/2015	672.00		672.00
101-1230-413.21-01	08/19/2014	W/E 08/17/2014 LOPEZ,L	129083	150113	02/2015	1,287.00		1,287.00
101-3020-422.21-01	08/19/2014	W/E 08/17/14 MEDLEY,A	129084	150168	02/2015	277.04		277.04
08/29/2014	85219	GOOGLE, INC.	2009					211.40
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08/29/2014 101-5010-431.21-23	85221 08/21/2014	HUDSON SAFE-T LITE RENTALS 08/21/2014	2382 SQUARE POSTS/ANCHORS	00025345	150062 02/2015	735.75 735.75	
08/29/2014 101-0000-209.01-08	85222 08/28/2014	I B FIREFIGHTERS ASSOCIATION 08/28/2014	214 PAYROLL AP PPE 8/21/14	20140828	02/2015	450.00 450.00	
08/29/2014 101-0000-209.01-10	85223 08/28/2014	ICMA RETIREMENT TRUST 457 08/28/2014	242 PAYROLL AP PPE 8/21/14	101840558	02/2015	5,479.52 5,479.52	
08/29/2014 101-3030-423.28-04	85224 07/01/2014	JUSTIN L. OLSON 07/01/2014	2358 REIMBURSE EMT RECERT FEES	019149	01/2015	54.00 54.00	
08/29/2014 101-0000-121.00-00	85225 08/19/2014	KAYLI DAVIDSON 08/19/2014	2 REFUND ADMIN CITATION FEE	A10405	02/2015	200.00 200.00	
08/29/2014 303-1250-413.20-01 303-1250-413.20-06	85226 08/15/2014 08/15/2014	KEYSER MARSTON ASSOC INC 08/15/2014 08/15/2014	620	0027544 0027545	150219 02/2015 150219 02/2015	10,413.04 356.88 10,056.16	
08/29/2014 101-1220-413.20-02 101-1220-413.20-01 502-1922-419.20-01 101-1220-413.20-01 101-1220-413.20-01	85227 07/31/2014 07/31/2014 07/31/2014 07/31/2014 07/31/2014	MCDUGAL LOVE ECKIS & 07/31/2014 07/31/2014 07/31/2014 07/31/2014 07/31/2014	962	86448 86446 86449 86470 86447	150116 01/2015 01/2015 01/2015 01/2015 01/2015	23,850.92 8,227.00 1,365.19 3,995.49 6,238.88 4,024.36	
08/29/2014 101-5010-431.21-04	85228 08/15/2014	MIRACLE BRAND DESIGN 08/15/2014	2260 STREET BANNERS	1016	150084 02/2015	1,344.60 1,344.60	
08/29/2014 101-5010-531.20-06	85229 07/24/2014	NINYO & MOORE 07/24/2014	2151 PAVEMENT EVALUATION	181874	01/2015	5,081.75 5,081.75	
08/29/2014 101-1210-413.28-11 101-1210-413.30-01 101-1110-412.30-01 101-1130-412.30-01 101-1210-413.30-01 101-1130-412.28-11 101-1210-413.30-01 101-3020-422.30-01 101-1110-412.30-01 101-1010-411.30-02 101-1020-411.29-04 101-1110-412.30-01 101-1130-412.30-02	85230 08/02/2014 08/05/2014 08/06/2014 08/06/2014 08/07/2014 08/08/2014 08/07/2014 07/30/2014 08/14/2014 07/10/2014 07/10/2014 07/10/2014 07/10/2014 07/10/2014	OFFICE DEPOT, INC 08/02/2014 08/05/2014 08/06/2014 08/06/2014 08/07/2014 08/08/2014 08/07/2014 07/30/2014 08/14/2014 07/10/2014 07/10/2014 07/10/2014 07/10/2014 07/10/2014	1262 BUS LICESE ENVELOPES MONITOR RISER/FOLDERS FOLDERS/PROTECTORS FOLDERS/PROTECTORS POST-IT FLAGS/PENS/MISC BUSINESS CARDS, WADE,G PAYROLL TONER CARTRIDGE/JOURNAL BATTERIES/DETERGENT STAPLER/MISC SUPPLIES STAPLER/MISC SUPPLIES STAPLER/MISC SUPPLIES STAPLER/MISC SUPPLIES STAPLER/MISC SUPPLIES	722032445001 723107446001 723366039001 723366039001 723579038001 723365943001 723518031001 722017279001 705509556001 719549414001 719549414001 719549414001 719549414001 719549414001	150000 02/2015 150000 02/2015 150000 02/2015 150000 02/2015 150000 02/2015 150000 02/2015 150000 02/2015 150000 01/2015 150000 02/2015 150000 01/2015 150000 01/2015 150000 01/2015 150000 01/2015 150000 01/2015	1,451.67 134.24 57.05 18.58 7.06 64.70 44.09 140.39 116.74 16.97 10.67 10.67 210.04 10.67	

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101-1210-413.30-01	08/12/2014	INVOICE PAPER/BATTERIES	710184465001	150000	02/2015	66.88	
101-3040-424.30-01	08/13/2014	ENVELOPES	709393094001	150000	02/2015	135.59	
101-1210-413.28-11	08/13/2014	RETURN ENVELOPES	723686006001	150000	02/2015	249.05	
101-3020-422.30-01	08/01/2014	CR-RETURNED ITEMS	722016046001	150000	02/2015	113.80-	
101-5020-432.30-01	08/08/2014	BUS CRD/CORTEZ, LAU, MERCER	723371540001	150000	02/2015	132.26	
101-3040-424.30-01	08/22/2014	TAPE/BATTERIRES/STAMP/MIS	725744751001	150000	02/2015	81.16	
101-1230-413.30-02	08/22/2014	NAPKINS	725745446001	150000	02/2015	3.48	
08/29/2014	85231	OFFICETEAM	1266			4,535.88	
101-1020-411.21-01	08/11/2014	W/E 08/08/14 CARBALLO,S	41003144	150117	02/2015	1,461.18	
101-1020-411.21-01	07/28/2014	W/E 07/25/14 CARBALLO,S	40895074	150117	01/2015	1,108.00	
101-1020-411.21-01	08/04/2014	W/E 08/01/14 CARBALLO,S	40954964	150117	01/2015	1,108.00	
101-1020-411.21-01	08/18/2014	W/E 08/15/2014 CARBALLO,S	41052503	150117	02/2015	858.70	
08/29/2014	85232	PRINCIPAL FINANCIAL GROUP	2414			3,975.83	
101-0000-209.01-14	08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015	630.94	
101-0000-209.01-16	08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015	617.57	
101-0000-209.01-21	08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015	743.68	
101-0000-209.01-14	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	630.94	
101-0000-209.01-16	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	617.57	
101-0000-209.01-21	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	725.04	
101-0000-209.01-16	08/28/2014	AUG 2014 BASIC LIFE/AD&D/	AUG 2014		02/2015	10.09	
08/29/2014	85233	PRINCIPAL FINANCIAL GROUP	2428			1,216.05	
101-0000-209.01-13	07/17/2014	PAYROLL AP PPE 7/10/14	20140717		01/2015	608.03	
101-0000-209.01-13	07/31/2014	PAYROLL AP PPE 7/24/14	20140731		01/2015	608.03	
101-0000-209.01-13	08/28/2014	AUG 2014 VOL LIFE INSURAN	AUG 2014		02/2015	.01-	
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101-0000-209.01-12	08/28/2014	AUG 2014 DENTAL PPO	AUG 2014		02/2015	73.32	
101-0000-209.01-12	08/28/2014	AUG 2014 DENTAL PPO	AUG 2014		02/2015	40.42	
101-0000-209.01-12	08/28/2014	AUG 2014 DENTAL PPO	AUG 2014		02/2015	5.08	
101-0000-209.01-12	08/28/2014	AUG 2014 DENTAL PPO	AUG 2014		02/2015	.05-	
08/29/2014	85235	PRINCIPAL FINANCIAL GROUP	2525			1,913.00	
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101-0000-209.01-12	07/03/2014	PAYROLL AP PPE 6/26/14	20140703		01/2015	835.19	
101-0000-209.01-12	07/30/2014	JUL 2014 DENTAL PPO	JUL 2014		01/2015	40.42	
101-0000-209.01-12	07/30/2014	JUL 2014 DENTAL PPO	JUL 2014		01/2015	73.32	
101-0000-209.01-12	07/30/2014	JUL 2014 DENTAL PPO	JUL 2014		01/2015	40.42	
101-0000-209.01-12	07/30/2014	JUL 2014 DENTAL PPO	JUL 2014		01/2015	44.23	
101-0000-209.01-12	07/30/2014	JUL 2014 DENTAL PPO	JUL 2014		01/2015	44.23	
08/29/2014	85236	PRINCIPAL FINANCIAL GROUP	2428			1,216.05	
101-0000-209.01-13	08/14/2014	PAYROLL AP PPE 8/07/14	20140814		02/2015	608.03	
101-0000-209.01-13	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	608.03	
101-0000-209.01-13	08/28/2014	SEP 2014 VOLUNTARY LIFE	SEP 2014		03/2015	.01-	

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101-0000-209.01-12	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	835.19	
101-0000-209.01-12	08/28/2014	SEP 2014 DENTAL PPO	SEP 2014		03/2015	73.32	
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101-0000-209.01-12	08/28/2014	SEP 2014 DENTAL PPO	SEP 2014		03/2015	5.08	
101-0000-209.01-12	08/28/2014	SEP 2014 DENTAL PPO	SEP 2014		03/2015	.05-	
08/29/2014	85238	RANCHO AUTO & TRUCK PARTS	1685				115.37
501-1921-419.28-16	07/31/2014	OIL FILTER/HLD DWN BLT	7693-201733	150014	01/2015	13.20	
501-1921-419.28-16	08/06/2014	COPPER SPRK PLG	7693-202362	150014	02/2015	6.05	
501-1921-419.28-16	08/12/2014	#602 HINGE PIN/BUSHING KT	7693-202919	150014	02/2015	16.52	
501-1921-419.28-16	08/13/2014	OIL FILTER	7693-203070	150014	02/2015	5.19	
501-1921-419.28-16	08/19/2014	BUNGEE ASST	7693-203604	150014	02/2015	18.20	
501-1921-419.28-16	08/20/2014	OIL FILTERS	7693-203679	150014	02/2015	56.21	
08/29/2014	85239	RECLAIMED AGGREGATES, INC.	2137				75.00
101-5010-431.29-04	08/09/2014	TRUCK TO RECYCLE	74-ACC-02897	150033	02/2015	75.00	
08/29/2014	85240	RELIABLE TIRES COMPANY	136				65.50
101-5040-434.21-04	08/25/2014	USED TIRES DISPOSAL	90063	F15039	02/2015	65.50	
08/29/2014	85241	SAFeway SIGN COMPANY	2309				684.61
101-5010-431.21-23	08/15/2014	SCHOOL SIGNS	99815	150061	02/2015	684.61	
08/29/2014	85242	SAFETY-KLEEN SYSTEMS	246				286.28
501-1921-419.29-04	08/12/2014	BRAKE CLEANER	64277758	150039	02/2015	286.28	
08/29/2014	85243	SD COUNTY FIRE CHIEFS' ASSOCIA	354				150.00
101-3020-422.28-12	08/19/2014	CAPTAIN FRENCH - TRAINING	2014/2015	F15028	02/2015	50.00	
101-3020-422.28-12	08/19/2014	CLARK,T-FIRE CHIEF ANNUAL	2014-2015	F15027	02/2015	100.00	
08/29/2014	85244	SEIU LOCAL 221	1821				1,629.04
101-0000-209.01-08	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	1,629.04	
08/29/2014	85245	SKS INC.	412				7,710.00
501-1921-419.28-15	08/14/2014	1090 GAL REG FUEL	1264150-IN	150041	02/2015	3,914.69	
501-1921-419.28-15	08/21/2014	1091 GAL REG FUEL	1264308-IN	150041	02/2015	3,795.31	
08/29/2014	85246	SOUTHWESTERN COLLEGE	492				3,362.00
101-3030-423.28-04	07/21/2014	LG MARINE SAFETY TRAINING	4219	150212	01/2015	3,362.00	
08/29/2014	85247	SPARKLETTS	2341				81.11
101-3020-422.30-02	08/27/2014	JUL 2014	12529930 080114	150213	02/2015	81.11	
08/29/2014	85248	SPRINT	2040				149.97
101-3020-422.27-05	07/29/2014	06/26/14-07/25/14	594768811-080	150194	01/2015	149.97	
08/29/2014	85249	SUNGARD PUBLIC SECTOR INC.	1370				14,231.00
503-1923-419.20-25	08/12/2014	NAVILINE UPGRADE	86290	F15032	02/2015	2,800.00	

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ACCOUNT #	TRN DATE	DESCRIPTION	INVOICE	PO #	PER/YEAR	TRN AMOUNT	
503-1923-419.20-25	06/27/2014	COGNOS BI	84403	F15030	01/2015	10,791.00	
503-1923-419.20-25	06/27/2014	COGNOS BI PROJECT MGMT	84362	F15031	01/2015	640.00	
08/29/2014	85250	KEARNY PEARSON FORD	2485			4,895.35	
101-3020-422.50-04	07/28/2014	2014 FORD UTILITY AWD	CIB-003	150198	01/2015	4,895.35	
08/29/2014	85251	TERRA BELLA NURSERY, INC.	1946			469.57	
101-6040-454.30-02	08/05/2014	PREFORMANCE PASPALUM	125713	150028	02/2015	242.85	
101-6020-452.30-02	08/15/2014	MULCH	126415	150028	02/2015	226.72	
08/29/2014	85252	TREASURER, COUNTY OF S.D.	688			735.64	
101-3020-422.20-06	07/30/2014	14/15 CITY MEMBERSHIP SHA	UDC 2014-08	150197	01/2015	735.64	
08/29/2014	85253	US BANK	2458			3,207.36	
101-0000-209.01-20	08/28/2014	PAYROLL AP PPE 8/21/14	20140828		02/2015	3,207.36	
08/29/2014	85254	VALLEY INDUSTRIAL SPECIALTIES,	767			581.99	
101-6040-454.30-02	07/07/2014	CARTRIDGE REPLACEMENT UNI	206301	150027	01/2015	271.04	
101-6040-454.30-02	07/23/2014	REFUND CREDIT	206788	150027	01/2015	38.97	
101-6040-454.30-02	08/12/2014	HANDLE/STEM ASSEMBLY-VALV	207326	150027	02/2015	349.92	
08/29/2014	85255	VERIZON WIRELESS	2317			1,444.57	
101-5020-432.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	744.97	
101-3040-424.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	48.81	
101-3020-422.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	128.53	
101-3030-423.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	186.11	
101-3070-427.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	50.09	
101-1230-413.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	38.01	
503-1923-419.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	237.41	
503-1923-419.27-05	08/08/2014	07/09/2014-08/08/2014	9729994142		01/2015	10.64	
08/29/2014	85256	VORTEX INDUSTRIES, INC.	786			1,388.52	
101-3020-422.28-01	07/18/2014	ALUMINUM DOOR REPAIR	11-845545-1	150199	01/2015	251.00	
101-3020-422.28-01	06/18/2014	ALUMINUM DOOR REPAIR	11-831604-1	150199	01/2015	251.00	
101-1910-419.21-04	08/14/2014	STEEL DOOR REPAIRS	11-853788-1	150098	02/2015	530.52	
101-1910-419.21-04	05/19/2014	OVERHEAD DOOR REPAIRS	11-831345-1	150098	01/2015	356.00	
08/29/2014	85257	WAGE WORKS INC.	2210			107.75	
101-1920-419.29-04	08/15/2014	AUG 2015 FSA FEES	125AI0337385	150104	02/2015	107.75	
08/29/2014	85258	WAXIE SANITARY SUPPLY	802			1,867.23	
101-6040-454.30-02	08/04/2014	JANITORIAL SUPPLIES	74743508	150011	02/2015	516.59	
101-6040-454.30-02	07/25/2014	JANITORIAL SUPPLY	74726670	150011	01/2015	707.73	
101-6040-454.30-02	08/12/2014	JANITORIAL SUPPLIES	74760497	150011	02/2015	642.91	
08/29/2014	85259	WEST GROUP CTR	826			131.58	
101-1020-411.28-14	08/01/2014	JUL 2014	830032776	150217	01/2015	131.58	
08/29/2014	85260	ZEE MEDICAL, INC.	872			33.36	
101-1920-419.29-04	08/13/2014	FIRST AID CABINET REFILL	0140690387	F15022	02/2015	33.36	

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101-1020-411.30-01	06/29/2014	MISC OFFICE SUPPLIES	010199	150120	01/2015	15.10	
101-1120-412.29-04	06/30/2014	PROMOTIONAL EVENT ITEMS	664477257	150127	01/2015	235.87	
101-1020-411.28-04	07/07/2014	STAFF MEAL	040678	150120	01/2015	90.00	
101-1110-412.28-04	07/17/2014	DEPT MEETING LUNCH	001474	150124	01/2015	42.00	
503-1923-419.30-22	07/14/2014	MONITOR STAND	110-8049103-535	150166	01/2015	65.98	
503-1923-419.30-22	07/14/2014	VIDEO ROOM BACKUP POWER	110-8857134-939	150166	01/2015	158.47	
503-1923-419.28-13	07/17/2014	ADOBE ACROBAT PRO	110-3714384-936	150166	01/2015	371.69	
503-1923-419.28-13	07/21/2014	AMAZON PRIME SHIPPING	07-21-2015	150166	01/2015	106.92	
503-1923-419.30-22	07/21/2014	VIDEO CABLES	110-7290400-279	150166	01/2015	32.10	
101-1130-412.20-06	07/12/2014	MONTHLY NEWSLETTER	1405150548504	150127	01/2015	30.00	
101-1010-411.28-04	07/14/2014	CITY COUNCIL REFRESHMNTS	00073139	150127	01/2015	29.76	
101-1120-412.29-04	07/14/2014	COMMUNITY EVENT ITEMS	047790/4576859	150127	01/2015	15.60	
101-1120-412.29-04	07/14/2014	COMMUNITY EVENT ITEMS	104-2622761-943	150127	01/2015	95.73	
101-1120-412.29-04	07/14/2014	EVENT SIGNS	1793	150127	01/2015	278.00	
101-1110-412.30-02	07/15/2014	FLASH MEMORY CARD	104-0905685-067	150127	01/2015	72.35	
101-1120-412.29-04	07/15/2014	VELCRO FASTENERS	9491684479	150127	01/2015	63.50	
101-1120-412.29-04	07/17/2014	KNIT SHIRTS	67277	150127	01/2015	169.95	
503-1923-419.30-02	06/25/2014	LINECORD	085041	150142	01/2015	7.11	
503-1923-419.28-12	07/01/2014	BRADLEY, D-MISAC MEMBERSHI	300000757	150142	01/2015	240.00	
101-1230-413.28-11	06/25/2014	LABOR ROLLER REPAIR	21726	150121	01/2015	435.00	
101-1230-413.28-11	07/03/2014	MICROFICHE IMAGES SCANNED	21660	150121	01/2015	62.64	
101-1230-413.28-04	07/17/2014	NAKAGAWA, J-ULI LUNCHEON	1741297	150123	01/2015	50.00	
101-3035-423.25-03	04/17/2014	JRLG UNIFORMS	27971	150141	01/2015	854.29	
101-3035-423.25-03	04/24/2014	JRLG UNIFORM TSHIRTS	28090	150141	01/2015	88.99	
101-3035-423.25-03	06/26/2014	JRLG UNIFORM TSHIRTS	67094	150141	01/2015	142.56	
101-3035-423.30-02	06/30/2014	BINDER CLIPS/LETTER FILES	010519	150141	01/2015	12.64	
101-3035-423.30-02	06/30/2014	JRLG AIRHORNS	042949	150141	01/2015	36.70	
101-3035-423.30-02	07/02/2014	JRLG WATERPROOF CAMERA	BBY01-656603024	150141	01/2015	346.90	
101-3030-423.30-02	07/03/2014	CAMERA BATTERY	75020300	150141	01/2015	18.36	
101-3035-423.28-04	07/09/2014	JRLG BANQUET REFRESHMENTS	07-09-2014	150141	01/2015	200.00	
101-3035-423.25-03	07/09/2014	JRLG INSTRUCTOR WETSUIT	6226307	150141	01/2015	64.66	
101-3035-423.25-03	07/13/2014	JRLG TEMP STORAGE	904511312	150141	01/2015	98.58	
101-3035-423.25-03	07/13/2014	JRLG TEMP STORAGE	904511313	150141	01/2015	98.58	
101-3035-423.30-02	07/14/2014	JRLG CERTIFICATES/SEALS	7582	150141	01/2015	38.08	
101-3035-423.30-02	07/16/2014	JRLG PARADE SUPPLIES	015778/2018245	150141	01/2015	11.56	
101-3030-423.30-02	07/16/2014	SHIPPING RETURN CHGS	073288	150141	01/2015	5.95	
101-3035-423.28-04	07/16/2014	JRLG BANQUET LUNCHEON	096285	150141	01/2015	168.84	
101-3035-423.28-04	07/16/2014	JRLG BANQUET AWARDS	8142	150141	01/2015	321.49	
101-3035-423.30-02	07/17/2014	JRLG PARADE EQUIPMENT	034718/1018609	150141	01/2015	2.59	
101-3035-423.28-04	07/17/2014	JRLG BANQUET LUNCHEON	930776	150141	01/2015	330.00	
101-1130-412.28-04	01/10/2014	CORTEZ, E TRAINING	11832	150128	01/2015	137.00	
101-3030-423.28-04	05/09/2014	ARC CARD PROCESSING	CCAC050214	150137	01/2015	38.00	
101-3040-424.28-12	06/03/2014	HOLDEN, J ICCI MEMBERSHIP	3006708	150122	01/2015	125.00	
101-3040-424.28-04	06/24/2014	HOLDEN -CERTIFICATION RNW	VTJPBFF02F68	150122	01/2015	181.25	
101-1230-413.28-11	06/24/2014	MICROFICHE TONER	06-24-2014	150122	01/2015	150.00	
101-1130-412.28-14	06/30/2014	LABOR LAW POSTERS	302648	150128	01/2015	86.18	
101-3030-423.28-04	06/04/2014	CPR CARD PROCESSING	CCAC060414	150137	01/2015	14.00	

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ACCOUNT #	TRN DATE	DESCRIPTION	INVOICE	PO #	PER/YEAR	TRN AMOUNT	
101-3030-423.28-01	06/18/2014	BEACH TOWER NUMBER SIGNS	0153030	150137	01/2015	302.24	
101-3030-423.25-03	06/23/2014	UNIFORMS WETSUIT	6224742	150137	01/2015	32.91	
101-3040-424.28-12	07/07/2014	HOLDEN, J-IAPMO MEMBERSHIP	N16615938779864	150122	01/2015	200.00	
101-1130-412.28-04	07/09/2014	DH TRAINING REFRESHMENTS	023560	150128	01/2015	4.50	
101-1130-412.28-04	07/09/2014	DH TRAINING LUNCH	046146	150128	01/2015	92.76	
101-1130-412.20-06	07/10/2014	LIVESCAN	056210	150128	01/2015	19.00	
101-3030-423.28-01	06/29/2014	SIGN RIVETS	054763/9563897	150139	01/2015	17.76	
101-3030-423.30-02	06/29/2014	OFFICE SUPPLIES	4055	150139	01/2015	194.75	
101-3030-423.30-02	06/26/2014	STABENOW, R UNIFORM SHOES	4576	150140	01/2015	64.79	
101-3030-423.28-01	06/30/2014	SIGN RIVETS	045039/8564004	150140	01/2015	19.38	
101-3030-423.30-02	07/09/2014	LG ACTIVITY CARDS	017491	150139	01/2015	367.20	
101-3030-423.30-02	07/14/2014	MEDICAL SUPPLIES	681172	150139	01/2015	151.06	
101-3030-423.30-02	07/14/2014	MEDICAL SUPPLIES	81485014	150139	01/2015	176.95	
101-3030-423.28-01	07/03/2014	BEACH TOWER CHAIR REPAIRS	644177	150140	01/2015	550.00	
101-3030-423.28-04	07/08/2014	CPR CERTIFICATION TRNG	R340340188	150140	01/2015	22.00	
101-3030-423.28-04	07/09/2014	STABENOW-USLA ASSOC DUES	300005310	150140	01/2015	30.00	
101-3030-423.30-02	07/14/2014	BATTERIES	045572/4580939	150140	01/2015	28.04	
101-3030-423.28-01	07/14/2014	COMMAND VEHICLE WASH	090814	150140	01/2015	8.99	
101-3030-423.30-02	07/14/2014	DIVE TEAM KNIFE	7998	150140	01/2015	108.75	
101-3030-423.25-03	06/27/2014	WOMENS UNIFORM SWIMSUITS	JOLYN-163160	150138	01/2015	123.34	
101-3020-422.29-02	06/24/2014	QUARTERLY EMPL LUNCHEON	00026386	150134	01/2015	280.62	
101-3020-422.30-02	06/30/2014	COLOR CARTRIDGES	9710380632	150134	01/2015	494.56	
101-3020-422.28-09	07/08/2014	SHIPPING ENGINE SUPPLIES	060908	150135	01/2015	34.20	
101-3020-422.30-02	07/18/2014	PARADE SIGN MATERIAL	036012/0590523	150135	01/2015	28.62	
101-3020-422.30-02	07/18/2014	MINI USB ADAPTER	2146532	150135	01/2015	21.55	
101-3020-422.28-09	07/01/2014	SHIPPING ENGINE PARTS	000964	150136	01/2015	22.84	
101-3020-422.30-02	07/10/2014	SAFETY VEST/ MOUNT	63661	150136	01/2015	111.23	
101-3020-422.30-02	07/11/2014	FD CLEANING SUPPLIES	017984	150136	01/2015	122.63	
101-3020-422.28-09	07/16/2014	SHIPPING -RADIOS	018383	150136	01/2015	184.85	
101-6040-454.30-02	07/03/2014	BEACH WHEELCHAIR BRAKES	1083	150147	01/2015	402.59	
101-1910-419.28-01	07/08/2014	07/03/14 LABOR-FIRE PANEL	20364	150147	01/2015	142.50	
101-6040-454.30-02	07/10/2014	SUNSCREEN	1038	150147	01/2015	37.76	
101-6040-454.30-02	07/10/2014	BEACH WHEELCHAIR PARTS/JO	1086	150147	01/2015	925.35	
101-6040-454.30-02	07/10/2014	BEACH WHEELCHAIR BRAKES	1086 -DOUBLE CH	150147	01/2015	925.35	
101-6040-454.30-02	07/10/2014	BEACH WHEELCHAIR MOTORS	1087	150147	01/2015	1,883.42	
101-5010-431.30-02	07/15/2014	WAX/RAGS/BUFFER-SURFBOARD	RP 3980-246954	150147	01/2015	38.83	
101-6020-452.30-02	07/15/2014	BENCH SKATE STOPPERS	19474	150147	01/2015	99.90	
101-5010-431.30-02	07/17/2014	WAX	084678	150147	01/2015	17.26	
101-5010-431.30-02	07/17/2014	WD-40/STEEL CLEANER	096754/1570335	150147	01/2015	30.09	
601-5060-436.28-01	07/18/2014	STAINLESS STEEL FLATBAR	370240	150157	01/2015	89.12	
101-5010-431.30-02	07/16/2014	T50 STAPLES/GORILLA GLUE	046979/2120651	150160	01/2015	21.87	
601-5060-436.30-02	06/25/2014	SILICONE & GLOVES	022983/3574047	150157	01/2015	115.82	
101-1910-419.30-02	06/23/2014	CLAMPS/SANDPAPER/TAPE	065041/5563030	150150	01/2015	31.50	
501-1921-419.30-02	06/23/2014	CLAMPS/SANDPAPER/TAPE	065041/5563030	150150	01/2015	12.11	
101-1910-419.30-02	06/23/2014	PLUMBING RESTOCK	205988	150150	01/2015	290.04	
101-3020-422.30-02	06/25/2014	ELECTRICAL SUPPLIES	073319/3573987	150150	01/2015	46.08	
101-6020-452.30-02	06/25/2014	IRRIGATION SUPPLIES	2019271-00	150150	01/2015	85.04	
601-5060-436.28-01	06/16/2014	PACKING MATERIAL	SO-38621	150152	01/2015	197.21	
501-1921-419.28-16	06/30/2014	#5403 REAR WINDOW REPLCMN	WCV034618	150161	01/2015	323.31	
101-5010-431.30-02	07/16/2014	GRAFFITI SUPPLIES	032883/2222492	150149	01/2015	82.39	

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101-6020-452.30-02	07/08/2014	CONCRETE-REAMA PARK	094207/0041805	150150	01/2015	53.78	
101-3020-422.30-02	07/14/2014	CLOSET SUPPLIES	039437/4017544	150150	01/2015	190.59	
101-3020-422.30-02	07/17/2014	CLOSET SUPPLIES	037847/1013556	150150	01/2015	19.56	
101-3020-422.30-02	07/17/2014	CLOSET SUPPLIES	074040/1013458	150150	01/2015	106.07	
201-5015-531.20-06	06/30/2014	CONSPICUITY TAPE/ROUNDABO	3082	150159	01/2015	339.82	
501-1921-419.28-16	06/23/2014	KUBOTA FILTERS	199128	150162	01/2015	120.66	
501-1921-419.28-16	06/30/2014	GROUNDING CLAMP	092922	150162	01/2015	15.11	
501-1921-419.28-16	06/30/2014	#600 DRIVER SEATBELT	15955	150162	01/2015	81.00	
101-6020-452.28-01	06/30/2014	REEL MOWER BLADE SHARPEN	211133	150162	01/2015	80.00	
101-5010-431.21-23	07/08/2014	GRAFFITI PAINT-RECYCLED	5468	150159	01/2015	205.89	
101-5010-431.30-02	07/14/2014	BIKE PATH POSTS EMULSION	088253/4017650	150159	01/2015	44.06	
101-5010-431.30-02	07/15/2014	FAST SETTING CONCRETE	017706/8017818	150159	01/2015	86.05	
101-5010-431.30-02	07/31/2014	CRAFFITI CAMERA BATTERY	2728	150159	01/2015	56.50	
501-1921-419.28-16	07/01/2014	#600 PASAGER SEATBELT	15999	150162	01/2015	54.00	
101-6020-452.28-01	07/07/2014	REEL SHARPENED/ADJUSTED	211205	150162	01/2015	80.00	
501-1921-419.28-01	07/14/2014	#102 SMOG	50396	150162	01/2015	41.75	
501-1921-419.28-01	07/17/2014	#107 SMOG	50419	150162	01/2015	41.75	
101-6040-454.30-02	06/17/2014	E-Z REACHERS	08183876-00	150155	01/2015	408.28	
101-6040-454.30-02	06/25/2014	POWDER COAT VENT & FRAME	06-25-2014	150155	01/2015	50.00	
101-6040-454.30-02	06/26/2014	GLOVES/BROOM/DUSTPAN	029932/2563509	150155	01/2015	68.80	
101-6040-454.30-02	06/27/2014	GROUT/TILE ADHESIVE	000341/1563610	150155	01/2015	77.53	
101-6040-454.30-02	06/27/2014	CARTIER, J-SAFETY BOOTS	045697	150155	01/2015	142.23	
101-1110-412.20-06	06/26/2014	PORTABLE RESTROOMS	06-26-2014	150163	01/2015	1,850.50	
101-6040-454.30-02	07/02/2014	WATERFALL BULLNOSE TILE	07-02-2014	150155	01/2015	89.58	
101-6040-454.30-02	07/02/2014	EMPLOYEE KEYS	07-02-2014	150155	01/2015	11.98	
101-6040-454.30-02	07/09/2014	5 GALLON PAINT	066533/9580285	150155	01/2015	149.69	
101-5010-431.30-02	07/03/2014	ALL PURPOST MULIT-TOOL	012483/5575159	150163	01/2015	157.47	
101-3020-422.30-01	07/08/2014	PLOTTER PAPER STOCK	H130107822	150163	01/2015	112.31	
101-6020-452.30-02	07/10/2014	COUNSTUCTION ADHESIVE	026405/8576245	150163	01/2015	10.39	
101-5020-432.28-04	07/17/2014	GLORIA, G-ELECTRICITY TRNG	91740	150163	01/2015	990.00	
101-5010-431.30-02	07/18/2014	TARP/UTILITY KNIVES	075034/0590421	150163	01/2015	49.90	
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DATE RANGE TOTAL *								1,514,247.46 *



**STAFF REPORT
CITY OF IMPERIAL BEACH**

TO: HONORABLE MAYOR AND CITY COUNCIL

FROM: ANDY HALL, CITY MANAGER *AH*

MEETING DATE: SEPTEMBER 17, 2014

ORIGINATING DEPT.: COMMUNITY DEVELOPMENT DEPARTMENT
GREG WADE, ASSISTANT CITY MANAGER/ COMMUNITY DEVELOPMENT DIRECTOR *GW*
JIM NAKAGAWA, AICP, CITY PLANNER

SUBJECT: CONSENT AGENDA: CONSULTANT SELECTION FOR CLIMATE READY SEA LEVEL RISE STUDY. MF 1025

EXECUTIVE SUMMARY:

Approve the selection of Revell Coastal/ USC Sea Grant as the consultant for the Imperial Beach Climate Ready Sea Level Rise Study.

PROJECT DESCRIPTION/ BACKGROUND:

The Imperial Beach City Council adopted Resolution No. 2013-7369 on August 7, 2013 to authorize the submittal of the Climate Ready Sea Level Rise (SLR) grant application. The City was awarded a \$300,000 Climate Ready grant (Grant Agreement 13-090) by the State Coastal Conservancy (SCC) on January 23, 2014 to conduct a vulnerability assessment and develop adaptation strategies to address the effects that sea level rise could have on coastal resources along the Pacific Ocean shoreline of Imperial Beach. \$190,000 of the grant is dedicated to the CoSMoS 3.0 modeling work that is to be done by Dr. Patrick Barnard of the USGS in Santa Cruz. The remaining \$110,000 of the grant would fund the stakeholder engagement process, the vulnerability assessment, and the development of adaptation strategies. \$57,000 of the grant was set aside for a consultant to assist the City with the SLR study.

PROJECT EVALUATION/ DISCUSSION:

Five proposals were submitted expressing interest in performing a SLR study for Imperial Beach. City staff in concert with the Tijuana River National Estuarine Research Reserve (TRNERR) staff conducted consultant interviews with four of the consultant teams. The rankings of the proposals were initially quite divergent which necessitated holding interviews to narrow down the selection. Each team brought certain strengths to the table but the interviews were helpful in being able to narrow down the choice to Revell Coastal/ USC SeaGrant and TerraCosta Consulting Group. Both teams had authored sea level rise studies that were held in high academic standing. It was exceedingly difficult to decide between the top two but the

interview panel felt that Revell had a slight edge in being able to communicate scientific climate change/sea level rise information to lay citizens. Revell Coastal and USC Sea Grant also had close ties to the development of the CoSMoS model by Patrick Barnard and were awarded a State Coastal Conservancy (SCC) grant to conduct a community outreach program of the CoSMoS model in Southern California. The SCC would also need to approve the City's selection of the consultant for the Climate Ready Imperial Beach Sea Level Rise study but the SCC advised the City that we may proceed with our selection process.

LOCAL COASTAL PROGRAM (LCP)/ GENERAL PLAN (GP): This grant does **not** include a requirement to amend the LCP that identifies strategies that would adapt to sea level rise impacts and protect coastal resources.

ENVIRONMENTAL DETERMINATION PURSUANT TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA):

The selection of a consultant for a sea level rise study is not a project per CEQA.

FISCAL ANALYSIS:

The contract amount of \$57,000 for the sea level rise consultant will be funded by the State Coastal Conservancy's Climate Ready grant.

DEPARTMENT RECOMMENDATION:

Approve the selection of Revell Coastal/ USC Sea Grant as the Climate Ready sea level rise consultant.

CITY MANAGER'S RECOMMENDATION:

Approve Department recommendation.

Attachments:

1. Revell Coastal Proposal
 2. Request For Proposals/Qualifications
- c: file MF 1025 IB Climate Action Plan
Danielle Boudreau, Tijuana River National Estuarine Research Reserve, 301 Caspian Way, Imperial Beach, CA 91932 dboudreau@trnerr.org
Nicola Hedge, Climate Initiative Manager, San Diego Foundation, 2508 Historic Decatur Road, San Diego, CA 92106 nicola@sdfoundation.org
Moiria McEnespy, Deputy Manager, South Coast Program, State Coastal Conservancy, 1330 Broadway, Floor 13, Oakland, CA 94612-2530 mmcenespy@scc.ca.gov
Dr. David Revell, Revell Coastal, LLC, 125 Pearl Street, Santa Cruz, CA 95060 revellcoastal@gmail.com and Juliette Finzi Hart, Ph.D., USC Sea Grant jahart@usc.edu

Walter Crampton and Reinhard Flick, Ph.D., TerraCosta Consulting Group, Inc., 3890
Murphy Canyon Road, Suite 200, San Diego, CA 92123
wcrampton@terracosta.com rflick@terracosta.com rflick@ucsd.edu
Andy Pendoley, MIG, Inc., 3965 5th Avenue, Suite 420, San Diego, CA 92103
andyp@migcom.com and Steve Messner, E360, 480 Gate 5 Road, Suite 300,
Sausalito, CA 94965 smessner@e360group.net
Justin Vandever, PE, and Teri Fenner, AECOM, 1420 Kettner Blvd., Suite 500, San
Diego, CA 92101 Justin.Vandever@aecom.com Teri.Fenner@aecom.com
Hany Elwany, Ph.D., Coastal Environments, 2166 Avenida de la Playa, Suite E, La Jolla,
CA 92037 hany@coastalenvironments.com

City of Imperial Beach Sea Level Rise Study "Adaptive" Vulnerability and Adaptation Planning for a Southern California Coastal City



Proposal Submitted by Dr. David Revell

Revell Coastal, LLC

125 Pearl Street, Santa Cruz, CA 95060

revellcoastal@gmail.com | 503.577.4515

Submittal Due Date: August 1, 2014





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Schedule of Rates & Proposed Budget (In Separate Sealed Envelope as requested)

Appendices

Appendix A: Data Needs for the Santa Barbara County Coastal Resiliency Phase 1 Project

Appendix B: US Geological Survey Letter of Support

Appendix C: Personnel Resumes

Appendix D: Insurance Coverage



Revell Coastal, LLC
125 Pearl Street
Santa Cruz, CA 95060
503-577-4515

Mr. James Nakagawa, AICP
Community Development Department
City of Imperial Beach
825 Imperial Beach Blvd.
Imperial Beach, CA 91932

Dear Jim and the City of Imperial Beach,

It is with great excitement that Revell Coastal, LLC, and the University of Southern California Sea Grant program submit this proposal to assist with the City of Imperial Beach's sea level rise vulnerability assessment and development of adaptation strategies. Our team commends the City for its leadership in supporting the U.S. Geological Survey's (USGS) Coastal Storm Modeling System (CoSMoS 3.0) regional Southern California project. We have assembled a unique team whose members have been leaders in the field of climate vulnerability and adaptation assessments across the state of California. This same team is working with USGS on vulnerability studies and outreach of CoSMoS 3.0 in Los Angeles, Orange, Santa Barbara, Ventura and San Diego Counties.

Our overall approach is to interpret and apply sound science and modeling results to examine the vulnerabilities within the City and identify suitable adaptation strategies. We will help develop communication products of study results to support building internal City capacity at the staff and elected official level, as well as to inform the general public to help them evaluate and ultimately vote to implement potential future courses of action. We will work closely with the City and TRNERR staff and propose to accomplish this project through a series of interactive workshops with relevant City departments consistent with the guidance identified by the California Coastal Commission and other regulatory agencies. Revell Coastal will lead the vulnerability and adaptation portions of the project with USC Sea Grant leading outreach and education, as well as providing support to Revell Coastal in the assessment of vulnerabilities.

The team is co-led by project managers Dr. David Revell of Revell Coastal in Santa Cruz, California and Dr. Juliette Finzi Hart of USC Sea Grant in Los Angeles, CA. Dr. Revell has been involved in pioneering climate-related work beginning in 2008 with the initial technical hazard analysis and vulnerability assessment for the Pacific Institute study "*The Impacts of Sea Level Rise to the Coast of California*." Dr. Revell has also completed hazard modeling for the *Coastal Resilience Ventura* project for the Nature Conservancy (2013), and the *Monterey Bay Sea Level Rise Vulnerability Assessment* (2014) for the State Coastal Conservancy. Dr. Revell led an interdisciplinary team of economists, legal scholars, and engineers to evaluate potential erosion mitigation strategies (adaptation strategies) in Southern Monterey Bay (2012). Revell Coastal combines the best available science on climate change to



interpret the potential hazards and vulnerabilities, and provides recommendations on adaptation strategies tailored to fit local jurisdictions needs and political will across multiple planning horizons.

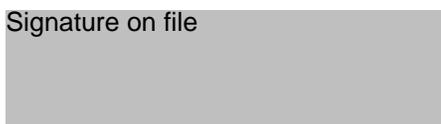
USC Sea Grant promotes and funds innovative research to help find solutions for pressing coastal management problems and to ensure that science truly help resolve the issues of greatest ecological and social importance. USC Sea Grant works to increase the public’s understanding of marine and climate science. They bring decision makers, scientists and the public together to find solutions that are informed by the best available science, which is understood by varied stakeholders and is supported by local communities. Dr. Hart and the team at USC Sea Grant have led a number of successful climate change and coastal hazards projects, including the statewide *2011 California Coastal Climate Adaptation Needs Assessment* to better understand coastal communities’ research and information needs and the barriers they face in planning for sea level rise and other climate impacts, the *Sea Level Rise Vulnerability Assessment for the City of Los Angeles (2013)*, as well as ongoing projects including the *Regional AdaptLA Coastal Impacts Project*, and CoSMoS 3.0 outreach for Southern California.

Revell Coastal and USC Sea Grant are currently under contract to work with Dr. Patrick Barnard and USGS to both map coastal hazards in L.A. County and to provide strong outreach and education to the multiple jurisdictions across the Southern California region (see accompanying letter of support from Dr. Barnard). Additionally, Revell Coastal is working with Dr. Barnard on improving the mapping of climate change hazards for Santa Barbara County. We note here that our team has no conflict of interest with any City officials or active climate leaders in the SD region nor does this proposal conflict with our ongoing work in the Southern California region.

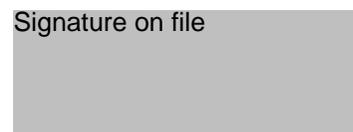
While our team does not have a strong local presence, our team actively works and coordinates with the Tijuana River National Estuarine Research Reserve, the Coastal Conservancy, Coastal Commission staff. We also work closely with many of the climate change leaders in San Diego, including the San Diego Foundation, the Nature Conservancy, Scripps Institution of Oceanography, and the San Diego Regional Climate Collaborative. Additionally, USC Sea Grant plays a leadership role as an executive committee member for the Alliance of Regional Collaboratives for Climate Adaptation (ARCCA).

Collectively our team brings an extensive range of skills and expertise from around the state and an approach that supports local community responses while remaining consistent with ongoing climate efforts around Southern California.

Sincerely,

Signature on file


David L. Revell, Ph.D.
Principal
Revell Coastal, LLC

Signature on file


Juliette Hart
Marine & Climate Science Specialist
USC Sea Grant

STRATEGIC PLAN

Introduction

The City of Imperial Beach (hereafter “The City”) was awarded a \$300,000 Climate Ready grant by the State Coastal Conservancy on January 23, 2014. The goal of this project is to conduct a vulnerability assessment to develop adaptation strategies to address the effects that sea level rise could have on coastal resources along the Pacific Ocean shoreline of Imperial Beach. Responding to the RFP/Q developed by the City, Coastal Revell, LLC and USC Sea Grant (hereafter “The Team”) are pleased to submit this application. Below, we describe our proposed strategic plan to work with the City to help develop a comprehensive, science-based and stakeholder-supported “adaptive” adaptation planning process.

Based on our previous experiences on adaptation planning projects across the state, we propose to work with City and TRNERR staff to organize Imperial Beach’s planning effort around six stakeholder meetings that will link directly to the tasks identified in the RFP/Q. We anticipate substantive follow-up work and report-writing to occur in between and subsequent to the stakeholder meetings. We will work with the City to develop a suite of working deliverables that include an existing conditions report, a geospatial data set used as a cornerstone of the vulnerability assessment, a vulnerability assessment report and an adaptation strategies report. At the culmination of the process, we anticipate also delivering a readily digestible report comprised of succinct sections on vulnerabilities and adaptation strategies with recommendations on additional technical studies, policy and plan updates.

The process we outline below relies heavily on stakeholder support and participation. Identifying those stakeholders who will be engaged throughout the process, who can provide the necessary information, and who will need to make difficult decisions as sea level rise begins to impact the coastline will be imperative. Having an involved and supportive internal and external stakeholder group builds support for decisions and future directions. We anticipate working closely with the City, TRNERR staff, and the Coastal Conservancy project manager, to identify the appropriate community representatives to form a robust and productive steering committee.

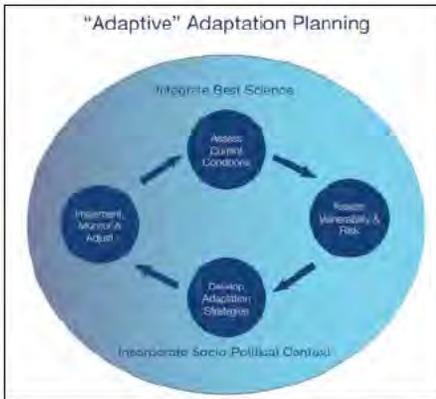
We understand our role as working with the City to support staff, expand technical capabilities and to complement their existing skill sets. Our hope is that by providing our unique experience and expertise we will be able to both provide guidance, support or leadership where/as needed, and also build capacity within the City so that the City is able to update and refine their adaptation plan as new information becomes available. What we describe below is our proposed approach, but we anticipate refining this approach based on the City’s needs and recommendations. In time we will measure our success when the City views us as critical members of their team.

Our team brings together distinctive skill sets that have been honed from a variety of climate-related projects across the state. The City will benefit from lessons learned elsewhere as well as an increased focus of our CoSMoS 3.0 outreach and education. Dr. Hart of USC Sea Grant will work with the City and TRNERR on tasks related to outreach, education, and stakeholder engagement with support from Revell Coastal. Revell Coastal with Dr. Revell as the project manager will lead the vulnerability assessment and adaptation planning with support from USC Sea Grant.

Timing of models and “adaptive” adaptation planning

Both Revell Coastal and USC Sea Grant are integrally involved with the development, release and outreach of CoSMoS 3.0 for the Southern California region and are therefore aware that results of the CoSMoS 3.0 model may not be available until the end of the process. However, we do not see this as an impediment to completing this project. As described in USC Sea Grant’s *AdaptLA*, a City of Los Angeles-focused assessment that laid the groundwork for the CoSMoS 3.0 outreach for Southern California cities, as well as through the Team’s collective experiences in California, we advocate developing an “adaptive” adaptation planning process that is nimble and iterative and accommodates

new sources of information as they becomes available. Science is ever-evolving and information becomes more refined as scientists increase their understanding, improve their models, and learn from the work of others through the scientific process.



As noted in the RFP, it is suggested that we begin the process utilizing results from CoSMoS 1.0. We will work with our wide network of contacts and the City to determine if this is still the best information to use given the current ongoing work in San Diego (San Diego 2050, the CURRV project, Pacific Institute, and ongoing work at Scripps Institution of Oceanography etc.). Both Revell Coastal and USC Sea Grant are adept at helping

communities understand the scientific process and identifying the best currently-available sources of scientific information to guide planning.

This best available information will be used to begin the data-gathering portion of the project to help identify existing conditions and vulnerable assets. We anticipate integrating CoSMoS 3.0 model results into the final vulnerability assessment as well as during the evaluation of potential adaptation strategies. We will work closely with City staff to help develop an “adaptive” process that will leverage existing information but also be able to accommodate this new information.

Link to regional CoSMoS 3.0 effort

As noted above, USC Sea Grant is contracted by the California State Coastal Conservancy to conduct outreach on the results of the CoSMoS 3.0 model and to build capacity in coastal communities throughout the Southern California region to utilize this information in their sea level rise planning (see also letter of support from Dr. Patrick Barnard). This outreach will include initial workshops throughout the Southern California region that will describe the adaptation planning process. This will be followed with a series of educational webinars that will provide information on topics ranging from overviews on relevant state guidance documents to legal implications of sea level rise planning to the economic impacts of climate change. (We identify below how these webinars will supplement our proposed strategic work plan.) A final workshop will be held once model results are available in which we will provide technical assistance and guidance on how to incorporate model results into planning activities. USC Sea Grant will be partnering and collaborating closely with the San Diego Climate Collaborative and the San Diego Foundation in these outreach efforts, which would serve to further link the City of Imperial Beach’s work to the larger regional San Diego context.

Dr. Revell will be collaborating with the USGS as a technical advisor on the development of COSMOS 3.0. As Revell Coastal is co-located in Santa Cruz with USGS, Dr. Revell will be working closely with Dr. Barnard and expanded modeling team to improve the projections of coastal hazards particularly the integration of long term coastal evolution, erosion and flooding.

If our proposal is selected, the City will receive guidance and support above and beyond what other communities in the Southern California region will receive. This is certainly warranted given the City’s substantial investment in helping fund the Southern California regional CoSMoS 3.0 effort. This places the City in a unique regional leadership position among its neighbors and simultaneously provides a synergistic opportunity for those neighbors to view an adaptation planning process “in action.”

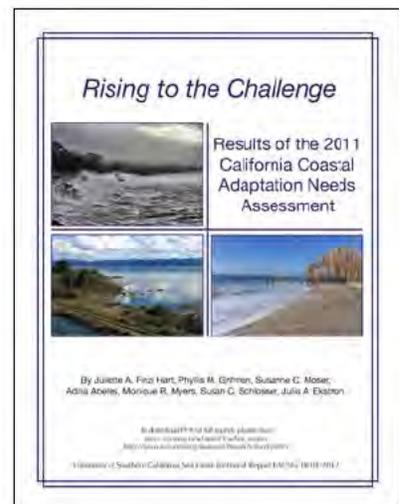
Communicating Climate Science

We understand that the City of Imperial Beach faces numerous challenges beyond those anticipated from projected sea level rise. The City has a small planning staff and numerous other pressures on community and economic development and reinvestment, particularly due to the recent cut in redevelopment funds. Indeed, in USC Sea Grant’s 2011 survey, we found that top barriers to adaptation planning include:

- Insufficient staff resources to analyze relevant information
- Lack of funding to implement a plan
- Current pressing issues are all-consuming.

The City is therefore, sadly, not unique in the challenges it faces. Both Revell Coastal and USC Sea Grant are skilled at working with communities to help them build internal capacity to understand and implement an adaptation planning process and to support City staff in ongoing planning efforts. Equally, both organizations are skilled at communicating complex science to varied audiences, with different viewpoints and potentially different political priorities.

In other efforts, this has helped move sea level rise planning higher on city agendas and priorities and change the tone between stakeholders who often hold conflicting perspectives. We anticipate that this will be an important component of our work on this project if we are selected.



Familiarity with Imperial Beach, the San Diego Region and Relevant Stakeholders

Finally, though neither Revell Coastal nor USC Sea Grant are located in San Diego, we have worked with a number of critical regional stakeholders and climate leaders throughout the years and therefore think we have strong understanding and connection to the needs of the City of Imperial Beach. Here we highlight some of our collaborations with San Diego regional entities over the last five years:

- The San Diego Climate Collaborative, the San Diego Foundation and TRNERR will all be important partners in the CoSMoS 3.0 outreach effort. We have already begun discussions about when and where to hold initial meetings and how to link CoSMoS 3.0 outreach to the multitude of San Diego sea level rise planning efforts.
- USC Sea Grant worked with Kristen Goodrich and Dani Boudreau from TRNERR in 2011 and 2012 to develop two multi-county climate adaptation planning workshops that were held in Costa Mesa, CA. The first focused on conservation in a changing climate and the second focused

on providing more information on the various sea level rise models and tools available to communities (see Qualifications section for more information on this “Beyond Bathub” workshop at which Dr. Revell presented one of the models).

- USC Sea Grant serves on the executive committee of the Alliance of Regional Collaboratives for Climate Adaptation (ARCCA) with Laura Engeman from the Climate Collaborative and Nicola Hedge from the San Diego Foundation. Through this work, we are able to share lessons learned and transfer knowledge on our respective planning efforts.
- Both Dr. Revell and Dr. Hart participated in the scenario planning exercise for the Temporal Investigations of Marsh Ecosystems, which fed into the TRNERR CURRV project for the Tijuana River Valley.

Proposed Workplan

In the following section, we provide details on a proposed workplan for accomplishing the tasks outlined in the RFP/Q. We have developed this process based on past experience and lessons learned from case studies. While we believe we have developed a robust and effective workplan, we certainly anticipate working with the City and TRNERR staff to develop a workplan that fits both the needs and planning requirements of the City.

Task 1: Stakeholder Engagement and Identification of Existing Conditions

Expected Timeframe: September – December 2014

In our experience, the most successful adaptation planning projects are those that pause at the beginning of the effort to set up a coordinated planning process that has buy-in and support from critical stakeholders. We will therefore work with the City and TRNERR to help establish a robust planning process and formalize a strong stakeholder working group (SWG) which represents diverse interests and cross-sectoral interests for SLR planning. This will include regulatory agency staff who will have to evaluate and permit the implementation of adaptation strategies for the Imperial Beach Pacific coastline.

Once the SWG is established, the first step will be to identify current observed vulnerabilities and existing conditions using historic storm events. We propose to work with the City to identify the best available scientific, socioeconomic, cultural and natural resource information. We will work with the City’s GIS specialist to overlay this information on coastal maps; Revell Coastal also has strong GIS capabilities and a GIS Specialist available to support this planning process as needed. In the first workshop, utilizing these maps, we will work with the

stakeholders to identify critical coastal assets and systems, including the various important ecosystems, contaminated sites, residential and commercial building stock, municipal assets, among others (e.g. those listed on pg. 5 of the City’s RFP/Q as well as



City of L.A. representatives discuss existing conditions and current vulnerabilities along the coast. (Photo: Marika Schulhof)

others identified by the SWG).

Following this workshop, we will help City staff collate the information gathered during the workshop, conduct follow-up with stakeholders to clarify any gaps or outstanding questions and support the City's development of the existing conditions report. Based on the budget allocation in the RFP/Q, we assume that the City will take the lead on this report. We are prepared to play a larger role in report development should this be requested by the City. The report will be sent to the SWG for review. We will then help revise relevant sections of the report based on SWG input to develop a final existing conditions report.

Task 1 Workshop Description

Workshop #1 – Identify Existing Conditions and Develop Report

- Introduce stakeholder working group to the City of IB SLR Planning Process
- Discuss data needs and schedule (see Appendix A for example from recent Santa Barbara work)
- Use maps of the coastline to identify coastal assets and highlight and examine current observed vulnerabilities and existing conditions. Begin by examining the systems listed in the RFP/Q and allow stakeholders to identify and prioritize other important systems/assets.
- Provide information on vulnerability assessments to prepare stakeholders for next phase.

Coordination with Regional CoSMoS 3.0 Outreach¹:

- We propose to hold the initial San Diego-region focused CoSMoS 3.0 outreach workshop at a time that matches most with the City's planning process. The entire SWG will be invited to participate.
- IB SWG will be invited to participate in webinars that will supplement the initial existing conditions work. These webinars may include overviews of the vulnerability assessment process, state level resources available, and/or legal implications of sea level rise adaptation planning.

Task 2: Develop and Refine Vulnerability Assessment

Expected Timeframe: December 2014 – August 2015

Once the City and SWG have a good understanding of what the current conditions are and what vulnerabilities they already currently face, the next step is to begin to assess vulnerabilities based on future sea level rise scenarios. While strong scientific information is critical for trying to identify what the future will look like for the City, it is unlikely that we will have the results from CoSMoS 3.0 to use at this stage in the process. We will work with the City and SWG to identify the best sea level rise information to use given the current ongoing work in San Diego (e.g. CoSMoS 1.0, San Diego 2050, modeling from the CURRV project, Pacific Institute, and/or other ongoing work at Scripps Institution of Oceanography). We recommend combining historic storm impacts coupled with the most appropriate interim climate projected data sets to support the vulnerability assessment. GIS-based maps will be developed that will help identify the City's assets' and systems' exposure, sensitivity and adaptive

¹ We would like to note that all Southern California communities will be invited to participate in the workshops and webinars for the CoSMoS 3.0 outreach. However, given the substantial contribution from the City to the development of CoSMoS 3.0, we anticipate holding the initial workshop to match the timing of the City's needs. Similarly, we will try to schedule webinar content to match the particular timely needs of the City.

capacity due to flooding, extreme events, erosion, salt water intrusion, water table rise and ecosystem shifts for selected coastal assets (these will be based on recommendations from the City and SWG and will likely begin with those listed on pg. 5 of the RFP/Q).

We suggest relying heavily on engaged stakeholder participation in the vulnerability assessment. City and community members in Imperial Beach have much of the information necessary for understanding the range of vulnerabilities. From recent experience in Santa Barbara County, we have a robust list of resources that help assess vulnerabilities; this will be adapted and used to support information gathering (see Appendix A). To the extent possible, we will groundtruth the initial vulnerability assessment with the SWG and the City.

We recommend developing the vulnerability assessment in this task through two stakeholder workshops. The first workshop (Workshop #2a)

would focus on providing training to the SWG and City on conducting a vulnerability assessment and initial data-gathering through map-based guided discussion breakout groups. We would also recommend inviting one or several regional stakeholders (e.g. Dani Boudreau from TRNERR, Nicola Hedge from the San Diego Foundation and/or Laura Engeman from the Climate Collaborative) to this workshop to help place the City’s work in the context of the larger San Diego region. We will help the City and TRNERR staff collate and summarize information gathered during the workshop and conduct follow-ups with stakeholders to address any outstanding questions



Coastal Resilience Ventura Vulnerability Assessment for evaluating impacts to Power generation stations (The Nature Conservancy)

and to refine our understanding of the specific vulnerabilities (e.g. what specific components of the wastewater treatment system are most critical). This will provide the basis for developing an initial vulnerability assessment report, which will be sent out for review to the SWG. The report will be revised based on the SWG input. The second workshop in this task (Workshop #2b) would focus on reviewing the vulnerability assessment report and examining it through a cross-sectoral lens. In many instances, vulnerabilities transcend departmental lines. Having the opportunity to discuss the vulnerabilities with all City stakeholders will help identify potential roadblocks and ultimately streamline the next phase of adaptation planning. From past experience we have found that this cross-pollination helps improve communication within jurisdictions and supports the identification and acceptance of multi-objective adaptation strategies. Based on results from this second workshop, we will work with the City to revise and finalize the vulnerability assessment.

Task 2 Workshop Descriptions

Workshop 2a - Initial Vulnerability Assessment Development (Workshop #2):

- Vulnerability Assessment Training
- Presentations by regional leaders to provide regional San Diego context for City’s planning effort

- Guided discussion in breakout groups by sector to identify asset vulnerabilities that will identify exposure, sensitivity and adaptive capacity due to flooding, extreme events, erosion, salt water intrusion, water table rise and ecosystem shifts.

Workshop 2b: Refining of Vulnerability Assessment Report

- Discuss report as a group.
- Identify cross-sectoral vulnerabilities
- Begin preliminary discussion of potential adaptation strategies

Coordination with CoSMoS 3.0 Regional Outreach:

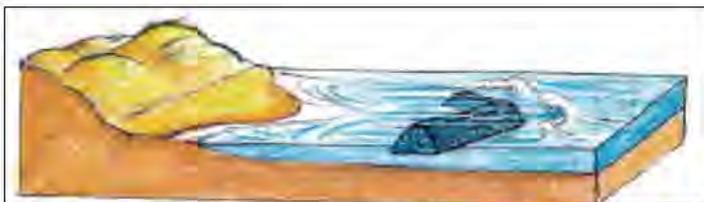
- IB SWG will be invited to participate in webinars that will supplement the vulnerability assessment. These webinars may include overviews on social vulnerability assessments, and/or what adaptation options are available to communities and/or the economic impacts of climate change.

Task 3: Development and Evaluation of Adaptation Strategies

Expected Timeframe: June 2015 – January 2015

We see this specific task as critical to project success so that the City has some specific directions to move forward with long range planning and implementation. There are a wide number of adaptation strategies available to communities. Identifying which are most in line with the City and community vision, and thus politically palatable, is critical. Through this adaptation task, the ultimate goal is to identify a subset of effective strategies that provide the City with options on how to reduce the identified vulnerabilities and move forward with planning, policy changes, and/or implementation including potential design and/or construction.

In this specific task we advise organizing the work through three workshops. The first workshop (Workshop #3a) could including training on the full suite of adaptation measures that could be applied to addressing sea level rise and coastal storm impacts. This suite will be drawn from national reports including the *Sea-Level Rise for the Coasts of California, Oregon, and Washington: Past, Present, and Future*, the *2009 California Adaptation Strategy* and the draft *Safeguarding California: Reducing*



Conceptual example of an artificial reef used as a sand retention structure as an adaptation measure.

ESA PWA 2012

Climate Risk, the Coastal Commission’s *Draft Sea Level Rise Policy Guidance*, the State Coastal Conservancy documents, as well as regional reports relevant to the San Diego region, including the *Sea Level Rise Adaptation Strategy for San Diego Bay*, and other regional efforts including the *Technical Erosion Mitigation Study* in Southern Monterey Bay (ESA PWA 2012), *Sea Level Rise Vulnerability Study for the City of Los Angeles*, and the *Climate Adaptation Plan* from the City of Santa Cruz.

We will also train the City and SWG on the “adaptive” adaptation planning approach we describe above. By this first workshop, we anticipate having model results from CoSMoS 3.0. If that is the case, we will work with the City and the SWG to use the adaptive planning approach to refine the vulnerability assessment by incorporating the new information.

Based on our experiences working on similar projects elsewhere and from case studies in other locales, the Team will select a subset of these adaptation measures that we believe would be viable and politically-tractable options for Imperial Beach. During the second workshop (Workshop # 3b), the Team will present on the subset of measures and provide more details on each approach including the opportunities and constraints of each. We will facilitate a guided discussion to gather input from the City and SWG to determine if this subset of measures is appropriate and if/how it should be amended. This adaptation measure list will be categorized into five types of approaches used: 1) Avoid hazards, 2) Move development away from hazards; 3) Move hazards away from development; 4) Provide barriers between hazards and development 5) Flood-proof.²

Following input from the City and stakeholder working group, the Team will develop a report that combines subsets of alternative measures into a series of adaptation strategies that could be applied to reduce the specific vulnerabilities identified in Task 2. A succinct adaptation strategy report chapter will be drafted including a series of overall strategies that could be applied to each vulnerability. Results will be partially presented through a summary matrix that clearly compares and contrasts the strength, weaknesses and relative costs of each strategy as it applies to the specific types of vulnerabilities.

In the final workshop (Workshop #3c), the results of this adaptation strategy report will be presented and feedback solicited from the City and the steering committee. The Team will work with the City and the TRNERR staff to incorporate revisions into a final report which will include specific recommendations for any additional technical studies such as a cost/benefit analysis over time or a feasibility study to get a better sense of actual costs or legal risks associated with selected adaptation measures or strategies.

Task 3 Workshop Descriptions

Workshop 3a - Initial Vulnerability Assessment Development (Workshop #2):

- Adaptation Strategies Training
- “Adaptive” Adaptation Planning Training
- If CoSMoS 3.0 model results are available, update vulnerability assessment

Workshop 3b: Refining of Vulnerability Assessment Report

- Team will present in the subset of range of measures and provide more details on each approach including the opportunities and constraints of each
- Facilitated discussion to gather feedback on proposed adaptation strategies
- Begin preliminary discussion of potential adaptation strategies

Workshop 3c: Refining of Vulnerability Assessment Report

- Review identified adaptation strategies and report

² For a matrix that identifies a suite of potential adaptation strategies, please see the *Sea Level Rise Vulnerability Study for the City of L.A.* The matrix starts on pg. 60. The summary report, which includes the matrix, can be accessed here: <http://dornsife.usc.edu/uscseagrant/la-slr/>

Coordination with CoSMoS 3.0 Regional Outreach:

- IB SWG will be invited to participate in the San Diego-focused CoSMoS 3.0 Technical Assistance workshop, which will provide detailed information about model results.
- IB SWG will be invited to participate in webinars that will supplement the adaptation strategies identification. These webinars may include overviews on “adaptive” adaptation planning and/or communicating climate science and policies.

Task 4: Presentation to the Tidelands Advisory Committee and Imperial Beach City Council

Expected Timeframe: December 2015 – January 2016

Revell Coastal and USC Sea Grant will work with the City to develop presentations to be delivered to the Tidelands Advisory Committee and Design Review Board (TBD) and the Imperial Beach City Council. As noted above, both organizations have considerable experience in developing communication products that are appropriate for a wide range of audiences. Revell Coastal will be available to present at these meetings, as appropriate. USC Sea Grant will provide support.

Summary and Concluding Thoughts

We believe we have assembled an excellent team to help the City of Imperial Beach in its sea level rise adaptation planning effort. Both Revell Coastal and USC Sea Grant have broad experience in applying and communicating scientific information related to climate change and supporting coastal communities identify vulnerabilities and implement adaptation strategies to reduce coastal impacts. We believe that our links to the CoSMoS 3.0 Southern California project, and substantive relationships with regional climate change partners in the San Diego region will prove to be very beneficial to the City. Both organizations are very excited about the opportunity to work with the City of Imperial Beach from start to finish, and hope that this project and process can serve as a demonstration to other communities across the country on how to complete an effective adaptation planning process.



QUALIFICATIONS AND EXPERIENCE

*To create and inspire sustainable communities by sharing our passions
for the coast and ocean environments with others.*

FIRM PROFILE AND QUALIFICATIONS

Revell Coastal, LLC is a coastal management firm that specializes in coastal geomorphology, coastal lagoon dynamics, beach and dune sediment management, and climate change vulnerability and adaptation planning with expertise along the entire U.S. West Coast. Revell Coastal approaches projects by applying the best available science on climate change to interpret the potential hazards and vulnerabilities, and provides recommendations on adaptation strategies tailored to fit local jurisdictions needs and political realities across multiple planning horizons. The firm focuses on improving the use of science in coastal management decisions and provides scientific facilitation, expert witness, technical analysis, field surveying and planning services. Revell Coastal, LLC was founded in 2014 by Dr. David Revell in Santa Cruz, California.

Dr. Revell has been involved in coastal management and coastal process assessments for over 20 years. He has been pioneering climate related work beginning in 2008 with the initial technical hazard analysis and vulnerability assessment for the Pacific Institute study “*The Impacts of Sea Level Rise to the Coast of California.*” Dr. Revell has also completed hazard modeling for the *Coastal Resilience Ventura* project for the Nature Conservancy (2013), and the *Monterey Bay Sea Level Rise Vulnerability Assessment (2014)*, for the California Coastal Conservancy. Dr. Revell has led an interdisciplinary team of economists, legal scholars, and engineers to evaluate potential erosion mitigation (adaptation strategies) strategies in Southern Monterey Bay (2012).

SELECT FIRM EXPERIENCE

Issues and Considerations: Incorporating Climate Change into the Local Coastal Program for the City of Goleta

Lead: Revell Coastal, LLC

Firm Project Manager: Dr. David Revell

Client Project Manager: Chandra Slaven (formerly Krout), Senior Planner, City of Goleta 805-961-7544, cslaven@cityofgoleta.org

Award Date: July 2014

Completion Date: October 2014 (anticipated)

Project Description: Revell Coastal is working for the City of Goleta to update and draft their LCP to include climate change impacts and be consistent with the California Coastal Commission DRAFT Guidance on Sea Level Rise. This includes reviewing technical background materials, conducting a high reconnaissance level vulnerability assessment, identifying adaptation strategies and policy recommendations and drafting sections of the Local Coastal Program related to coastal hazards, specifically in the open space, safety and conservation elements. Revell Coastal will be developing a summary report for use in internal City capacity building and externally as outreach and educational materials. Funding for this work was acquired from the California Coastal Commission through their climate granting program.

Goleta Slough Sea Level Rise Vulnerability and Adaptation Study and Ecosystem Management Plan Update

Lead: ESA (former employer of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: Rachel Couch, California Coastal Conservancy for the Goleta Slough Management Committee, 805-845-8853 rcouch@scc.ca.gov

Award Date: October 2012

Completion Date: October 2014 (scheduled)

Project Description: The Goleta Slough Ecosystem Management Plan was prepared by the Goleta Slough Management Committee and adopted by the City of Santa Barbara in 1997. As part of an update to the Management Plan, the Coastal Conservancy is funding a vulnerability and adaptation study to re-evaluate the study area based on projected sea level rise. Through a facilitated stakeholder process and technical analyses, the study is assessing vulnerability and risk to both natural and human resources and infrastructure. Based on results of the vulnerability assessment a series of adaptation strategies are being recommended that include both capital improvements and policy recommendations. Dr. Revell has managed the project, the stakeholder process, led the technical analyses and provided guidance and direction to the subcontractors.

Monterey Bay Sea Level Rise Vulnerability Assessment, Monterey and Santa Cruz Counties

Lead: ESA (former employer of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: Dennis Long, Executive Director, Monterey Bay Sanctuary Foundation (831) 647-4209, dennis@mbnmsf.org

Award Date: June 2012

Completion Date: June 2014

Project Description: With funding from the California Coastal Conservancy, Natural Capital Project, and City of Capitola, ESA PWA (former employer of Dr. Revell) modeled projected climate change impacts to the coast of Monterey Bay at a scale suitable for planning purposes. Deliverables included projected future coastal hazards which include a new integrated approach of stepping through time eroding the coast and flooding newly eroded areas through hydraulic connectivity. The project was being advised by a Monterey Bay region wide technical advisory group comprised of research institutions (UCSC, Naval Postgraduate School, Moss Landing, CSUMB and USGS), local planning agencies (Santa Cruz, Monterey Counties, Cities of Monterey, Santa Cruz, Seaside, Sand City, Capitola), and other technical experts. Uncertainty in the sea level rise projections were represented by mapping the range of projected coastal impacts and overlapping them to develop an uncertainty index that showed the relative risk. Dr. Revell managed the project and led the technical work for ESA.

The Nature Conservancy's Coastal Resilience - Ventura

Lead: ESA (former employer of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: Sarah Newkirk, J.D., Coastal Programs Director, (415) 730-7437, snewkirk@tnc.org

Award Date: September 2012

Completion Date: July 2014

Project Description: The Coastal Resilience Ventura (www.coastalresilience.org) project encompasses the entire Ventura County coastline, including Mugu Wetlands, the Santa Clara River, and the Ventura River. Working through an interactive stakeholder process with multiple agencies, local government representatives, and several non-profit organizations, Dr. Revell led a technical team which modeled current and potential future coastal and fluvial hazards for a variety of climate change scenarios, including sea level rise and changes to rainfall and sediment delivery. We applied a habitat evolution model (Sea Level Affecting Marsh Migration - SLAMM) to predict long-term wetland conversion with sea level rise, as well as to identify areas of ecological vulnerability based on potential adaptation strategies. SLAMM model results were then used to calculate the greenhouse gas emissions related to multiple adaptation strategies. The results are being used by local communities and the Department of Defense to evaluate vulnerabilities and consider adaptation strategies. Dr. Revell was the project manager, led the technical modeling, and worked with the client on a project development team and as a co-facilitator for the stakeholder process.

Goleta Beach Erosion Projects for the County of Santa Barbara

Lead: PWA, ESA, UCSC (former employers of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: (multiple) Current project manager is Alex Tuttle, Planner, Development Review County of Santa Barbara, 805-884-6844, atuttle@countyofsb.org

Award Date: September 2003

Completion Date: April 2014

Project Description: The 1997-98 El Niño catalyst an erosion wave which caused substantial erosion to Goleta Beach County Park in Santa Barbara County. Dr. Revell has been involved in multiple stakeholder processes and technical studies over the last 11 years working primarily for the County of Santa Barbara. These studies have included peer reviewed scientific research, technical studies on various adaptation strategies with several alternative park reconfigurations and managed retreat options. Dr. Revell also peer reviewed other technical modeling and led a technical team to conduct wave run-up and climate change modeling to support the Environmental Impact Report on a managed retreat strategy. Throughout the process, Dr. Revell has presented research results to stakeholder group.

Neskowin Shoreline Assessment, Neskowin, Tillamook County, Oregon

Lead: ESA (former employer of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: Patrick Corcoran, Oregon Seagrass Coastal Hazard Specialist, 503-325-8573, patrick.corcoran@oregonstate.edu

Award Date: August 2012

Completion Date: May 2013

Project Description: In response to high rates of erosion and sea level rise that have diminished the beaches and now threatens homes and roads in Neskowin, OR, ESA analyzed the viability of various coastal erosion mitigation strategies on an eroding shore, utilizing existing information from local academics (Oregon State University) and agencies (including the Department of Geology and Mineral Industries), as well as applying our experience completing assessments for similar high-energy wave-exposed coastal areas. We applied modeling to evaluate physical changes from various adaptation strategies and provided conceptual level engineering cost estimates for each strategy to inform



community decision making. The community was striving to find a balance of private property protection with maintenance of a sandy beach to support the tourist economy. Dr. Revell managed the project, led the technical analyses and co-facilitated the community process.

Technical Evaluation of Erosion Mitigation Alternatives and Regional Sediment Management Plan for Southern Monterey Bay

Lead: ESA(former employer of Dr. Revell)

Firm Project Manager: Dr. David Revell

Client Project Manager: Brad Damitz, Natural Resource Specialist, 415-250-8406

brad.damitz@noaa.gov

Award Date: October 2010

Completion Date: May 2012

Project Description: The Evaluation of Erosion Mitigation Alternatives for Southern Monterey Bay (Alternatives Study - <http://montereybay.noaa.gov/research/techreports/esapwa2012.pdf>) provided an assessment of various erosion mitigation measures (adaptation strategies) to support development of a regional strategy to address coastal hazards. In this study, 22 different erosion mitigation measures including land use planning tools, soft engineering solutions and hard engineering solutions. The measures were compared using a variety of criteria including an innovative effectiveness criteria which compared each measures merit at protecting upland property and beach widths (a highly valued community resource). The Study then compared the costs and benefits of each measure over multiple time horizons by tracking the physical impacts of each measure on the beach and upland through time. The holistic cost/benefit analysis included accounting for beach recreation and ecosystem services in addition to traditional storm damages. While this initial study did not include climate impacts directly, it began to inform adaptation strategies across the region and was the basis for a recently kicked off project called Adapt Monterey Bay funded by the California Coastal Conservancy Climate Ready Grant Program. Dr. Revell managed the project for ESA and led the technical work as well as contributed to the successful Climate Ready grant proposal.

Coastal Infrastructure and Vulnerability Impacts Assessment for the Pacific Institute

Lead: Pacific Institute

Firm Project Manager: Dr. David Revell (working for Philip Williams and Associates)

Client Project Manager: Matt Heberger, P.E., Water Resource Associate/Climate Program Coordinator, Pacific Institute, 510-251-1600, mheberger@pacinst.org

Award Date: June 2008

Completion Date: January 2009

Project Description: With funding from the Ocean Protection Council as part of the 2008 California Climate Impacts Assessment through the California Energy Commission, PWA conducted the first California statewide coastal hazard assessment resulting from sea level rise. This groundbreaking project, the first of its kind on the U.S. West Coast, mapped projected future coastal erosion and coastal flooding hazards. (<http://www.energy.ca.gov/2009publications/PWAOPC-1000-2009-013/PWAOPC-1000-2009-013.PDF>) To complete this project, PWA (led by Dr. Revell) developed a new methodology which evaluated geomorphic response of various backshore types by applying a total water level methodology (Revell *et al* 2011); collaborated with climate change researchers at Scripps, USGS, and Oregon State; organized and engaged a technical and regulatory peer review team on methods and results; then collaborated with Pacific Institute to vulnerability assessment associated with coastal



hazards which were published in the Pacific Institute Report: *The Impacts of Sea Level Rise to the California Coast*. Dr. Revell managed the project and led the technical work for PWA.

USGS-UCSC Coastal Processes Study for Santa Barbara and Ventura Counties

Lead: USGS

Firm Project Manager: Dr. David Revell (for University of California Santa Cruz)

Client Project Manager: Dr. Patrick Barnard, Coastal Hazards Scientist, USGS 831-460-7556, pbarnard@usgs.gov

Award Date: October 2005

Completion Date: December 2007

Project Description: The USGS and UCSC collaborated on a large scale coastal process study of the Santa Barbara littoral cell. <http://pubs.usgs.gov/of/2009/1029/>. Building on research by Dr. Revell (formerly at UCSC) assessing the long term changes to beaches from climate change and human impacts, the study examined seasonal changes through extensive field data collection campaigns and conducted numerical modeling to further inform observations of short term and seasonal changes and provide insights into long term shoreline evolution. Dr. Revell, conducted research and managed the UCSC research team led by Dr. Gary Griggs.

USC SEAGRANT: SELECT QUALIFICATIONS

Rising to the Challenge: 2011 California Coastal Climate Adaptation Needs Assessment

Lead: USC Sea Grant

Firm Project Manager: Dr. Juliette Finzi Hart

Client Project Manager: Joshua Brown, Climate Lead & Deputy Fiscal Officer, NOAA Sea Grant, (301) 734-1271, Joshua.brown@noaa.gov

Award Date: January 2011

Completion Date: May 2012

Project Description: USC Sea Grant, with 16 statewide partners – including TRNERR – conducted a survey of coastal professionals with the goal of understanding the needs and barriers coastal communities have in planning for climate change, developing appropriate trainings and technical assistance for communities, and determining the best way to link communities to resources and tools already available. This survey of over 600 coastal professionals found that while there is a now significant awareness of the need for adaptation, California coastal communities are by and large still very early in their adaptation efforts, (Finzi Hart 2012). Citing the results of this study, the State of California made \$2.3 million available to coastal communities to help them prepare and plan for the impacts of climate change along the coastline.

City of Los Angeles Sea Level Rise Vulnerability Study

Lead: USC Sea Grant

Firm Project Manager: Dr. Juliette Finzi Hart, Phyllis Grifman, Alyssa Newton Mann

Client Project Manager: Beth Jines, Director of Strategic Initiatives, Los Angeles Department of Water and Power, 213.367.5368, beth.jines@ladwp.com

Award Date: December 2011

Completion Date: January 2014

Project Description: USC Sea Grant led the City of L.A., along with a team of science and outreach experts, to develop a science-based and stakeholder-supported adaptation planning process to begin planning for the impacts of climate change. The team conducted an assessment of the potential physical, social and economic impacts of sea level rise on the City's resources and population, as well as the possible impacts to coastal and shoreline assets.

Beyond Bathtub: Modeling and Responding to Sea Level Rise and Shoreline Change

Lead: USC Sea Grant

Firm Project Manager: Dr. Juliette Finzi Hart

Client Project Manager: Moira McEnespy, Deputy Program Manager, South Coast, State Coastal Conservancy, 510.286.4165, mmcenespy@scc.ca.gov

Award Date: May 2012

Completion Date: December 2012

Project Description: Coastal managers in California are faced with the challenge of protecting coastal environments and resources from the impacts of climate change. Shoreline change - resulting from the confluence of sea-level rise (SLR), coastal erosion, storm surge, El Niño, flooding, and inundation - threatens coastal communities, infrastructure, and natural habitats. As more models and tools have become available to aid in the development of vulnerability assessments and adaptation strategies, a need was articulated among coastal managers to better understand these models and tools. In order to develop a better understanding of the above topics, this workshop convened a variety of speakers to help bridge the gap between coastal managers and modelers. This effort resulted in the project team developed for the Regional AdaptLA project described below and was instrumental in securing funding for CoSMoS 3.0.

Regional AdaptLA: Coastal Impacts Planning for the Los Angeles Region

Lead: USC Sea Grant

Firm Project Manager: Dr. Juliette Finzi Hart, Alyssa Newton Mann and Phyllis Grifman

Client Project Manager: Shannon Parry, Sustainable Santa Monica Coordinator, 310.348.2227, Shannon.parry@smgov.net

Award Date: Contract still being finalized

Completion Date: TBD

Project Description: The Los Angeles Coastal Climate Change Impacts Project, funded by the Ocean Protection Council (OPC) will fund the development of a shoreline change and coastal erosion model for the Los Angeles region. The model will be developed by a strong team of experts (Revell Coastal, ESA, and TerraCosta Consulting Group – based in San Diego). They will integrate their work, which is focused on shoreline and beach response, with the work led by the U.S. Geological Survey to develop a CoSMoS 3.0 for Southern California.

Coastal Storms Modeling System 3.0 – Outreach and Capacity-Building for Southern California

Lead: USC Sea Grant

Firm Project Manager: Dr. Juliette Finzi Hart, Alyssa Newton Mann and Phyllis Grifman

Client Project Manager: Moira McEnespy, Deputy Program Manager, South Coast, State Coastal Conservancy, 510.286.4165, mmcenespy@scc.ca.gov

Award Date: May 2014

Completion Date: Underway



Surf. Sand. Sustainability.

Project Description: USC Sea Grant is contracted by the California State Coastal Conservancy to conduct outreach on the results of the CoSMoS 3.0 model and to build capacity in coastal communities throughout the Southern California region to utilize this information in their sea level rise planning (see also letter of support from Dr. Patrick Barnard). This outreach will include initial workshops throughout the Southern California region that will describe the adaptation planning process. This will be followed with a series of educational webinars that will provide information on topics ranging from overviews on relevant state guidance documents to legal implications of sea level rise planning to the economic impacts of climate change. A final workshop will be held once model results are available in which we will provide technical assistance and guidance on how to incorporate model results into planning activities.

LIST OF PROJECT PERSONNEL

See attached Personnel Resumes (Appendix C)

Prime Consultants

David L. Revell, PhD

Principal, Chief Scientist

Revell Coastal, LLC

Primary Responsibility: Project Manager, Vulnerability and Adaptation Planning

Alex Snyder

GIS Specialist

Revell Coastal, LLC

Primary Responsibility: GIS, Spatial Analyses, and Vulnerability Assessment

Sub-consultants

Juliette Finzi Hart

Marine & Climate Science Specialist

USC Sea Grant

Primary Responsibility: Co-Project Manager, Stakeholder Engagement and Outreach

Phyllis Grifman

Associate Director

USC Sea Grant

Primary Responsibility: Stakeholder Engagement and Outreach

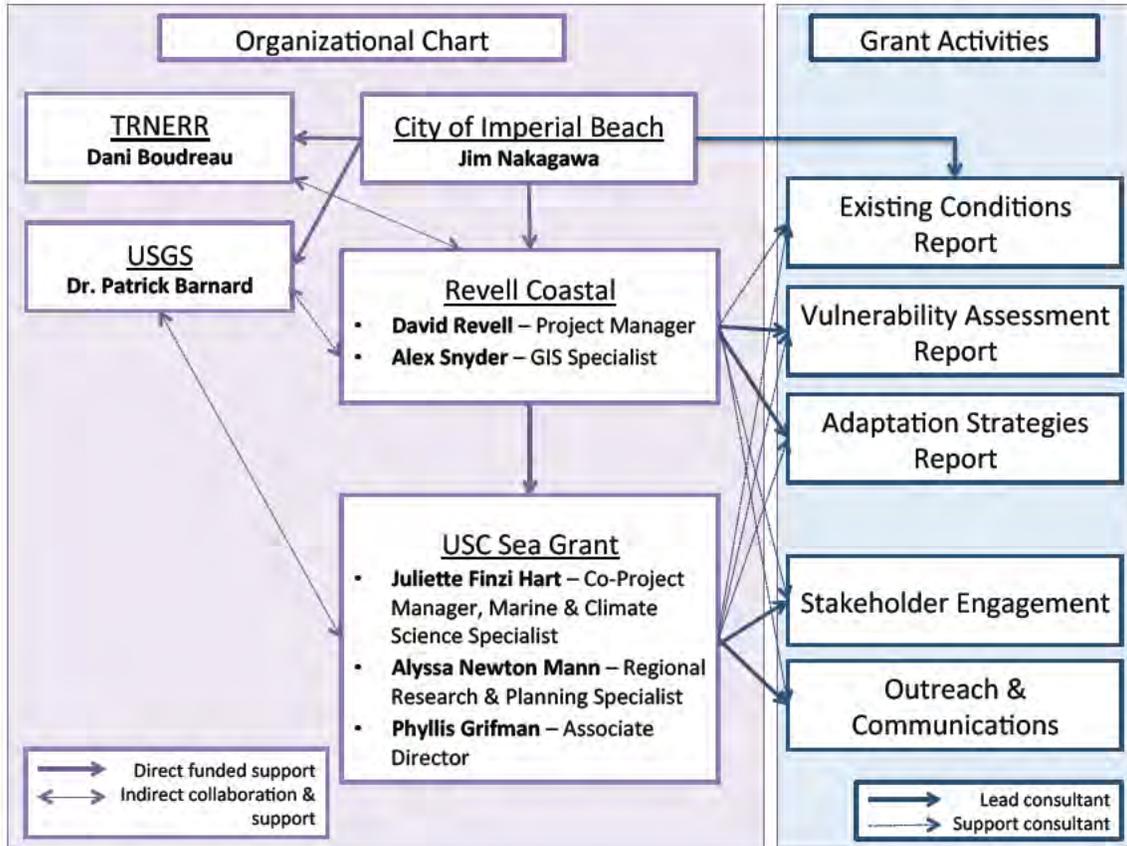
Alyssa Newton Mann

Regional Research & Planning Specialist

USC Sea Grant

Primary Responsibility: Stakeholder Engagement and Outreach

ORGANIZATIONAL CHART





INSURANCE COVERAGE

See attached Certificates of Insurance from Revell Coastal, LLC and USC Sea Grant for specific details (Appendix D)

Revell Coastal, LLC

General Commercial Liability

Professional Liability

Commercial Automobile Insurance

*Note all staff are independent contractors

USC Seagrant

General Commercial Liability

Professional Liability

Commercial Automobile Insurance

Workers Compensation



APPENDICES

Appendix A: Data Needs for the Santa Barbara County Coastal Resiliency Phase 1 Project

Appendix B: US Geological Survey Letter of Support

Appendix C: Personnel Resumes

Appendix D: Insurance Coverage



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Data Needs for the Santa Barbara County Coastal Resiliency Phase 1 Project

By David L.Revell, PhD

The list below represents data needed to initiate and complete the Santa Barbara County Coastal Resiliency Phase 1 Project. This data falls into several tasks within the larger scope of work. Ideally this information would be in a digital and georeferenced format (GIS preferred, but CAD ok as well). It is assumed that much of this data exists within various departments within each jurisdiction covering varying areas of interest. It is requested that all data that exists not be clipped to a specific jurisdiction (unless significant data size limitations exist) as this will be used to compile a regional data set that is hoped to benefit all of the participating jurisdictions.

Task 3: Regional Resource Data: This data collection will support the vulnerability assessment (Tasks 6, and 7)

- 3.1. Habitat classification and vegetation mapping – specifically NON national scale databases (e.g. National Wetland Inventory, NOAA C-CAP data, etc)
- 3.2. Area of Special Biological Significance
- 3.3. Locations of Threatened and Endangered Species (public version)
- 3.4. Disadvantaged communities (special state designation)
- 3.5. Landuse / Zoning
- 3.6. Location and or elevation of any utilities (e.g. natural gas, electrical, fiber optic)
- 3.7. Location and elevation of wastewater infrastructure (including septic fields)
- 3.8. Location and elevation of water supply infrastructure (potable and reclaimed)
- 3.9. Parcel maps and associated assessors databases
- 3.10. Building footprints
- 3.11. Base floor elevations
- 3.12. Cultural resources (public version)
- 3.13. Road surface elevations

The following data needs are specifically for **Task 4: Modeling and Mapping of Coastal Hazards**. This data will be used to calibrate and/or validate model results, and to understand hydraulic connectivity across the landscape which will affect flood flow pathways.

- 4.1. Any historic photos or documentation of extents of past storm damages from coastal flooding and or coastal erosion including depth of flooding or size of cliff failure (e.g. Shoreline Park 2007, More mesa 1989). This should come with a sense of location and date (time stamp is even better).
- 4.2. Location (footprints) and elevations of barriers to wave up-rush and flooding (e.g. seawalls, revetments).
- 4.3. Locations and elevations of any infrastructure on the beach (e.g. groins, outfalls)
- 4.4. Location and elevations of water control infrastructure (e.g. tide gates)
- 4.5. Location and elevation of the top and bottom of the storm drains including descriptors of size, presence or absence of flap gates



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- 4.6. Any consulting or scientific reports on geologic stability. Specifically measurements of subsidence or uplift within the study area.
- 4.7. Compilation, citations, consulting reports of cliff and/or shoreline erosion rates

GIS specific questions

Does the County have a preferred projection and Horizontal datum (preferably not UTM since the Zone changes between 10 and 11 at ~El Capitan)?

Can we request data with a standardized vertical datum (preferably to NAVD88)?

Does the County have an FTP site where all of this data can reside, or should ESA set up a location?



United States Department of the Interior

U. S. GEOLOGICAL SURVEY
Coastal and Marine Geology
400 Natural Bridges Drive
Santa Cruz, California 95060-5792

July 30, 2014

Mr. James Nakagawa, AICP
City Planner
Community Development Department
City of Imperial Beach
825 Imperial Beach Blvd.
Imperial Beach, CA 91932

Dear Jim Nakagawa,

I am writing to express my strong support for the enclosed proposal from Revell Coastal and the University of Southern California (USC) Sea Grant program to assist the City of Imperial Beach in assessing its vulnerability to sea level rise and helping identify a suite of potential adaptation strategies. The project managers from Revell Coastal (David Revell, Ph.D.) and USC Sea Grant (Juliette Finzi Hart, Ph.D.) have both worked on sea level rise projects throughout the state and have a deep understanding of both sea level rise science and how to use this science in policy development and planning as well as communicate to a wide range of audiences. I have worked closely with both David and Juliette on previous projects, including the current Southern California Climate Impacts Project, and recommend Revell Coastal and USC Sea Grant highly for this proposed project.

I have been working with David since 2003 and have published multiple scientific papers with him on coastal processes in Southern California. We have also been involved on technical committees reviewing sea level rise and coastal modeling approaches, and I have been involved as a peer reviewer on some of his coastal hazard modeling work for the Pacific Institute. David, who was formerly with ESA PWA, has been a leader and collaborator on sea level rise modeling and will be involved in the development of COSMOS 3.0 in Southern California. He will be working closely with us to complete the coastal hazard modeling work specifically in Santa Barbara and Los Angeles County, which may extend throughout the Southern California region depending on upcoming funding levels. David has worked on several groundbreaking projects not just on the modeling of coastal hazards but also assessing the impacts and vulnerabilities of infrastructure and natural resources from those hazards and ultimately identifying and evaluating adaptation strategies. One of David's strengths lies in his ability to communicate technical scientific information to a variety of stakeholders, not just scientists, but agency staff, elected officials, and the general public.

I first worked with USC Sea Grant in their *AdaptLA: Sea Level Rise Planning for the City of Los Angeles*, which started in 2011. In partnership with the City of L.A., the USC Sea Grant team utilized results from CoSMoS 1.0 to assess which City of L.A. assets were vulnerable to sea level rise and to identify first steps in adaptation planning. Through both this experience and her background in biological oceanography, Juliette has a strong understanding of the science that drives both CoSMoS 1.0

and our forthcoming Southern California-focused CoSMoS 3.0. In fact, USC Sea Grant was integral in building support for and helping USGS secure funding for our Southern California regional project. USC Sea Grant will also lead the outreach of CoSMoS 3.0 model results throughout the Southern California region. This will be a tremendous benefit to the City of IB as working with USC and Juliette will link the City into the broader regional effort and provide the City with technical assistance beyond what will be offered to other coastal communities along the Southern California Coast.

Revell Coastal and USC Sea Grant are an excellent team with complementary skills. USC Sea Grant will provide a non-biased and analytical assessment of the science and a link to the regional coastal storms and sea level rise modeling effort; Revell Coastal can provide science-, professional- and experience-based recommendations on how the City of I.B. can move forward in planning for sea level rise. Both groups understand the science behind CoSMoS 3.0, the adaptation planning process and the policy implications of this planning and have considerable experience working with California communities. Please feel free to contact me with any questions you may have at pbarnard@usgs.gov or at 831-460-7556.

Sincerely,

Signature on file

Patrick L. Barnard, Ph.D.
Research Geologist

DAVID L. REVELL, Ph.D.

Principal / Chief Coastal Scientist

Dr. David Revell is a coastal geomorphologist with 15 years experience studying marine, coastal and estuarine processes, in particular in the science and management of coastal processes and climate change. He has been involved in a wide variety of contentious community stakeholder processes ranging from evaluating erosion hazard alternatives to climate change vulnerability impacts to fisheries management, water quality, and marine spatial planning. Much of his work has involved using Geographical Information Systems (GIS) to facilitate communication of science to inform decision making. He received his M.S. in 2000 from Oregon State University in Marine Resource Management with an Emphasis on Earth Science Information and Technology. He received his Ph.D. in Coastal Geology in 2007 from UC Santa Cruz with his research focusing on climate change, shoreline evolution, storm response, and coastal monitoring in Santa Barbara and Ventura Counties. He has served as an advisor on a range of topics related to ocean and coastal management especially at the intersection of how physical processes and alterations affect habitats, sensitive species, and human use. David served as a technical advisor to the Oregon Coastal Management Program on regional coastal hazards assessments and GIS. David currently advises multiple local jurisdictions on climate change, beach, dune and coastal sediment management, and lagoon processes and inlet management.

Education

Ph.D., Earth Sciences,
University of California,
Santa Cruz

M.S., Marine Resource
Management, Oregon State
University

B.A., Geography and
Environmental Studies,
University of California,
Santa Barbara

Selected Work Experience

Principal and Chief Scientist, Revell Coastal, LLC July 2014 - Present

Founded company to provide consulting services to coastal management agencies, local jurisdictions and non-profit organizations. Communicates the best available science to inform better coastal management decisions

Senior Coastal Geomorphologist, Environmental Science Associates (formerly Philip Williams & Associates), Jan. 2008 – July 2014

Managed projects and lead technical analyses on projects related to climate change, coastal lagoons, coastal restoration, sea level rise vulnerabilities, adaptation planning and coastal regional sediment management

Adjunct Professor, Monterey Institute of International Studies, Aug. 2013 to May 2014

Co-instructed graduate level courses on International Marine Science and Policy and Sustainable Coastal Management. Assist with framing the strategic planning for the Center for the Blue Economy with specific emphasis on climate change opportunities.

Project Scientist, Marine Science Institute, UC Santa Barbara – June 2009 – July 2012

Coastal research scientist collaborating on a Seagrant investigation of changes to the sandy beach ecosystems in Southern California. Responsible for physical process field data collection, evaluation of historic trends in shoreline and sand volume changes to integrate with ecological changes. Managed graduate student researcher summer 2009 and 2010.

Coastal Scientist, CoastalCOMs & Business Development, Coastal Watch USA, Jan. 2008 – May 2012

International business development of coastal monitoring systems for integrated coastal observation. Identification and development of coastal management data products. Applications of video imagery to

Relevant Experience (Continued)

nearshore processes, coastal engineering, and marine protected areas with an emphasis on integrating ocean and coastal observations. Focus on coastal processes, ports and harbors, socio-economic data collection. Supported USGS data collection efforts for projects in TRNERR, Goleta Beach, and Surfers' Point.

Postdoctoral Scholar/Research Associate – Institute of Marine Sciences, UCSC Apr. 2007 – April 2008

Researched historic shoreline change along Santa Barbara and Ventura County coasts using a variety of GIS, remote sensing and field collection techniques. Collaborated with USGS, USACE, and BEACON to assess coastal hazards and model sediment transport along the Santa Barbara coast.

Surf 2 Sea Consulting, GIS, Marine and Coastal Processes Consultant – Aug. 2002 – Dec. 2007

Sole proprietor consultant. Contracted with Ecoshore International to develop a beach and groundwater monitoring plan for a passive beach dewatering system in Hillsboro FL (2007). Subconsulted with Moffat and Nichols on Coastal Processes Section of Goleta Beach Environmental Impact Report (2006). Collaborated with PWA on historic shoreline changes to Goleta Beach County Park in Santa Barbara, and helped identify alternative solutions to park protection (2004-05). Worked for oceanfront property owners to assess coastal erosion alternatives and processes affecting property boundaries (2005). Created GIS and planning databases for the City of Bandon in Oregon (2000-03). ---Completed an inventory for the Council for Environmental Cooperation on whale watch operators and guidelines (2002). --- Coordinated the Port Orford Ocean Resources Team GIS project, a community based management effort that interviewed 33 local fishermen and recreational users regarding ocean use, harvest practices, and marine conservation. Digitized interviews into GIS and facilitated socio-economic analyses with Ecotrust (2002-03).

Ocean Wilderness Network (OWN) – Mar. 2002 - Aug. 2003

Organized Surfrider Foundation efforts to collaborate with OWN members and other coastal community activists to build a constituency in favor of marine protected areas. Authored, distributed, and presented educational information on threats and solutions in the marine environment. Coordinated with volunteers, science teachers, watershed councils, and state agencies to implement the first volunteer ocean water quality monitoring program in Oregon. Represented environmental and recreational interests on the Oregon Ocean Shore Management Planning Steering Committee.

NOAA Coastal Management Fellowship – Aug. 2000 – Aug. 2002

Received a NOAA Fellowship through an extended application process working as a technical advisor to the Oregon Coastal Management Program on littoral cell management planning. Developed coastal hazard GIS inventories for five jurisdictions - Coos, Curry, Lincoln, and Tillamook Counties and City of Bandon. Conducted a hazard assessment for the Bandon Littoral cell. Worked on the Oregon Coastal Atlas project as a member of the Project Development Team. This project collects pertinent GIS and database information for ocean areas, rocky shores, sandy shores, and estuaries, and facilitates various spatial analyses such as hazard assessment through a regional Internet Map Server.

Graduate Research Assistant – Oregon State University - July 1998 – July 2000

Constructed the Netarts Littoral Cell Coastal Hazard GIS inventory for Oregon Sea Grant, Oregon Parks and Recreation Department, Oregon Coastal Management Program, and Tillamook County. This involved survey fieldwork, data processing, map making, and project management. Conducted local stakeholder workshops to educate, facilitate and receive feedback on GIS design and hazard avoidance strategies. Recommended mitigation alternatives to State Parks regarding the Cape Lookout Dune Restoration Project - Section 227 – Army Corp of Engineers.

Selected Publications

Weaver, C.P., C. Brown, J.A. Hall, R. Lempert, **D. L. Revell**, D. Sarewitz, and J. Shukla, 2013. Climate Modeling Needs for Supporting Robust Decision Frameworks. *WIRE's Climate Change*

Revell, D.L., R. Battalio, B. Spear, P. Ruggiero, and J. Vandever, 2011. A Methodology for Predicting Future Coastal Hazards due to Sea-Level Rise on the California Coast. *Climatic Change* 109:S251-S276. DOI 10.1007/s10584-011-0315-2.

Orme, A.R., Griggs, G.B., **Revell, D.L.**, Zoulas, J.G., Chenault, C., Koo, H. 2011. Beach changes along the southern California coast during the twentieth century: A comparison of natural and human forcing factors. *Shore and Beach*

Revell, D.L., Dugan, J.E., and Hubbard, D.M. 2011. Physical and ecological responses of sandy beaches to the 1997-98 ENSO. *Journal of Coastal Research*. 27(4)718-730

Barnard, P.L., **Revell, D.L.**, Hoover, D., Warrick, J., Brocatus, J., Draut, A.E., Dartnell, P., Elias, E., Mustain, N., Hart, P.E., and Ryan, H.F., 2009, Coastal processes study of Santa Barbara and Ventura Counties, California: U.S. Geological Survey Open-File Report 2009-1029, 904 p.
<http://pubs.usgs.gov/of/2009/1029/>

Revell, D.L., Barnard, P. and Mustain, N. 2008. Influence of Harbor Construction on Downcoast Morphological Evolution: Santa Barbara, California. Published in Coastal Disasters '08 Conference, April 2008 North Shore, HI.

Dugan, J.E., Hubbard, D.M., Rodil, I., and **Revell, D.L.** 2008. Ecological Effects of Coastal Armoring on Sandy Beaches. *Marine Ecology*.

Revell, D.L., Marra, J.J., and Griggs, G.B. 2007. Sandshed Management. Special issue of Journal of Coastal Research - Proceedings from International Coastal Symposium 2007, Gold Coast, Australia.

Revell, D. L and Griggs, G.B. 2006. Beach Width and Climate Oscillations along Isla Vista, Santa Barbara, California. *Shore and Beach*. 74(3)8-16.

Revell, D.L., Komar, P.D., Sallenger, A.H. Fall 2002. *An Application of LIDAR to Analyses of El Niño Erosion in the Netarts Littoral Cell, Oregon*. *Journal of Coastal Research*, ACEC Vol. 18 4:702-801.

References

Dr. Gary Griggs, University of California, Santa Cruz
griggs@ucsc.edu 831-459-5006

Dr. Patrick Barnard, United States Geological Survey
pbarnard@usgs.gov 831-460-7556



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Alexander G. Snyder

Geomorphologist and GIS Specialist

EDUCATION

California State University, Monterey Bay, Projected Graduation December 2014

MS in Coastal and Watershed Science and Policy

University of California, Davis, December 2009

BS in Geology with a Quantitative/Geophysics Emphasis

Minor in Physics

RESEARCH AND EMPLOYMENT EXPERIENCE

Geomorphologist / GIS Specialist 7/2014-Present

United States Geological Survey

- ◇ Providing spatial analysis, GIS services, and generating map products using ArcGIS.
- ◇ Compiling and interpreting geomorphic information for use in planning and assessment.

Research Contractor (part time) 7/2014-Present

United States Geological Survey

- ◇ Documenting coastal change by generating topographic beach maps using Matlab and ArcGIS.
- ◇ Assisting in coastal surveys using GPS and development of mobile LiDAR survey techniques

Master's Thesis 9/2012-Present

California State University, Monterey Bay

- ◇ Modeling dune erosion at a small beach nourishment project in Southern Monterey Bay using wave impact theory and wave runup data.
- ◇ Performing topographic surveys using both RTK GPS and mobile terrestrial LiDAR.
- ◇ Collecting significant wave height and period data using a pressure sensor anchored offshore.
- ◇ Using Fledermaus and ArcGIS software to process and analyze data. Presented preliminary results in 2013 at AGU, H2O Headwaters to the Ocean, and MBNMS Sanctuary Currents conferences.

GIS / Lidar Technician 10/2012-7/2014

California State University, Monterey Bay - Seafloor Mapping Lab

- ◇ Performing data collection and analysis with the Seafloor Mapping Lab, which specializes in high resolution geospatial data collection and analysis of coastal environments.
- ◇ Leading topographic surveys using a mobile terrestrial laser scanner in the field, processing the data, and producing final products and analyses in ArcGIS.

Staff Geologist 8/2010-5/2012

Cardno ERI

- ◇ Conducted groundwater assessments and remediation planning and activities.
- ◇ Performed field work including well installation, borehole logging, groundwater sampling, and construction.
- ◇ Composed assessment reports, work plans and remedial action plans with a professional geologist.
- ◇ Prepared permit applications, cultivating relationships with subcontractors and regulators.

Scientific Aid

3/2010-8/2010

California Department of Fish and Game

- ◇ Generated a GIS database of potential barriers to fish passage in California.
- ◇ Documented potential barriers using GPS units in the field, then compiled information in ArcGIS.

Student Intern

7/2008-12/2009

California Energy Commission, PIER Program

- ◇ Researched potential energy related environmental impacts and prepared literature reviews.
- ◇ Organized datasets using MS Office, compiled bibliographies, and edited technical reports.

Undergraduate Researcher

7/2007-6/2008

University of California, Davis

- ◇ Modeled the flexural rigidity of the subducting plate at a convergent boundary using bathymetric and gravity data to calculate the admittance.
- ◇ Compiled and processed bathymetric and gravity data in Matlab.

References Available Upon Request.

SPECIAL SKILLS AND TRAINING

Field Methods

- ◇ Surveying using RTK GPS, Terrestrial LiDAR, Total Station and autolevel instruments.
- ◇ Monitoring water quality, measuring in-channel flow, sampling suspended sediment and bedload.
- ◇ Borehole logging and well completion, soil vapor sampling, soil sampling, and groundwater monitoring.

Computer

- ◇ Visualization, analysis, and modeling spatial data using ArcGIS, including the Digital Shoreline Analysis System.
- ◇ Familiar with R and Matlab programming languages.
- ◇ Environmental modeling using HEC-HMS/RAS, MS Excel, and R software.
- ◇ Microsoft Excel, Access, PowerPoint, and Word.

Project

- ◇ Communicating with regulators, contractors and stakeholders to achieve project goals.
- ◇ Preparing permit applications and ensuring compliance with local, regional, and state regulations.
- ◇ Writing consulting and technical reports for a variety of clients.

Recent Workshops

- ◇ Association of Environmental Professionals: Intro to the California Environmental Quality Act.
- ◇ Center for Ocean Solutions: Coastal Collaboration – Negotiation Basics and Coastal Stakeholders.

PUBLICATIONS AND PRESENTATIONS

CSUMB Class ENVS 660: Urness J, Beck E, Gehrke M, Geisler E, Goodmansen A, Leiker S, Phillips S, Rhodes J, Schat A, Snyder A, Teaby A, Wright D. 2013. **Understanding Stormwater Management Options Using a Water Balance Framework**. The Watershed Institute, California State Monterey Bay, Publication No. WI-2013-06, 50 pp.

Snyder AG, Smith DP, Kvitek RG, Latham B. 2013. **Employing LiDAR and RTK GPS to Evaluate a Small Beach Nourishment in Southern Monterey Bay**. Presented at the Headwater to the Ocean H2O Conference; 2013 May. San Diego, CA. Poster presented at the American Geophysical Union Fall Meeting; 2013 December 7-13. San Francisco, CA. Poster presented at the Monterey Bay National Marine Sanctuary Symposium: Sanctuary Currents; 2013 April 27. Seaside, CA.

JULIETTE FINZI HART

1140 HIGHLAND AVE. #206, MANHATTAN BEACH, CA 90266
310.801.3435 | JULIETTE@THALASSARESEARCH.COM

EDUCATION

- 2000 - 2007 Doctor of Philosophy in Ocean Sciences, University of Southern California, Los Angeles, CA
- 2002 - 2004 Graduate Certificate in Environmental Sciences, Policy and Engineering: Sustainable Cities, University of Southern California, Los Angeles, CA
- 1992 - 1996 Bachelor of Arts in Art History, Columbia University, New York, NY

FELLOWSHIPS

- 2006 - 2007 Fellow, John A. Knauss Marine Policy Fellowship, Washington, DC
- 2004 - 2005 Fellow, The Tyler Prize for Environmental Achievement, USC, Los Angeles, CA
- 2002 - 2004 Fellow, National Science Foundation: Sustainable Cities, USC, Los Angeles, CA

PROFESSIONAL EMPLOYMENT

- 2007 - present Assistant Professor (Research), Marine Environmental Biology Program, University of Southern California, Los Angeles, CA | Regional Research and Planning Specialist, University of Southern California Sea Grant Program, Los Angeles, CA
Lead on sea level rise adaptation planning for the City of L.A.; lead investigator on statewide coastal climate adaptation needs assessment survey; communications specialist conducting outreach of Sea Grant funded research and co-editor of USC Sea Grant's quarterly mini-journal (Urban Mariner).
- 2013 – present Founder | Principal Researcher
Thalassa Research & Consulting, LLC
Provide marine and climate science for public and private entities.
- 2006 – 2007 Assistant Professor (Research), Department of Geography, University of Southern California, Los Angeles, CA
Research on sustainable tourism; developed Integrated Island Management Plan and Visitor Management Framework for Catalina Island Conservancy.
- 2000 – 2006 Doctoral Graduate Student, University of Southern California, Marine Environmental Biology Program, Los Angeles, CA
Research on open ocean carbon and nitrogen cycling; phytoplankton production/irradiance studies; study of intracellular carbon and nitrogen cycling within phytoplankton utilizing nanoSIMS.
- 1999 - 2000 Research Assistant, Scripps Institution of Oceanography, UCSD, San Diego, CA
Research on feeding dynamics of coastal zooplankton.

- 1997 - 1999 Research Assistant, Center for Coastal Studies, Provincetown, MA, USA
Research on North Atlantic right whale feeding and habitat; zooplankton studies along eastern seaboard of U.S.
- 1996 - 1997 Art Consultant, Kate Chertavian Fine Art, London, UK
Assisted Modern British art dealer; assisted in curating gallery shows highlighting current Cornwall painters; managed art installations for David Bowie.

SELECTED PEER-REVIEWED PUBLICATIONS

Moser, S.C. and **J.A. Finzi Hart** (in review) *The Long Arm of Climate Change: Exploring Climate Change Impacts on California via Teleconnections*. Journal of Climatic Change.

Finzi-Hart, J.A., A. Subramaniam, J.A. Burns, D.G. Capone (in revision) *Photophysiological Parameters in Natural Populations of Trichodesmium in the Subtropical N. Atlantic and N. Pacific*. To be resubmitted to Aquatic Microbial Ecology.

Finzi Hart, J.A., A. Kustka, D.G. Capone (in revision) *The Effect of Fe- and P-Limitation on the Photophysiological Parameters P_{max} and α in Trichodesmium IMS101*. To be resubmitted to Aquatic Microbial Ecology.

Finzi-Hart, J.A., J. Pett-Ridge, P.K. Weber, R. Popa, S. J. Fallon, T. Gunderson, I. D. Hutcheon, K.H. Neelson, and D.G. Capone (2009) *Fixation and fate of C and N in the cyanobacterium Trichodesmium using nanometer-scale secondary ion mass spectrometry*. Proceedings of the National Academy of Sciences USA 106:6345-6350.

R. Popa, P.K. Weber, J. Pett-Ridge, **J. A. Finzi**, S. J. Fallon, I.D. Hutcheon, K.H. Neelson and D.G. Capone (2007) *Carbon and nitrogen fixation and metabolite exchange in and between individual cells of Anabaena oscillarioides*. International Society for Microbial Ecology Journal 1:354 –360.

Finzi, J.A. (2007) *Photophysiological parameters for CO₂ and N₂ fixation of Trichodesmium spp. in natural populations and in culture nutrient limitation experiments*. Ph.D. Dissertation.

SELECTED PUBLICATIONS AND REPORTS

Grifman, P., **J. Finzi Hart, J.** Ladwig, A. Newton Mann, M. Schulhof (2013) *Sea level rise vulnerability study for the City of Los Angeles*. University of Southern California Sea Grant Program. USCSG-TR-05-2013.

Finzi Hart, J.A., P. Griman, S. Moser, A. Abeles, M. Myers, S. Schlosser, J. Ekstrom (2012) *Rising to the Challenge: Results of the 2011 Climate Adaptation Needs Assessment Survey*. USC Sea Grant Technical Report, USCSG-TR-01-2012.

Finzi Hart, J.A., C. de la Rosa (2009) *Catalina Island Integrated Management Plan and Visitor Management Framework*. Catalina Island Conservancy Technical Report.

Finzi, J.A and C.A. Mayo (1999) *Preliminary description of zooplankton composition in North Atlantic right whale (Eubalaena glacialis) habitats on the Scotian Shelf*. Report to NEFSC/NMFS.

REFERENCES

Moira McEnespy, Deputy Program Manager, South Coast
California Coastal Conservancy
510-286-4165 / mmcenespy@scc.ca.gov

Shannon Parry, Sustainable Santa Monica Coordinator
City of Santa Monica, Office of Sustainability and the Environment
310-458-2227 / Shannon.Parry@smgov.net

CURRICULUM VITA
PHYLLIS M. GRIFMAN

ADDRESS

Sea Grant Program, University of Southern California
3616 Trousdale Parkway, AHF-209
Los Angeles, CA 90089-0373
Telephone: 213-740-1963
Email: grifman@usc.edu

EDUCATION

B.A., University of California, Santa Barbara. Film Studies, Literature
M.A., University of California, Santa Barbara. Political Science

PROFESSIONAL EXPERIENCE

1997-present	Associate Director, University of Southern California Sea Grant Program, USC Wrigley Institute for Environmental Studies
1993-1997	Assistant Director for Outreach, University of Southern California Sea Grant Program, USC Wrigley Institute for Environmental Studies
1988-1993	Communications Manager, University of Southern California Sea Grant Program
1983-1986	Teaching Associate, Environmental Studies Program, University of California, Santa Barbara

PROFESSIONAL ACTIVITIES

Vice Chair, Channel Islands National Marine Sanctuary Advisory Council, 2013-2015
(Public-at-Large Seat 2007-2008/Alternate 2005-2007, Secretary 2008-2013)
South Coastal Regional Stakeholder Group, California Marine Life Protection Act Initiative (September 2008-December 2009)
Board of Directors, California Shore and Beach Preservation Association, 2001-present
Chair, National Sea Grant College Program Communications Steering Committee, 1995-1997 (Past-Chair 1997-1999)
Board of Directors, Malibu Foundation for Environmental Education

SELECTED PUBLICATIONS

Grifman, P.M., J.F. Hart, J. Ladwig, A.G. Newton Mann, M. Schulhof (2013) Sea Level Rise Vulnerability Study for the City of Los Angeles, University of Southern California Sea Grant Program (USCSG-TR-05-2013).

Hart, J. A., P. M. Grifman, S. C. Moser, et al., (2012) Rising to the Challenge: Results of the 2011 California Coastal Adaptation Needs Assessment. University of Southern California Sea Grant Program (USCSG-TR-01-2012).

McCreary, Scott, P. Grifman, Evaluating the South Coast Marine Life Protection Act Initiative Process: Learning from Complexity , in prep. 2014

Stevenson, Charlotte N. (author) and P. Grifman,(editor), (2009-14) *Urban Mariner* – Sea Grant Newsletter Series, University of Southern California Sea Grant Program

Grifman, P., J. Hart, J. Risien, M. Wainstein. 2008. Developing a Sea Grant Regional Research and Information Needs Strategy for the Pacific Coast, The Coastal Society, June/July 2008

Strategic Plan 2014-2017, University of Southern California Sea Grant Program

Strategic Plan 2009-2013, University of Southern California Sea Grant Program

Strategic Plan 2003-2008, University of Southern California Sea Grant Program

Grifman, P. 2002. *Research and Outreach for the Urban Ocean, and Teaching Marine Education in Urban Settings*. University of Southern California Sea Grant Program

Grifman, P., and J. Ladwig, 1995. *Environmental Education: Making a Difference*, Coastal Zone '95, Billy L. Edge, Editor. Published by the American Society of Civil Engineers, p. 282-83.

Grifman, P. (Executive Producer), and J. Ladwig. 1994. *Environmental Education: Making a Difference*, video program produced by the University of Southern California Sea Grant Program

Grifman, P., and J.A. Fawcett (editors). 1993. *International Perspectives on Coastal Ocean Space Utilization*, Proceedings of the 2nd International Symposium on Coastal Ocean Space Utilization, April 2-4, 1991, Long Beach California. University of Southern California Sea Grant Program, 788 pp.

Grifman P., and S. Yoder (editors). 1992. *Perspectives on the Marine Environment*, Proceedings from a Symposium on the Marine Environment of Southern California, May 10, 1991, Los Angeles, California. University of Southern California Sea Grant Program, 130 pp.

Grifman, P. 1990. *Pocket Guides to Los Angeles and Orange County Beaches*, University of Southern California Sea Grant Program

Grifman, P. (editor). 1988. *The Shipping Act of 1984: A Debate of the Issues*, Proceedings of a Conference held February 18-19, 1988, Long Beach, California. University of Southern California Sea Grant Program

Cicin-Sain, B., and P. Grifman. 1982. *Management of Marine Conflicts: The Role of Third Parties*, Proceedings of the California Ocean Studies Symposium held November 1982 at Asilomar, California. California Coastal Commission

Grifman, P., B. Cicin-Sain and J. Richards. 1982. *Social Science Perspectives on Managing Conflicts Between Marine Mammals and Fisheries*, Marine Policy Program, Marine Science Institute, University of California Santa Barbara

Alyssa G. Newton Mann

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Los Angeles, CA 90089-0373
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EDUCATION

- 2008-2010 Master of Public Administration (MPA), University of Southern California, School of Policy, Planning & Development
- 2008 Executive Program in Counter-Terrorism
University of Southern California, National Center for Risk & Economic Analysis of Terrorism Events
- 2002-2006 Bachelor of Arts, International Political Economy
University of Puget Sound, Tacoma, WA

PROFESSIONAL EXPERIENCE

- 2012-present Regional Research & Planning Specialist, University of Southern California Sea Grant Program, Los Angeles, CA
- 2010-2012 International Relations Specialist, Federal Emergency Management Agency, Washington, DC
- 2011-2012 Foreign Affairs Officer, Bureau of International Organizations Affairs, U.S. Department of State, Washington, DC (7-month detail assignment)
- 2008-2010 Research Fellow, University of Southern California, National Center for Risk & Economic Analysis of Terrorism Events (CREATE), Los Angeles, CA
- 2009-2010 Business Continuity & Emergency Planning Consultant,
Clients – Montecito Bank & Trust; California Community Colleges Office of the Chancellor
- 2008-2009 Associate Government Programs Analyst, Office of the Chief of Staff, California Emergency Management Agency, Sacramento, CA
- 2006-2008 Policy Analyst, Executive Office, California Governor's Office of Homeland Security, Sacramento, CA

FELLOWSHIPS

- 2006-2007 California Governor's Executive Fellow, California Capital Fellowship Program
- 2008-2010 USC-CREATE Homeland Security Fellow, U.S. Department of Homeland Security
- 2010-2012 Presidential Management Fellow (PMF), Federal Emergency Management Agency

PUBLICATIONS

Grifman, P., J. Finzi Hart, J. Ladwig, A. Newton Mann, M. Schulhof, 2013. *Sea Level Rise Vulnerability Study for the City of Los Angeles*. University of Southern California Sea Grant Program. USCSG-TR-05-2013.

Barrett, A., Rosoff, H, Newton, A., Maya, I., 2010. *RDD Attack Risk Analysis and Countermeasure Investment Decision Analysis*. University of Southern California Homeland Security Center for Risk and Economic Analysis of Terrorism Events (CREATE). Prepared for the Federal Emergency Management Agency.

Progressive
PO Box 94903
Cleveland, OH 44101
1-800-444-4487

PROGRESSIVE[®]

Policy number: 03224993-0

Underwritten by:
Progressive Express Ins Company
July 30, 2014
Page 1 of 1

Certificate of Insurance

Certificate Holder	Insured	Agent
Additional Insured THE CITY OF GOLETA 130 CREMONA STE GOLETA, CA 93117	DAVID L REVELL REVELL COASTAL LLC 125 PEARL ST SANTA CRUZ, CA 95060	USAA INS AGCY INC 9800 FRDRCKSBRG HSWCW SAN ANTONIO, TX 78288

This document certifies that insurance policies identified below have been issued by the designated insurer to the insured named above for the period(s) indicated. This Certificate is issued for information purposes only. It confers no rights upon the certificate holder and does not change, alter, modify, or extend the coverages afforded by the policies listed below. The coverages afforded by the policies listed below are subject to all the terms, exclusions, limitations, endorsements, and conditions of these policies.

Policy Effective Date: Jul 28, 2014

Policy Expiration Date: Jan 28, 2015

Insurance coverage(s)	Limits
Bodily Injury/Property Damage	\$1,000,000 Combined Single Limit
Uninsured/Underinsured Motorist	\$1,000,000 Combined Single Limit

Description of Location/Vehicles/Special Items

Scheduled autos only

Certificate number

21114A09993

Please be advised that additional insureds and loss payees will be notified in the event of a mid-term cancellation.

Signature on file

Form 5241 (10/02)



CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)
05/22/2014

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER 1-818-539-2300 Arthur J. Gallagher & Co. Insurance Brokers of California, Inc. License #0726293 505 North Brand Boulevard, Suite 600 Glendale, CA 91203-3944 Jason Westfall	CONTACT NAME Jason Westfall PHONE (A/C, No, Ext) 818-539-1286 FAX (A/C, No) 818-539-1586 E-MAIL ADDRESS jason.westfall@ajg.com
INSURED University of Southern California 3434 S. Grand Avenue, CAL 120-G Los Angeles, CA 90089-2814	INSURER(S) AFFORDING COVERAGE INSURER A United Educators Ins RRG Inc NAIC # 10020 INSURER B ACE AMER INS CO 22667 INSURER C INSURER D INSURER E INSURER F

COVERAGES

CERTIFICATE NUMBER: 39784501

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSR	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	GENERAL LIABILITY <input checked="" type="checkbox"/> COMMERCIAL GENERAL LIAB LTY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> \$2M SIR applies GENL AGGREGATE L MIT APPL ES PER: <input checked="" type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC			GLX201400015100	05/01/14	05/01/15	EACH OCCURRENCE \$ 1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ MED EXP (Any one person) \$ N/A PERSONAL & ADV INJURY \$ GENERAL AGGREGATE \$ 2,000,000 PRODUCTS - COMP/OP AGG \$ 2,000,000
A	AUTOMOBILE LIABILITY <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> ALL OWNED AUTOS <input checked="" type="checkbox"/> HIRED AUTOS <input checked="" type="checkbox"/> \$2M SIR <input type="checkbox"/> SCHEDULED AUTOS <input checked="" type="checkbox"/> NON-OWNED AUTOS			GLX201400015100	05/01/14	05/01/15	COMBINED SINGLE LIMIT (Ea accident) \$ 1,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$
	UMBRELLA LIAB EXCESS LIAB DED RETENTION \$						EACH OCCURRENCE \$ AGGREGATE \$
B	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPR ETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCR PTION OF OPERATIONS below			WCUC47869687 (SIR: \$2M)	05/01/14	05/01/15	<input checked="" type="checkbox"/> WC STATU-TORY L MITS <input type="checkbox"/> OTH-ER E.L. EACH ACC DENT \$ 1,000,000 E.L. DISEASE - EA EMPLOYEE \$ 1,000,000 E.L. DISEASE - POLICY LIMIT \$ 1,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (Attach ACORD 101, Additional Remarks Schedule, if more space is required)

The certificate holder is included as additional insured for general liability coverage as required by virtue of a written contract or agreement and to the extent insurable as respects the operations of the named insured.

Subject to all policy terms and conditions.

CERTIFICATE HOLDER**CANCELLATION**

State of California State Coastal Conservancy 1330 Broadway, Floor 13 Oakland, CA 94612 USA	SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS. AUTHORIZED REPRESENTATIVE Signature on file
---	--

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ACORD 25 (2010/05)
jasonwest
39784501

The ACORD name and logo are registered marks of ACORD

Attachment 2



THE CITY OF IMPERIAL BEACH

**REQUEST FOR QUALIFICATIONS/ PROPOSALS FOR
PROFESSIONAL CONSULTANT SERVICES**

**TO ASSIST IN THE CITY'S
SEA LEVEL RISE VULNERABILITY ASSESSMENT AND IN
THE DEVELOPMENT OF ADAPTATION STRATEGIES**

**City of Imperial Beach
Community Development Department
825 Imperial Beach Blvd.
Imperial Beach, CA 91932
(619) 628-1355**

July, 2014



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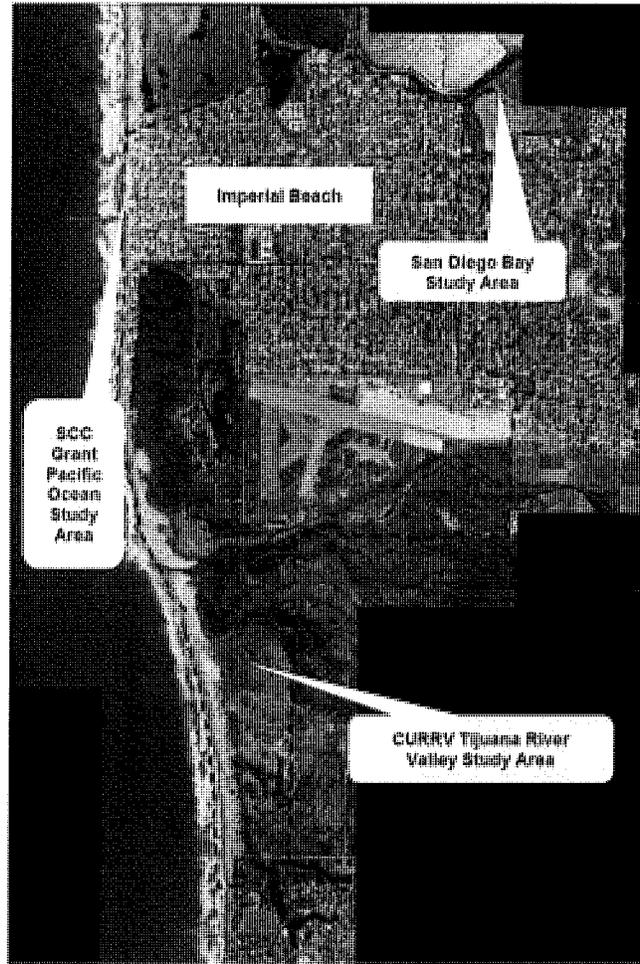
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BACKGROUND INFORMATION:

The City of Imperial Beach is a low-lying coastal community 4.5 square miles in area with a population of 26,324 (2010 US Census) located at the southwest corner of the continental United States in San Diego County. AB 32 and SB 375 have prompted both the State of California and local governments to address climate change and sea level rise impacts in its programs and projects.

The City participated in The 2012 Sea Level Rise (SLR) Adaptation Strategy for San Diego Bay that was prepared by ICLEI and funded by the San Diego Foundation. The City is also collaborating with the Tijuana River National Estuarine Research Reserve (TRNERR) who was recently awarded a three-year grant from NOAA's Climate Program Office on the *Climate Understanding and Resilience in the River Valley* (CURRV) project where a vulnerability assessment of Tijuana River Valley resources will be conducted to inform the development of adaptation strategies addressing flooding and inundation caused by sea level rise.

The Port of San Diego, who participated in the San Diego Bay SLR Adaptation Strategy, is preparing the adaptation component of its Climate Plan and Naval Base Coronado is also involved in SLR studies.



COASTAL CONSERVANCY CLIMATE READY GRANT:

The City was awarded a \$300,000 Climate Ready grant (Grant Agreement 13-090) by the State Coastal Conservancy (SCC) on January 23, 2014 to conduct a vulnerability assessment and develop adaptation strategies to address the effects that sea level rise could have on coastal resources along the Pacific Ocean shoreline of Imperial Beach. \$190,000 of the grant is dedicated to the CoSMoS 3.0 modeling work that is to be done by Dr. Patrick Barnard of the USGS in Santa Cruz. The remaining \$110,000 of the grant would fund the stakeholder engagement process, the vulnerability assessment, and the development of adaptation strategies. \$4,000 to \$9,000 or more of City staff in-kind services (which was not required by the grant) is anticipated to be expended on the project in addition to the grant funds. The CoSMoS 3.0 model is anticipated to lag during the study. The CoSMoS 1.0 model will be utilized until the CoSMoS 3.0 model becomes available in September of 2015 and, as noted in the work plan, will be folded into the refined vulnerability assessment and adaptation strategies. The following table identifies the tasks, schedule, and budget amounts that are anticipated during the course of the study.

Task	Subtask	Responsible Party Expenditure of SCC Funds			SCC Funds	Deliverables	Task Schedule
		IB staff:	TRNERR staff:	IB Consultant			
Stakeholder engagement/ workshop support/ climate change communications		\$2,000	\$2,000	2,000	\$6,000	Notices, Agendas, & General Communications Materials	July 2014 - Jan 2016
Sea-Level Rise Modeling & Mapping							
	CoSMoS 3.0 or most current model	USGS: \$190,000			\$190,000	CoSMoS 3.0 model	July 2014 - Sep 2015
	Data Gathering, Inventory existing facilities / land uses	\$5,000	\$1,000	\$3,000	\$9,000	Existing Conditions Report	July 2014 - Dec 2014
Initial Vulnerability Assessment:		\$12,000	\$4,000	\$15,000	\$31,000		Dec 2014 - May 2015
Refine Vulnerability Assessment:		\$4,000	\$2,000	\$4,000	\$10,000	Vulnerability Assessment Report	May 2015 - Aug 2015
Initial Development and Evaluation of Adaptation Strategies		\$8,000	\$4,000	\$24,000	\$36,000		June 2015 - Nov 2015
Refine Adaptation Strategies		\$5,000	\$2,000	\$8,000	\$15,000	Adaptation Strategies Report	Nov 2015 - Jan 2016
Presentation to Tidelands Advisory Committee and possibly Design Review Board		\$1,000		\$500	1,500	Draft Imperial Beach Sea Level Rise Study	Dec 2015
Presentation to City Council		\$1,000		\$500	1,500	Final Imperial Beach Sea Level Rise Study	Jan 2016
Total		\$38,000	\$15,000	\$57,000	\$ 300,000		

The City of Imperial Beach is requesting qualifications and proposals from consultant firms that provide professional services in the specialized area of climate change science and sea level rise studies focused on identifying vulnerabilities and appropriate adaptation strategies.

SCOPE OF WORK

The tasks to conduct workshops, prepare the vulnerability assessment and the development of adaptation strategies are budgeted for \$110,000. Much of the data gathering is expected to be performed by City staff. However, expertise is needed to provide guidance and oversight of these tasks by a consultant. Additionally, the consultant would be expected to provide significant assistance in the preparation of reports. It is anticipated that \$57,000 of the \$110,000 budgeted for these tasks would be set aside for the work performed by the consultant who would provide the requisite guidance and oversight. The scope of such services would include the following:

- Review/validate the background narrative that would describe the science behind the sea level rise scenarios. Typical scenarios for sea level rise studies have been projected for the years 2050 and 2100 under both "bathtub" and extreme storm event conditions (100-year flood event occurring at high tide during an El Nino season). While State SLR figures have been published for these scenario years, it is anticipated that the CoSMoS 2.0 model will include the more dynamic and more realistic factors that contribute to SLR impacts.
- Oversee the preparation of the vulnerability assessment of both the natural and built environmental systems along the oceanfront of Imperial Beach. The vulnerability assessment would analyze three components of vulnerability – exposure, sensitivity and adaptive capacity – relative to sea level rise impacts – flooding, extreme storm events, erosion, salt water intrusion, water table rise and ecosystem shifts. The systems to be analyzed may include:
 - Subtidal aquatic ecosystems
 - Transitional ecosystems
 - Upland ecosystems
 - Contaminated sites
 - Residential building stock
 - Commercial building stock
 - Parks, recreation, and public access/streets
 - Emergency response facilities
 - Potable water
 - Wastewater
 - Stormwater management
- Oversee the development of adaptation strategies for the built and natural environmental systems intended to respond to sea level rise impacts. Provide cost estimates of the adaptation strategies if they are available.
- Coordinate sea level rise data and information with the USGS, the US Navy, the CURRV project, the Port District, and other agencies that may be relevant to the Imperial Beach Sea Level Rise Study.
- Attend steering committee and public/community meetings as needed (6-8 meetings are anticipated during the 2-year study).
- Review for scientific accuracy and assist in the preparation of the final reports.
- Assist in incorporating CoSMoS 3.0 information into the refined vulnerability assessment and adaptation strategies.

APPLICABLE DOCUMENTS TO BE REVIEWED

It is advisable that the consultant be familiar with the following documents and information:

- Sea Level Rise Adaptation Strategy for San Diego Bay:
http://www.icleiusa.org/climate_and_energy/Climate_Adaptation_Guidance/san-diego-bay-sea-level-rise-adaptation-strategy-1
- Tijuana River National Estuarine Research Reserve CURRV documents:
<http://trnerr.org/currv/>

- State Sea Level Rise Guidance Document: <http://www.opc.ca.gov/2013/04/update-to-the-sea-level-rise-guidance-document/>
- Coastal Commission draft SLR Guidance Document: <http://www.coastal.ca.gov/climate/SLRguidance.html>
- State Coastal Conservancy Climate Change Policy: <http://scc.ca.gov/2009/01/21/coastal-conservancy-climate-change-policy-and-project-selection-criteria/#more-100>
- San Diego Port District Climate Plan: <http://www.portofsandiego.org/climate-mitigation-and-adaptation-plan.html>
- 2012 National Research Council: Sea-Level Rise for the Coasts of California, Oregon, and Washington: Past, Present, and Future http://www.nap.edu/catalog.php?record_id=13389
- 2014 National Climate Assessment: Climate Change Impacts in the US <http://nca2014.globalchange.gov/>
 - Imperial Beach General Plan/ Local Coastal Plan http://www.imperialbeachca.gov/index.asp?Type=B_BASIC&SEC={AE64390E-8D66-4015-AF21-5499DFFD2684}

PUBLIC DISCLOSURE

As a general rule, all documents received by the City of Imperial Beach are considered public records and will be made available for public inspection and copying upon request. If you consider any documents submitted with your response to be proprietary or otherwise confidential, please submit a written request for a determination of whether the documents can be withheld from public disclosure no later than ten (10) days prior to the due date of your proposal. If you do not obtain a determination of confidentiality prior to the submittal deadline, any document(s) submitted will be subject to public disclosure.

SUBMITTAL FORMAT AND CONTENT

All respondents are required to follow the format specified below. The contents of the submittal must be clear, concise, and complete. Each section of the submittal shall be tabbed according to the numbering system shown below to aid in expedient information retrieval.

Submittal Cover – Label the submittal as “City of Imperial Beach Sea Level Rise Study” along with the proposer’s business name, address, email address, the telephone number of the principal firm, and submittal due date.

Table of Contents - Include a listing of headings and pages to allow easy reference to key information.

- I. **Cover Letter** - The cover letter should be brief (two pages maximum) and any changes to the format or deletions of the RFQ/P should be explained in the cover letter. Identify the project manager for the study including the location of the firm's offices and the project manager's contact information. Identify any conflicts of interest that the firm and any of its sub-consultants may have with officials of the City of Imperial Beach.
- II. **Proposer's Strategic Plan** - Describe how the firm will approach and carry out the Scope of Work within the budget and schedule provided by the terms of the Climate Ready grant.

Provide a list of tasks that would be consistent with those proposed for the Climate Ready grant.

- III. **Qualifications and Experience** - This section shall contain the following: a description of the consultant's experience in providing climate change and sea level rise or other relevant consulting services for public entities and/or the private sector. The firms' experiences specifically related to the scope of work shall be listed consecutively with the awarding and completion dates noted. Each listed experience shall include the name(s) and telephone number(s) of the firm's project manager and the client's project manager for each listing. Describe the experience and the tasks that each sub-consultant will perform.
- IV. **List of Project Personnel** - This list should include the identification of the contact person with primary responsibility for this project, other project personnel, including partners and/or sub-consultants, and their individual areas of responsibility. The persons listed will be considered as committed to the project. A résumé for each professional and technical person assigned to the project, including partners and/or sub-consultants, shall be submitted. The résumés shall include at least two references from previous assignments.
- V. **Organization Chart** - An organization chart containing the names of all key personnel, joint venture partners, and sub-consultants with titles and their specific task assignment for this project shall be provided in this section.
- VI. **Insurance Coverage** - Description of insurance coverage for prime respondent and co-venture partner(s) (types of coverage and policy limits, deductible, exclusions, and outstanding claims);
- VII. **Schedule of Rates** - The respondent shall provide a Schedule of Rates in a sealed envelope along with the submittal.

SUBMITTAL SELECTION PROCESS AND EVALUATION CRITERIA

A selection committee composed of representatives of the City and other agencies and organizations that are involved with sea level rise studies may be convened to review and evaluate the proposals. The selection committee may "short-list" the most qualified firms, utilizing the selection criteria listed below. In the event that an interview is required by the selection committee, it is mandatory that all principal firms and the designated project manager attend.

Evaluation of the submittals will consider the following:

- Conformance to the specified RFQ/P format;
- Organization, presentation, and content of the submittal;
- Specialized experience and technical competence of the firm(s), (including principal firms, joint venture-partners, and sub-consultants), considering the types of service required; the complexity of the project; record of performance; and the strength of the key personnel who will be dedicated to the project;
- Proposed methods and overall strategic plan to accomplish the work in a timely and competent manner, while demonstrating the ability to be adaptive and dynamic throughout the process;
- Knowledge and understanding of the local, social, economic, and political environment;

- Reference checks, financial stability of the principal consultant and/or a consultant team;
- Demonstrated competency in the sea level rise planning process, including vulnerability assessment and adaptation strategy;
- Familiarity with the scenario planning approach;
- Skilled at communicating complex science concepts in a public presentation context; and
- Technical report writing.

The tentative schedule for the solicitation, receipt, evaluation of the submittals, and the selection of the consultant is provided as follows. (Note: Dates are subject to change.)

Distribution/Advertisement	July 1, 2014
Deadline for Submittal of Qualifications/Proposals	August 1, 2014
Submittal Review, Interview & Negotiation	To be Determined (TBD)
City Council Approval	TBD
Notice to Proceed	TBD

Submission: One original and (6) hard copies of the submittal and one PDF file of the proposal on a flash drive shall be delivered no later than 5:00 p.m. on the due date listed above to the project manager and contact person below. Copies received by FAX shall not be deemed received. Incomplete submittals, incorrect information, or late submittals shall be cause for disqualification.

Mr. James Nakagawa, AICP
 City Planner
 Community Development Department
 City of Imperial Beach
 825 Imperial Beach Blvd.
 Imperial Beach, CA 91932
 (619) 628-1355
jnakagawa@imperialbeachca.gov

Questions or inquiries regarding this RFQ/P shall be submitted via email no later than 4 business days prior to the submittal deadline to the project manager listed above.

TERMS AND CONDITIONS

Issuance of this RFQ/P does not commit the City of Imperial Beach to award a contract, to pay any costs incurred in the preparation of a response to this request, or to procure a contract for services. All respondents should note that the execution of any contract pursuant to this RFQ/P is dependent upon the approval of the City Council of the City of Imperial Beach.

The City retains the right to reject any or all submittals. Selection is also dependent upon the negotiation of a mutually acceptable contract with the successful respondent. A sample contract is attached hereto as Attachment 1. Each submittal shall be valid for not less than ninety (90) days from the date of receipt.

EQUAL OPPORTUNITY PROGRAM REQUIREMENTS

The City of Imperial Beach is committed to equal opportunity in solicitation of professional service consultants to assure that consultants doing business with, or receiving funds from, the City is equal

opportunity contractors and employers. The City encourages prime consultants to share this commitment.

The selected respondent(s) and each of its (their) Sub-consultants and/or co-venture partners, shall comply with Title VII of the Civil Rights Act of 1964, as amended, Executive Orders 11246, 11375, and 12086, the California Fair Employment Practices Act, and any other applicable federal and state laws and regulations hereinafter enacted. The respondent shall not discriminate against any employee or applicant for employment based on race, religion, color, ancestry, age, gender, sexual orientation, medical condition or place of birth.



City of Imperial Beach **AGREEMENT FOR PROFESSIONAL SERVICES**

TO ASSIST IN THE CITY OF IMPERIAL BEACH SEA LEVEL RISE VULNERABILITY ASSESSMENT AND IN THE DEVELOPMENT OF ADAPTATION STRATEGIES

This Agreement, entered into this ____ day of August, 2014, by and between the **CITY OF IMPERIAL BEACH** (hereinafter referred to as "**CITY**"), and **Sea Level Rise and Associates** (hereinafter referred to as "**CONSULTANT**") (collectively "**PARTIES**").

RECITALS

WHEREAS, CITY recognizes that on November 14, 2008, Governor Schwarzenegger issued Executive Order S-13-08 directing state agencies to plan for sea-level rise and climate impacts; and

WHEREAS, CITY recognizes that Coastal Act Section 30006.5 calls for the need to have the Coastal Commission utilize sound and timely scientific advice when considering policy and development decisions with regard to issues such as coastal erosion, marine biodiversity, wetland restoration, sea level rise, and other fields; and

WHEREAS, CITY recognizes that Coastal Act Section 30253 requires new development to minimize risks to life and property in areas of high geologic, flood, and fire hazard; and

WHEREAS, CITY was awarded a \$300,000 Climate Ready grant (Grant Agreement 13-090) by the State Coastal Conservancy (SCC) on January 23, 2014 to conduct a vulnerability assessment and develop adaptation strategies to address the effects that sea level rise could have on coastal resources along the Pacific Ocean shoreline of Imperial Beach; and

WHEREAS, CITY recognizes that the preparation of a sea level rise study requires specialized planning knowledge and skills and desires to employ a **CONSULTANT** to furnish professional services in the field of climate change and sea level rise vulnerability assessment and adaptation strategies; and

WHEREAS, **CONSULTANT** is a limited liability company and has represented that **CONSULTANT** possesses the necessary qualifications to provide such services; and

WHEREAS, CITY, on August 20, 2014, adopted Resolution No. 2014-XXXX authorizing the City Manager to enter into a professional services agreement to have **CONSULTANT** provide the services as hereinafter set forth.

NOW, THEREFORE, IT IS MUTUALLY AGREED THAT CITY DOES HEREBY RETAIN CONSULTANT ON THE FOLLOWING TERMS AND CONDITIONS:

Section 1. EMPLOYMENT OF CONSULTANT.

CITY hereby agrees to engage CONSULTANT and CONSULTANT hereby agrees to perform the services hereinafter set forth, in accordance with all terms and conditions contained herein. CONSULTANT represents that all professional services required hereunder will be performed directly by CONSULTANT, or under direct supervision of CONSULTANT.

Section 2. SCOPE OF SERVICES AND COMPENSATION.

- A. CONSULTANT shall provide services as described in Exhibit "A" entitled "Proposal For The City of Imperial Beach Sea Level Rise Study," attached hereto and made a part hereof.
- B. CONSULTANT shall coordinate its work and services with those of other consultants and agencies hired by or partnered with CITY on this study, such as the Tijuana River National Estuarine Research Reserve (TRNERR) in its Climate Understanding and Resilience in the River Valley (CURRV) study and the United States Geological Survey (USGS) in the development of the Coastal Storm Modeling System (CoSMoS) 3.0.
- C. As additional consideration, CONSULTANT and CITY agree to abide by the terms and conditions contained in this Agreement.
- D. CONSULTANT will, in a professional manner, furnish all labor and all personnel; all supplies, materials, equipment, printing, vehicles, transportation, office space, and facilities; all testing, analyses, and calculations; and all other means, except as otherwise expressly specified to be furnished by CITY, that are necessary or proper to complete the work and provide the required professional services.
- E. CONSULTANT shall be compensated for work completed, not to exceed **\$57,000.00** for basic services rendered under this Section 2, as more particularly described in Exhibit A. CONSULTANT shall be compensated for additional services only upon prior written approval of CITY.
- F. CONSULTANT shall submit monthly statements for basic and additional services rendered in accordance with this Agreement. Payments to CONSULTANT will be made by CITY within thirty (30) days of receipt of invoice. CITY agrees that the CONSULTANT's billings are correct unless CITY, within ten (10) days from the date of receipt of such billing, notifies CONSULTANT in writing of alleged inaccuracies, discrepancies, or errors in billing. In the event CITY disputes part or all of an invoice, CITY shall pay the undisputed portion of the invoice within the above mentioned thirty days.

Section 3. PROJECT COORDINATION AND SUPERVISION.

The City Planner, James Nakagawa, is hereby designated as the PROJECT COORDINATOR for CITY and will monitor the progress and execution of this Agreement.

Section 4. LENGTH OF CONTRACT.

The contract between CONSULTANT and CITY will be terminated upon completion of the work as set forth in Section 2 above or in accordance with Section 16 below.

Should CONSULTANT begin work on any phase in advance of receiving written authorization to proceed, any professional services performed by CONSULTANT in advance of the said date of authorization shall be considered as having been done at CONSULTANT'S own risk and as a volunteer unless said professional services are so authorized.

Any delay occasioned by causes beyond the control of CONSULTANT may be reason for the granting of extension of time for the completion of the aforesaid services. When such delay occurs, CONSULTANT shall immediately notify the PROJECT COORDINATOR in writing of the cause and the extent of the delay, whereupon the PROJECT COORDINATOR shall ascertain the facts and the extent of the delay and determine whether an extension of time for the completion of the professional services is justified by the circumstances.

Section 5. CHANGES.

If changes in the work seem merited by CITY or CONSULTANT, and informal consultations with the other party indicate that a change is warranted, it shall be processed by CITY in the following manner: a letter outlining the changes shall be forwarded to CITY by CONSULTANT with a statement of estimated changes in fee or time schedule. An amendment to the Agreement shall be prepared by CITY and executed by both parties before performance of such services or CITY will not be required to pay for the changes in the scope of work. Such amendment shall not render ineffective or invalidate unaffected portions of this Agreement.

Section 6. OWNERSHIP OF DOCUMENTS.

All documents, data, studies, drawings, maps, models, photographs and reports prepared by CONSULTANT under this Agreement shall be considered the property of CITY. CONSULTANT may retain such copies of said documents and materials as desired but shall deliver all original materials to CITY.

Section 7. AUDIT OF RECORDS.

7.1. At any time during normal business hours and as often as may be deemed necessary, the CONSULTANT shall make available to a representative of CITY for examination all of its records with respect to all matters covered by this Agreement and shall permit CITY to audit, examine and/or reproduce such records. CONSULTANT shall retain such financial and program service records for at least four (4) years after termination or final payment under this Agreement.

7.2. The CONSULTANT shall include the CITY'S right under this section in any and all of their subcontracts, and shall ensure that these sections are binding upon all subcontractors.

Section 8. PUBLICATION OF DOCUMENTS.

Except as necessary for performance of service under this Agreement, no copies, sketches, or graphs of materials, including graphic art work, prepared pursuant to this Agreement shall be released by CONSULTANT to any other person or agency without CITY'S prior written approval. All press releases, including graphic display information to be published in newspapers or magazines, shall be approved and distributed solely by CITY, unless otherwise provided by written agreement between the parties. After project completion, CONSULTANT may list the project and the general details in its promotional materials.

Section 9. COVENANT AGAINST CONTINGENT FEES.

CONSULTANT declares that it has not employed or retained any company or person, other than a bona fide employee working for CONSULTANT, to solicit or secure this Agreement, that it has not paid or agreed to pay any company or person, other than a bona fide employee, any fee, commission, percentage, brokerage fee, gift or any other consideration, contingent upon or resulting from the award or making of the Agreement. For breach of violation of this warranty, CITY shall have the right to annul this Agreement without liability, or, at its sole discretion, to deduct from the Agreement price or consideration, or otherwise recover the full amount of such fee, commission, percentage, brokerage fee, gift or contingent fee.

Section 10. NO ASSIGNMENTS.

Neither any part nor all of this Agreement may be assigned or subcontracted, except as otherwise specifically provided herein, or to which CITY, in its sole discretion, consents to in advance thereof in writing. Any assignment or subcontracting in violation of this provision shall be void.

Section 11. INDEPENDENT CONTRACTOR.

At all times during the term of this Agreement, CONSULTANT and any subcontractors employed by CONSULTANT shall be an independent contractor and shall not be an employee of the CITY. CITY shall have the right to control CONSULTANT only insofar as the results of CONSULTANT'S services rendered pursuant to this Agreement. However, CITY shall not have the right to control the means by which CONSULTANT accomplishes its services. Any provision in this Agreement that may appear to give CITY the right to direct CONSULTANT or sub consultant as to the details of doing the work or to exercise a measure of control over the work means that CONSULTANT shall follow the direction of the CITY as to end results of the work only.

Neither CONSULTANT nor CONSULTANT's employees shall in any event be entitled to any benefits to which CITY employees are entitled, including, but not limited to, overtime, any retirement benefits, workers' compensation benefits, any injury leave or other leave benefits, CONSULTANT being solely responsible for all such matters, as well as compliance with social security and income tax withholding and all other regulations and laws governing such matters.

Section 12. LICENSES, PERMITS, ETC.

CONSULTANT represents and declares to CITY that it has all licenses, permits, qualifications, and approvals of whatever nature that is legally required to practice its profession. CONSULTANT represents and warrants to CITY that CONSULTANT shall, at its sole cost and expense, keep in effect at all times during the term of this Agreement, any license, permit, or approval which is legally required for CONSULTANT to practice its profession.

Section 13. INSURANCE.

CONSULTANT shall maintain, during the term of this Agreement, Workers' Compensation and Employer's Liability Insurance as prescribed by applicable law. Upon request, CITY shall be provided with satisfactory evidence that premiums have been paid and shall deliver to CITY certificates of insurance and endorsements as to each policy. Each certificate of insurance shall provide that the policy will not be materially altered or cancelled without first giving 10 days written notice to the CITY by certified mail. Coverage shall include appropriate waivers of subrogation as

to the CITY. CONSULTANT agrees to this requirement irrespective of any other similar obligation imposed on others and CONSULTANT agrees to do so in conformity with the requirements set forth herein including those requirements set forth for certificates of insurance.

CONSULTANT shall assume liability for the wrongful or negligent acts, errors and omissions of its officers, agents and employees and subcontractors in regard to any functions or activity carried out by them on behalf of CITY pursuant to the terms of this Agreement.

Section 14. CONSULTANT NOT AN AGENT.

Except as CITY may specify in writing, CONSULTANT shall have no authority, expressed or implied, to act on behalf of CITY in any capacity whatsoever as an agent. CONSULTANT shall have no authority, expressed or implied, pursuant to this Agreement to bind CITY to any obligation whatsoever.

Section 15. INDEMNITY.

To the fullest extent permitted by law, CONSULTANT shall indemnify, defend, and hold harmless the CITY and their respective officers, officials, agents and employees from any and all claims, demands, costs or liability that arise out of, pertain to, or relate to the negligence, recklessness, or willful misconduct of CONSULTANT, its employees, agents, and subcontractors in the performance of services under this AGREEMENT. CONSULTANT's duty to indemnify under this section shall not include liability for damages for death or bodily injury to persons, injury to property, or other loss, damage or expense arising from the active or sole negligence or willful misconduct by the CITY or its respective elected officials, officers, agents, and employees. CONSULTANT's indemnification obligations shall not be limited by the insurance provisions of this AGREEMENT. The PARTIES expressly agree that any payment, attorney's fees, costs or expense CITY incurs or makes to or on behalf of an injured employee under the CITY's self-administered workers' compensation is included as a loss, expense, or cost for the purposes of this section, and that this section will survive the expiration or early termination of this AGREEMENT.

Section 16. TERMINATION.

CITY may terminate this Agreement at any time by giving ten (10) days' written notice to CONSULTANT of such termination and specifying the effective date thereof at least ten (10) days before the effective date of such termination. In that event, all finished or unfinished documents, data, studies, surveys, drawings, maps, reports and other materials prepared by CONSULTANT shall, at the option of CITY, become the property of CITY. If this Agreement is terminated by CITY as provided herein, CONSULTANT will be paid an amount which bears the same ratio to the total compensation as the services actually performed would bear to the total services of CONSULTANT covered by this Agreement, less payments of compensation previously made.

Should CONSULTANT be in default of any covenant or condition hereof, CITY may immediately terminate this AGREEMENT for cause if CONSULTANT fails to cure the default within ten (10) calendar days of receiving written notice of the default.

Section 17. NON-DISCRIMINATION.

CONSULTANT shall not discriminate against any employee or applicant for employment because of race, color, religion, sex or national origin, nor shall CONSULTANT discriminate against any qualified individual with a disability. CONSULTANT will take affirmative action to insure that

applicants are employed and that employees are treated during employment without regard to their race, color, religion, sex or national origin and shall make reasonable accommodation to qualified individuals with disabilities. Such action shall include, but not be limited to the following: employment, upgrading, demotion, transfer, recruitment, or recruitment advertising, layoff or termination, rates of pay or other forms of compensation, and selection for training, including apprenticeship. CONSULTANT agrees to post in conspicuous places available to employees and applicants for employment any notices provided by CITY setting forth the provisions of this non-discrimination clause.

Section 18. GENERAL CONDITIONS.

CONSULTANT shall provide no services for any private client within the corporate boundaries of CITY during the period that this Agreement is in effect, nor shall CONSULTANT, without, previous written permission from the PROJECT COORDINATOR, review any plan, map or other work which to the best of CONSULTANTS knowledge has been submitted by a private client for which the CONSULTANT has performed work within the previous 12 months or anticipates performing work in the succeeding 12 months. CONSULTANT shall immediately notify the PROJECT COORDINATOR in writing whenever CONSULTANT has reason to believe that aforementioned circumstance exists. CONSULTANT knows of no interests where it holds nor of any relationship it has or may have that would constitute a conflict of CONSULTANT performing the duties set forth in this Agreement solely in the best interest of CITY.

Section 19. OFFICE SPACE AND CLERICAL SUPPORT.

Consultant shall provide its own office space and clerical support at its sole cost and expense.

Section 20. SUBCONTRACTORS.

20.1. The CONSULTANT's hiring or retaining of third parties (i.e. subcontractors) to perform services related to this Agreement is subject to prior approval by the CITY.

20.2. All contracts entered into between the CONSULTANT and its subcontractor shall also provide that each subcontractor shall obtain insurance policies which shall be kept in full force and effect during any and all work related to this Agreement and for the duration of this Agreement. The CONSULTANT shall require the subcontractor to obtain all policies described in Section 13 above in the amounts required by the CITY/AGENCY, which shall not be greater than the amounts required of the CONSULTANT.

20.3. In any dispute between the CONSULTANT and its subcontractor, the CITY shall not be made a party to any judicial or administrative proceeding to resolve the dispute. The CONSULTANT agrees to defend and indemnify the CITY as described in Section 15 of this Agreement should the CITY be made a party to any judicial or administrative proceeding to resolve any such dispute.

Section 21. CONFIDENTIAL RELATIONSHIP.

CITY may from time to time communicate to CONSULTANT certain information to enable Consultant to effectively perform the services. CONSULTANT shall treat all such information as confidential, whether or not so identified, and shall not disclose any part thereof without the prior written consent of CITY. CONSULTANT shall limit the use and circulation of such information, even

within its own organization, to the extent necessary to perform the services. The foregoing obligation of this Section 21, however, shall not apply to any part of the information that (i) has been disclosed in publicly available sources of information (ii) is, through no fault of CONSULTANT, hereafter disclosed in publicly available sources of information; (iii) is now in the possession of CONSULTANT without any obligation of confidentiality; or (iv) has been or is hereafter rightfully disclosed to CONSULTANT by a third party, but only to the extent that the use or disclosure thereof has been or is rightfully authorized by that third party.

CONSULTANT shall not disclose any reports, recommendations, conclusions or other results of the services or the existence of the subject matter of this contract without the prior written consent of CITY. In its performance hereunder, CONSULTANT shall comply with all legal obligations it may now or hereafter have respecting the information or other property of any other person, firm or corporation.

Section 22. MEDIATION.

In the event of a dispute between CITY and CONSULTANT concerning the terms of this Agreement or its performance, the parties may, but are not required to, agree to submit such dispute to mediation. If the Parties agree to mediation, CITY and CONSULTANT agree to cooperate in good faith to promptly select a mediator, to schedule a mediation session, and to attempt to settle the claim or dispute through mediation.

Section 23. NOTICES.

All communications to either party by the other party shall be deemed made when received by such party at its respective name and address, as follows:

<u>CITY</u>	<u>CONSULTANT</u>
James Nakagawa	XXX
City Planner	Principal
City of Imperial Beach	Sea Level Rise and Associates LLC
825 Imperial Beach Blvd.	123 Climate Change Avenue, Suite 212
Imperial Beach CA 91932	Imperial Beach, CA 91932

Any such written communications by mail shall be conclusively deemed to have been received by the addressee five days after the deposit thereof in the United States Mail, postage prepaid and properly addressed as noted above.

Section 24. CALIFORNIA LAW; VENUE.

This Agreement and its performance shall be governed, interpreted, construed, and regulated by the laws of the State of California. Any action brought to enforce or interpret any portion of this Agreement shall be brought in the County of San Diego, California. CONSULTANT hereby waives any and all rights it might have pursuant to California Code of Civil Procedure § 394.

Section 25. ENTIRE AGREEMENT.

This Agreement, and its Attachments and Exhibits, set forth the entire understanding of the parties. There are no other understandings, terms or other agreements expressed or implied, oral or written. The following attachments are a part of this Agreement: **Proposal dated August 1, 2014.** No change, alteration, or modification of the terms or conditions of this Agreement, and no

verbal understanding of the PARTIES, their officers, agents, or employees shall be valid unless agreed to in writing by both PARTIES.

Section 26. SEVERABILITY.

If any portion of this Agreement is declared by a court of competent jurisdiction to be invalid or unenforceable, then such portion shall be deemed modified to the extent necessary in the opinion of the court to render such portion enforceable and, as so modified, such portion and the balance of this Agreement shall continue in full force and effect.

Section 27. TIME IS OF ESSENCE.

Time is of the essence for each and every provision of this agreement that states a time for performance and for every deadline imposed by the PROJECT COORDINATOR.

Section 28. COMPLIANCE WITH LAW.

CONSULTANT shall comply with applicable laws in effect at the time the services are performed hereunder which, to the best of its knowledge, information and belief, apply to its obligations under this Agreement.

Section 29. STATEMENT OF EXPERIENCE.

By executing this Agreement, CONSULTANT represents that it has demonstrated trustworthiness and possesses the quality, fitness, and capacity to perform the Agreement in a manner satisfactory to CITY. CONSULTANT represents that its financial resources, surety and insurance experience, service experience, completion ability, personnel, current workload, experience in dealing with private owners, and experience in dealing with public agencies all suggest that CONSULTANT is capable of performing the proposed contract and has a demonstrated capacity to deal fairly and effectively with and to satisfy a public agency.

Section 30. CONFLICTS OF INTEREST AND POLITICAL REFORM ACT OBLIGATIONS.

During the term of this Agreement CONSULTANT shall not act as consultant or perform services of any kind for any person or entity whose interests conflict in any way with those of the CITY. CONSULTANT shall at all times comply with the terms of the Political Reform Act and the local conflict of interest ordinance. CONSULTANT shall immediately disqualify itself and shall not use its official position to influence in any way any matter coming before the CITY in which the CONSULTANT has a financial interest as defined in Government Code Section 87103. CONSULTANT represents that it has no knowledge of any financial interests which would require it to disqualify itself from any matter on which it might perform services for the Agency.

CONSULTANT shall comply with all of the reporting requirements of the Political Reform Act and local ordinance. Specifically, CONSULTANT shall file Statements of Economic Interest with the City Clerk of the CITY in a timely manner on forms which CONSULTANT shall obtain from the City Clerk.

Section 31. RESPONSIBILITY FOR EQUIPMENT.

CITY shall not be responsible nor held liable for any damage to persons or property consequent upon the use, misuse, or failure of any equipment used by CONSULTANT or any of CONSULTANT's employees or subcontractors, even if such equipment has been furnished, rented, or loaned to CONSULTANT by CITY. The acceptance or use of any such equipment by CONSULTANT, CONSULTANT's employees, or subcontractors shall be construed to mean that CONSULTANT accepts full responsibility for and agrees to exonerate, indemnify and hold harmless CITY from and against any and all claims for any damage whatsoever resulting from the use, misuse, or failure of such equipment.

Section 32. NO WAIVER.

No failure of either the CITY or the CONSULTANT to insist upon the strict performance by the other of any covenant, term or condition of this Agreement, nor any failure to exercise any right or remedy consequent upon a breach of any covenant, term, or condition of this Agreement shall constitute a waiver of any such breach of such covenant, term or condition.

Section 33. DRAFTING AMBIGUITIES.

The PARTIES agree that they are aware that they have the right to be advised by counsel with respect to the negotiations, terms and conditions of this Agreement, and the decision of whether or not to seek advice of counsel with respect to this Agreement is a decision which is the sole responsibility of each Party. This Agreement shall not be construed in favor of or against either Party by reason of the extent to which each Party participated in the drafting of the Agreement.

Section 34. CONFLICTS BETWEEN TERMS.

If an apparent conflict or inconsistency exists between the main body of this Agreement and the Exhibits, the main body of this Agreement shall control. If a conflict exists between an applicable federal, state, or local law, rule, regulation, order, or code and this Agreement, the law, rule, regulation, order, or code shall control. Varying degrees of stringency among the main body of this Agreement, the Exhibits, and laws, rules, regulations, orders, or codes are not deemed conflicts, and the most stringent requirement shall control. Each Party shall notify the other immediately upon the identification of any apparent conflict or inconsistency concerning this Agreement.

Section 35. EXHIBITS INCORPORATED.

Exhibit "A" is incorporated into the Agreement by this reference.

Section 36. SIGNING AUTHORITY.

The representative for each Party signing on behalf of a corporation, partnership, joint venture or governmental entity hereby declares that authority has been obtained to sign on behalf of the corporation, partnership, joint venture, or entity and agrees to hold the other Party or PARTIES hereto harmless if it is later determined that such authority does not exist.

IN WITNESS WHEREOF the parties hereto have executed this contract the day and year first hereinabove written.

CITY OF IMPERIAL BEACH:
A municipal corporation

CONSULTANT:
Sea Level Rise and Associates LLC

Andy Hall, City Manager

XXX /Principal

APPROVED AS TO FORM:

APPROVED AS TO CONTENT:

Jennifer M. Lyon, City Attorney

Greg Wade, Assistant City Manager/
Community Development Director

ACKNOWLEDGMENT

State of California)
County of San Diego)

On _____ before me, _____, a Notary Public,
personally appeared _____, who proved to me on the basis of satisfactory
evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to
me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their
signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed
the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing
paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____ (Seal)

State of California)
County of San Diego)

On _____ before me, _____, a Notary Public,
personally appeared _____, who proved to me on the basis of satisfactory
evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to
me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their
signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed
the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing
paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____ (Seal)

State of California)
County of San Diego)

On _____ before me, _____, a Notary Public,
personally appeared _____, who proved to me on the basis of satisfactory
evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to
me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their
signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed
the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing
paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____ (Seal)

EXHIBIT "A"

[Proposal and Scope of Services]



AGENDA ITEM NO. 2.4

STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *AH*
MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: PUBLIC WORKS *AH*
SUBJECT: RESOLUTION NO. 2014-7513 AWARDED A PUBLIC WORKS CONTRACT; TO WIT: ANNUAL MAIN LINE AND MANHOLE REPAIRS FY 11/12 CIP # W12-201

EXECUTIVE SUMMARY:

This resolution will award a contract for a sewer Annual Main Line and Manhole repairs project on selected sewer pipes and manholes within the City of Imperial Beach that have deteriorated infrastructure. Those sewer lines are pipe sections 251, 151, 231, 463, 896, 740, 700, 669, 650, & 2046 and manhole 691 as shown in attachment 2.

BACKGROUND:

The Five-Year Capital Improvement Program (CIP) Projects Budget for Fiscal Years 2009/2010 through 2013/2014 included the Annual Main Line and Manhole W12-201 repairs project funded through Sewer Enterprise fund.

On May 16, 2012, Council authorized the reordering of the "Annual Main Line Repairs" CIP projects by replacing the original project list with the "red flag" and "high priority" mainline defects identified during a more recent CCTV (sewer mainline televising) project. This Annual Main Line and Manhole Repairs CIP project - W12-201 - is the work identified as red flag and high priority.

October 16, 2013, resolution 2013-7407 approved the first phase of a two-phased list of projects for fiscal year 2013/14 Capital Improvement Program and appropriated the funds for the ongoing Annual Main Line and Manhole W12-201 project.

The project drawings and specifications were completed in July 2014. Staff advertised for requests for bids (RFB) for this work on July 31, 2014 in the Eagle & Times Newspaper and on E-Bid Board. The bid opening was scheduled for Thursday, August 21, 2014.

ANALYSIS:

The project bids were opened and evaluated Thursday, August 21, 2014 in an advertised public meeting at 2:00 p.m. The lowest responsive and qualified bidder for the Annual Main Line and Manhole Repairs FY 11/12 Project W12-201 was Just Construction at a bid price of \$98,015.50.

The three (3) contractors who submitted proposals are listed below along with their proposal amounts:

1. Just Construction	\$ 98,015.50
2. Transtar Pipeline, Inc.	\$106,500.00
3. Piperin Corp.	\$118,120.00

The engineer's estimate for this construction was \$101,480.

ENVIRONMENTAL DETERMINATION:

Project is exempt from CEQA pursuant to CEQA Guidelines Section 15302(c): Replace or Reconstruction of Existing Utility Systems and Facilities.

FISCAL IMPACT:

Revenue (per resolution 2013-7407)	
Total Sewer Enterprise Fund	\$210,360
Expenditures/Encumbrances	
Project Design & Construction Support	\$ 54,190.00
Soils Technical Support	\$ 17,290.00
Contract Administration	\$ 9,802.00
Project Construction	\$ 98,015.50
TOTAL EXPENDITURE	\$179,297.50

There are sufficient appropriated funds to cover the cost of this project construction. All remaining funds will be returned to the Sewer Enterprise Fund balance to be used in a subsequent sewer improvement project.

RECOMMENDATION:

1. Receive this report.
2. Adopt the attached resolution awarding a contract to the lowest responsive bidder.
3. Authorize the City Manager to sign the construction contract with the lowest responsive bidder.
4. Authorize the City Manager to approve a purchase order for the amount of the bid price.

Attachments:

1. Resolution No. 2014-7513
2. Sewer Repair Locations Map

RESOLUTION NO. 2014-7513

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH, CALIFORNIA, AWARDED A PUBLIC WORKS CONTRACT; TO WIT: ANNUAL MAIN LINE AND MANHOLES REPAIRS FY 11/12 W12-201

WHEREAS, the Five-Year Capital Improvement Program (CIP) Projects Budget for Fiscal Years 2009-2010 through 2013-2014 included annual Sewer Main Line and Manholes Repairs FY 11/12 funded through the Sewer Enterprise budget; and

WHEREAS, Council Resolution No. 2013-7407 (Approving the First of a Two Phased List of projects for Fiscal Year 2013/14 Capital Improvement Program and Appropriating Funds for these Projects) adopted October 16, 2013 reaffirmed the funding for Annual Mainline and Manholes Repairs FY 2011/12 W12-201 and gave staff direction to proceed with the work; and

WHEREAS, the construction project included repairs to sewer pipe sections 251, 151, 231, 463, 896, 740, 700, 669, 650, & 2046 and manhole 691; and

WHEREAS, staff advertised for requests for bids (RFB) for this work on July 31, 2014 in the Eagle & Times Newspaper and on Construction Bidboard; and

WHEREAS, the project bids were to be opened and evaluated Thursday, August 21, 2014 in an advertised public meeting at 2:00 p.m.; and

WHEREAS, the lowest responsive and qualified bidder for the Sewer Main Line and Manholes Repairs FY 11/12 Project W12-201 was Just Construction at a bid price of \$98,015.50; and

WHEREAS, the total revenue appropriated for this project is \$210,360; and

WHEREAS, the engineer's estimate for this construction was \$ 101,480; and

WHEREAS, there is sufficient appropriated revenue (Council Resolution No. 2013-7407) to cover the cost of this project construction;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Imperial Beach as follows:

1. The above recitals are true and correct.
2. The legislative body hereby rejects all proposals for bids except that identified as the lowest responsible bid. The bid of the lowest, responsible qualified bidder will be on file with the transcript of these proceedings and open for public inspection in the City Clerk Department on file as Contract No. _____.
3. The contractor shall not commence construction or order equipment until he/she has received a Notice to Proceed.
4. The works of improvement shall be constructed in the manner and form and in compliance with the requirements as set forth in the plans and specifications for the project.
5. The City Manager is authorized to sign a purchase order with the lowest responsible qualified bidder.

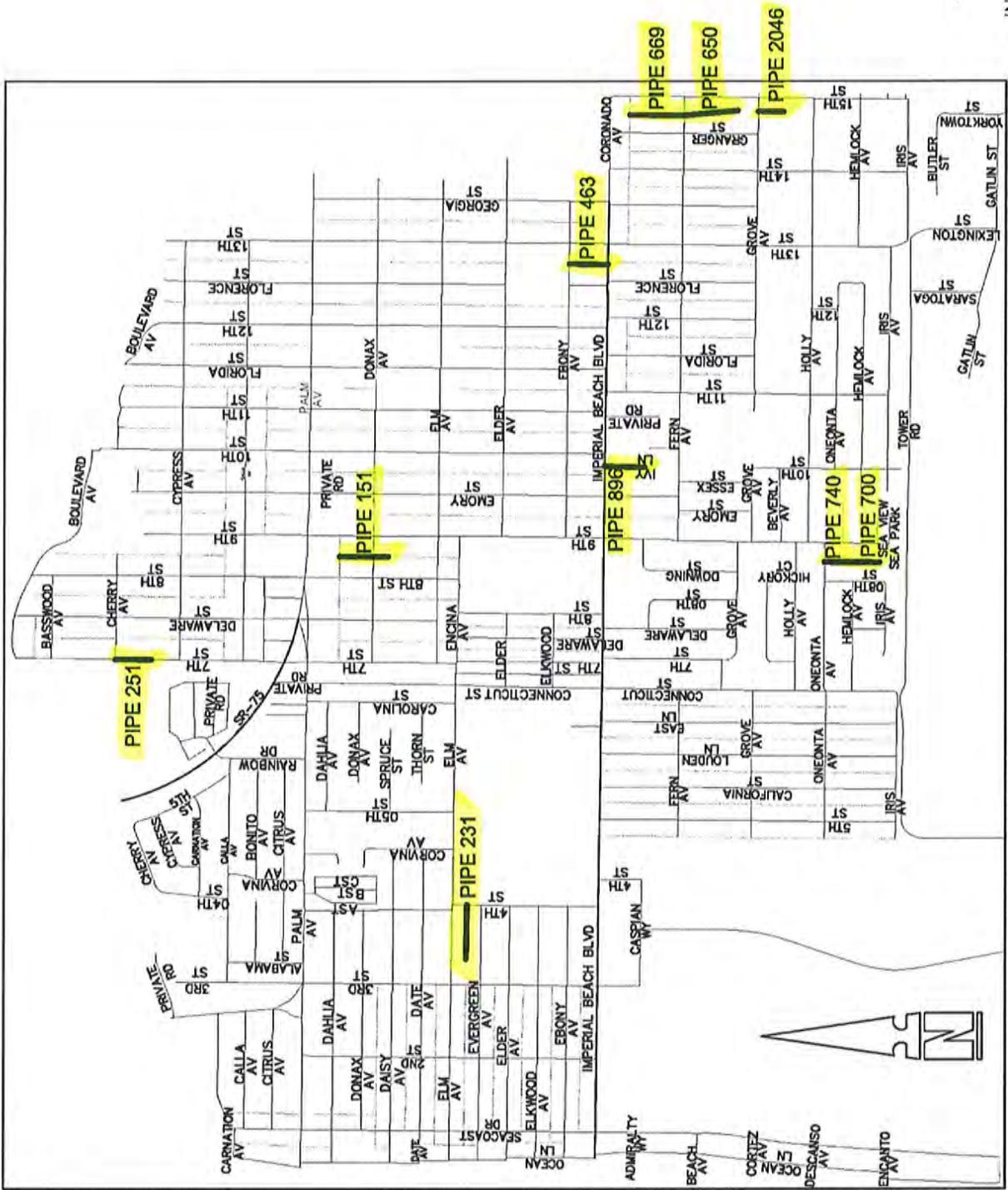
PASSED, APPROVED, AND ADOPTED by the City Council of the City of Imperial Beach at its meeting held on the 17th day of September 2014, by the following vote:

AYES: COUNCILMEMBERS:
NOES: COUNCILMEMBERS:
ABSENT: COUNCILMEMBERS:

JAMES C. JANNEY, MAYOR

ATTEST:

JACQUELINE M. HALD, MMC
CITY CLERK



IND

LOCATION MAP

NO SCALE



AGENDA ITEM NO. 2.5

STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *AH*
MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: PUBLIC WORKS DEPARTMENT *HH*
SUBJECT: MAYOR PROCLAMATION FOR TIJUANA RIVER ACTION MONTH FOR OCTOBER

EXECUTIVE SUMMARY:

This proclamation from the Mayor recognizes the important efforts made by multiple agencies and community groups in the Tijuana River Watershed to help raise public awareness and involvement in cleanup, restoration, and stewardship activities as part of the 5th annual Tijuana River Action Month.

BACKGROUND:

2015 marks the 5th Annual Tijuana River Action Month (TRAM) which involves a series of bi-national stewardship, public education, and cleanup activities during the months of September and October to benefit the Tijuana River Watershed. This time period is critical since there is a very small window of opportunity between the end of bird nesting season and the start of the rainy season to enter the river valley and remove as much trash as possible. Tijuana River Action Month is the compilation of efforts by multiple agencies and community groups over the month of September and October to raise public awareness and involvement to cleanup and restore the Tijuana River Valley.

Tijuana River Action Month is coordinated each year through the Tijuana River Action Network, which consists of representatives from local NGOs and agencies in the watershed to help plan, coordinate, and implement activities that will engage the public through conservation and restoration of the Tijuana River. The efforts by the Tijuana River Action Network are important because it brings together the existing work in the watershed from multiple agencies and NGOs under a common framework that can most effectively provide advocacy for the cleanup and restoration of the watershed. The Mayor of Imperial Beach has provided a supporting proclamation each year for Tijuana River Action Month since the start of the event in 2010.

ANALYSIS:

The City is actively involved on cleanup and restoration efforts in the Tijuana River Valley and this proclamation is one additional way for the City to raise awareness on the issues in the Tijuana River.

The following is a list of activities and cleanups that are planned for the months of September and October:

- | | |
|----------------------------|--|
| September 20 th | <ul style="list-style-type: none">• Coastal Cleanup Day
Border Field State Park and Dairy Mart Road Bridge (9AM-12PM)• Salvemos La Playa Playas
Playas de Tijuana (8AM-12PM)• Native Species Planting
Monument Mesa in Border Field State Park (11AM-1PM) |
| September 26 th | <ul style="list-style-type: none">• Composting Workshop and Waste Collection Event
Tijuana (12PM-4PM) |
| September 27 th | <ul style="list-style-type: none">• National Public Lands Day
Native Plant Restoration Border Field State Park (9AM-12PM) |
| October 4 th | <ul style="list-style-type: none">• Effie May Trail Cleanup
Tijuana River Valley (9AM-12PM) |
| October 11 th | <ul style="list-style-type: none">• Goat Canyon Cleanup Event
Border Field State Park (9AM-12PM)• Tijuana Rio Conecta Exhibition Butterfly Release
Playas de Tijuana Lighthouse (All Day)• Border Field Gateway Ceremony and Volunteer Appreciation Party
Monument Mesa Friendship Park (12PM-3PM) |

ENVIRONMENTAL DETERMINATION:

Not a project as defined by CEQA.

FISCAL IMPACT:

No impact to budget. This is part of a regular program of activities within the Environmental Division Budget

RECOMMENDATION:

1. Mayor to provide a proclamation for Tijuana River Action Month

Attachments:

1. Tijuana River Action Month Proclamation



PROCLAMATION

October is Tijuana River Action Month

WHEREAS, *the City is dedicated to improving the water quality and environmental habitat of the bi-national Tijuana River watershed; and*

WHEREAS, *the City is dedicated to improving the water quality and environmental habitat of the bi-national Tijuana River watershed; and*

WHEREAS, *the Tijuana River watershed provides important ecological and recreational resources to South San Diego County; and*

WHEREAS, *the health and well-being of the South Bay community is directly connected to the health of The Tijuana River watershed; and*

WHEREAS, *the month of October recognizes key efforts and investments by public and private agencies, nonprofits, and community groups to protect and restore the Tijuana River.*

NOW, THEREFORE, BE IT RESOLVED, *that I, James C. Janney, Mayor of the City of Imperial Beach, California, together with the City Council, do hereby recognize the month of October as Tijuana River Action Month.*

Dated: September 17, 2014

James C. Janney, Mayor

Attest:

Jacqueline M. Hald, MMC, City Clerk



STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *AH*
MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: PUBLIC WORKS *AH*
SUBJECT: RESOLUTION NO. 2014-7515 APPROPRIATING AN
ADDITIONAL \$33,000 SEWER ENTERPRISE CIP FUNDS TO
THE SEWER SYSTEM CIP PROJECT TO WIT – TELEWISE
SEWER MAINLINES PROJECT NO. W15-202

EXECUTIVE SUMMARY:

On February 5, 2014, City Council adopted Resolution No. 2014-7451, approving the second of a two phased list of projects for Fiscal Years 2013/14 and 2014/15 Capital Improvement Program and appropriated the funds for these projects. One of the projects on this list was W15-202 – Telewise the remaining 142,000 linear feet of Sewer Mains in the City. The City Council appropriated \$160,000 for the completion of the televising work. Tran Consulting Engineers (City's contract Sewer Engineer) was tasked to submit cost estimate for this work plus the cost to prepare an assessment report following the CCTV. The fee total was \$193,000. Staff is proposing City Council appropriate additional funds in the amount of \$33,000 to allow this project to be completed.

BACKGROUND:

On February 5, 2014, City Council adopted Resolution No. 2014-7451, approving the second of a two phased list of projects for the Fiscal Years 2013/2014 and 2014/15 Capital Improvement Program and appropriated the funds for these projects. One of the projects on this list, W15-202 – Telewise Sewer Mainlines - was approved and was appropriated \$160,000 from the Sewer Enterprise CIP Fund to complete the work.

ANALYSIS:

A subsequent and more detailed analysis of sewer mainlines found that 148,462 linear feet of mains remained to be CCTV'ed. Additionally, for the CCTV to be actionable, the videos needed to be analyzed thus an assessment of the repairs needed to be prepared. Staff directed Tran Consulting Engineers - City's Sewer Engineer consultant - to provide the City an estimate to perform the CCTV and to prepare an Assessment Report. The scope was based on 148,462 linear feet of sewer lines at a cost of \$1.30 per linear foot. The total fee amount is \$193,000, thus leaving the project underfunded by \$33,000. It is suggested that an additional \$33,000 be appropriated in order to complete the project with the additional work of preparing the Assessment Report.

ENVIRONMENTAL DETERMINATION:

Not a project as defined by CEQA.

FISCAL IMPACT:

Revenue:

Appropriated Sewer Enterprise Fund	\$160,000
Proposed additional Sewer Enterprise Fund CIP Reserve	\$ 33,000
TOTAL REVENUE	\$193,000

Expenses:

Consultant Contract	\$193,000
TOTAL EXPENSES	\$193,000

The Sewer Enterprise Fund CIP Reserve has approximately \$1,765,721 through FY 2014/15, in addition to the \$2 million minimum emergency reserve. There are sufficient Sewer Enterprise CIP reserves for this additional appropriation.

RECOMMENDATION:

1. Receive this report.
2. Appropriate an additional \$33,000 from the Sewer Enterprise Fund to the CIP Project W14-202.

Attachments:

1. Resolution No. 2014-7515

RESOLUTION NO. 2014-7515

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH, CALIFORNIA, APPROPRIATING AN ADDITIONAL \$33,000 SEWER ENTERPRISE CIP FUNDS TO THE SEWER SYSTEM CIP PROJECT TO WIT – TELEWISE SEWER MAINLINES PROJECT NO. W15-202

WHEREAS, on February 5, 2014, City Council adopted Resolution No. 2014-7451, approving the second of a two phased list of projects for Fiscal Years 2013/14 and 2014/15 Capital Improvement Program and appropriated the funds for these projects; and

WHEREAS, one of the projects on this list was W15-202 – Telewise the remaining 142,000 linear feet of Sewer Mains in the City (CCTV); and

WHEREAS, the City Council appropriated \$160,000 for the completion of the televising work; and

WHEREAS, a subsequent and more detailed analysis of sewer mainlines found that 148,462 linear feet of mains remained to be CCTV'ed; and

WHEREAS, for the CCTV to be actionable, the video's needed to analyzed thus an assessment of the repairs needed to be prepared; and

WHEREAS, Tran Consulting Engineers (City's contract Sewer Engineer) was tasked to submit a cost estimate for this work plus the cost to prepare an assessment report following the CCTV; and

WHEREAS, the fee total was \$193,000; and

WHEREAS, staff proposes City Council appropriate additional funds in the amount of \$33,000 to allow this project to be completed; and

WHEREAS, the Sewer Enterprise Fund CIP Reserve has approximately \$1,765,721 through FY 2014/15, in addition to the \$2 million minimum emergency reserve; and

WHEREAS, there are sufficient Sewer Enterprise CIP funds reserve for this additional appropriation.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Imperial Beach as follows:

1. The above recitals are true and correct.
2. This legislative body approves the appropriation of an additional \$33,000 from the Sewer Enterprise Fund CIP Reserve to CIP Project W15-202 – Telewise Sewer Mainlines.
3. The City Manager is authorized to transfer \$33,000 from the Sewer Enterprise Fund CIP Reserve to CIP Project W15-202.

PASSED, APPROVED, AND ADOPTED by the City Council of the City of Imperial Beach at its meeting held on the 17th day of September 2014, by the following vote:

AYES: COUNCILMEMBERS:
NOES: COUNCILMEMBERS:
ABSENT: COUNCILMEMBERS:

JAMES C. JANNEY, MAYOR

ATTEST:

JACQUELINE M. HALD, MMC
CITY CLERK



AGENDA ITEM NO. 3.1

STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *AH*
MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: PUBLIC SAFETY *JS*
SUBJECT: INTRODUCTION AND FIRST READING OF ORDINANCE NO. 2014-1147 OF THE CITY OF IMPERIAL BEACH, CALIFORNIA AMENDING SECTION 10.36.160 AND ADDING SECTION 10.36.161 OF THE IMPERIAL BEACH MUNICIPAL CODE RELATED TO PARKING REGULATIONS ON STREET-ENDS WEST OF SEACOAST DRIVE.

EXECUTIVE SUMMARY:

City staff requests that City Council introduce Ordinance No. 2014-1147 for the first reading by title only to adopt the new regulations for street-ends as indicated herein. Street-end parking is available in the City of Imperial Beach as primary access to the beach and local businesses and residences along Seacoast Drive. Public Safety presented a related item for discussion to City Council on June 4, 2014 and was directed by Council to return with recommended adjustments to the hours when parking is not allowed on street-ends. The action provides changes to the Municipal Code that includes allowing earlier start times to accommodate surfers, fishers and beachgoers, and later closure times on selected street-ends to accommodate patrons of businesses with later closing hours, as well as visitors to nearby residents.

BACKGROUND:

Street-End parking is available in the City of Imperial Beach as primary access to the beach and local businesses and residences along Seacoast Drive. The City has established codes governing the use of these parking spaces, aimed to optimize access to these areas while balancing the priorities of residents and businesses. These codes have been adjusted occasionally to address changes in priorities or new issues.

ANALYSIS:

Public Safety has received feedback from the surfers, fishers and the business community regarding the No Parking periods of time on the street-ends. Surfers and fishers would like all of the street-ends that currently open at 7:00 am to open earlier instead to accommodate opportunities to do so before work, and when water conditions are often the ideal. Some of the popular area businesses remain open later, and would like parking near their establishments to be available later.

To address these issues, Public Safety is recommending a limited set of changes to the street-end parking. Public Safety is recommending that the daily opening time of street-ends, which

currently open at 7 a.m., be changed to open at 5:00 a.m. to accommodate early surfers, fishers, and beach goers. This includes Dahlia Avenue, Daisy Avenue (Dunes Park Parking Lot), Date Avenue, Elm Avenue, Elkwood Avenue, Ebony Avenue, Imperial Beach Blvd, Admiralty Way, Beach Avenue, Cortez Avenue, and Descanso Avenue.

Additionally, Dahlia Avenue, Date Avenue, Elm Avenue, Elkwood Avenue, and Ebony Avenue are all located near businesses with later hours of operation. Public Safety recommends changing the closure time from 10 p.m. to 12 a.m. (midnight) to accommodate patrons of these establishments and promote business opportunity.

There are no changes proposed to the Palm Avenue street-end, which does not have any parking restrictions, nor to the Elder Avenue street-end which has one Disabled Person Parking Space, available 24 hours daily, and is otherwise reserved for emergency vehicles.

ENVIRONMENTAL DETERMINATION:

Not a project as defined by CEQA.

FISCAL IMPACT:

There will be a cost of approximately \$4,000 for materials and labor to replace the signs currently installed in the affected street-ends. Approval of this item will authorize expenditures from the General Fund Reserve to be utilized to implement these changes.

RECOMMENDATION:

That the City Council consider public input, introduce and hold the first reading of Ordinance No. 2014-1147 by title only, waive further reading in full, and schedule the second reading and adoption of the Ordinance on October 1, 2014.

Attachments:

1. Ordinance No. 2014-1147
2. Street-End Maps

ORDINANCE NO. 2014 - 1147

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH, CALIFORNIA AMENDING SECTION 10.36.160 AND ADDING SECTION 10.36.161 TO THE IMPERIAL BEACH MUNICIPAL CODE RELATED TO PARKING AT THE BEACHFRONT AREA STREET-ENDS

WHEREAS, the California Vehicle Code ("CVC") and Imperial Beach Municipal Code ("IBMC") list violations related to the parking, standing and stopping of vehicles in the City of Imperial Beach ("City"); and

WHEREAS, IBMC chapter 10.36 lists specific parking, standing and stopping vehicle restrictions; and

WHEREAS, pursuant to a City Council discussion related to street-end parking in _____ of 2014, City Council directed staff to draft ordinance revisions regarding street-end parking; and

WHEREAS, the City desires to change the time period relating to parking on certain street-ends west of Seacoast Drive, except on Palm Avenue.

NOW, THEREFORE, IT IS ORDAINED BY THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH AS FOLLOWS:

Section 1: Section 10.36.160 of the Imperial Beach Municipal Code is hereby amended to read as follows

"10.36.160. Driving and Parking on the beach.

No person shall drive, park or leave unattended any vehicle upon the beach area of the city anytime, including Sundays and Holidays, except in designated parking spaces. "Beach area" shall include the beach, sand, pier, dunes, boardwalk, parks, and grass area west of Seacoast Drive.

~~B.—Overnight Parking Prohibited—Beachfront Area Street-ends. It is unlawful for any person to park a vehicle overnight on any street west of Seacoast Drive. For the purpose of this section, "overnight parking" means parking at one or more locations on a street end between the hours of ten p.m. and seven a.m. except on Palm Avenue. (Amended during 1991 republication; Ord. 613 § 1, 1983; Ord. 555 § 1, 1981, Ord. 93-866, § 1, 1993)."~~

Section 2: Section **10.36.161** of the Imperial Beach Municipal Code is hereby added to read as follows:

"10.36.161. Beachfront area street-end parking.

A. Beachfront street-end parking west of Seacoast Drive shall be allowed as follows:

- 1. Between the hours of 5:00 a.m. and midnight (12:00 a.m.) on the following beachfront street-ends: Dahlia Avenue, Date Avenue, Elm Avenue, Elkwood Avenue, Ebony Avenue, and Imperial Beach Boulevard;
- 2. Between the hours of 5:00 a.m. and 10:00 p.m. on the Daisy Avenue beachfront street-end, including Dunes Park;
- 3. Between the hours of 5:00 a.m. and 10:00 p.m. on Admiralty Way, Beach Avenue, Cortez Avenue, and Descanso Avenue beachfront street-ends.

B. It shall be unlawful to park in the beachfront street-ends west of Seacoast Drive at times other than as listed in subsection A above.

C. The parking regulations in this section 10.36.161 shall apply at all times, including Sundays and holidays.

D. The parking regulations in this section 10.36.161 shall not apply to the beachfront street-end on Palm Avenue.”

Section 3: Severability. If any section, subsection, subdivision, paragraph, sentence, clause or phrase of this Ordinance, or its application to any person or circumstance, is for any reason held to be invalid or unenforceable, such invalidity or unenforceability shall not affect the validity or enforceability of the remaining sections, subsections, subdivisions, paragraphs, sentences, clauses or phrases of this Ordinance, or its application to any other person or circumstance. The City Council declares that it would have adopted each section, subsection, subdivision, paragraph, sentence, clause or phrase hereof, irrespective of the fact that any one or more other sections, subsections, subdivisions, paragraphs, sentences, clauses or phrases hereof be declared invalid or unenforceable.

Section 4: The City Clerk is directed to prepare and have published a summary of this ordinance no less than five days prior to the consideration of its adoption and again within fifteen (15) days following adoption indicating votes cast.

EFFECTIVE DATE: This Ordinance shall be effective thirty (30) days after its adoption.

INTRODUCED AND FIRST READ at a regular meeting of the City Council of the City of Imperial Beach, California, on the ____ day of _____ 2014;

THEREAFTER ADOPTED at a regular meeting of the City Council of the City of Imperial Beach, California, on the ____ day of _____ 2014 , by the following vote:

AYES:
NAYS:

ABSENT:

Jim Janney, Mayor

ATTEST:

Jacqueline Hald, City Clerk

APPROVED AS TO FORM:

Jennifer M. Lyon, City Attorney

I, City Clerk of the City of Imperial Beach, do hereby certify the foregoing to be an exact copy of Ordinance No. 2014 - _____, "AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH, CALIFORNIA AMENDING SECTION 10.36.160 AND ADDING SECTION 10.36.161 TO THE IMPERIAL BEACH MUNICIPAL CODE RELATED TO PARKING AT THE BEACHFRONT AREA STREET-ENDS"

JACQUELINE HALD, CITY CLERK

DATE

Palm Avenue



Dahlia Avenue



Change From 10pm-7am to Midnight - 5am
8 Regular Spaces

Daisy Avenue



Change opening time from 7am -to 5am.
Closing time remains at 10pm, same as all parks.
14 Regular Spaces, 1 Disabled Space

Date Avenue



Change From 10pm-7am to Midnight to 5am
14 Regular Spaces, 1 Disabled Space

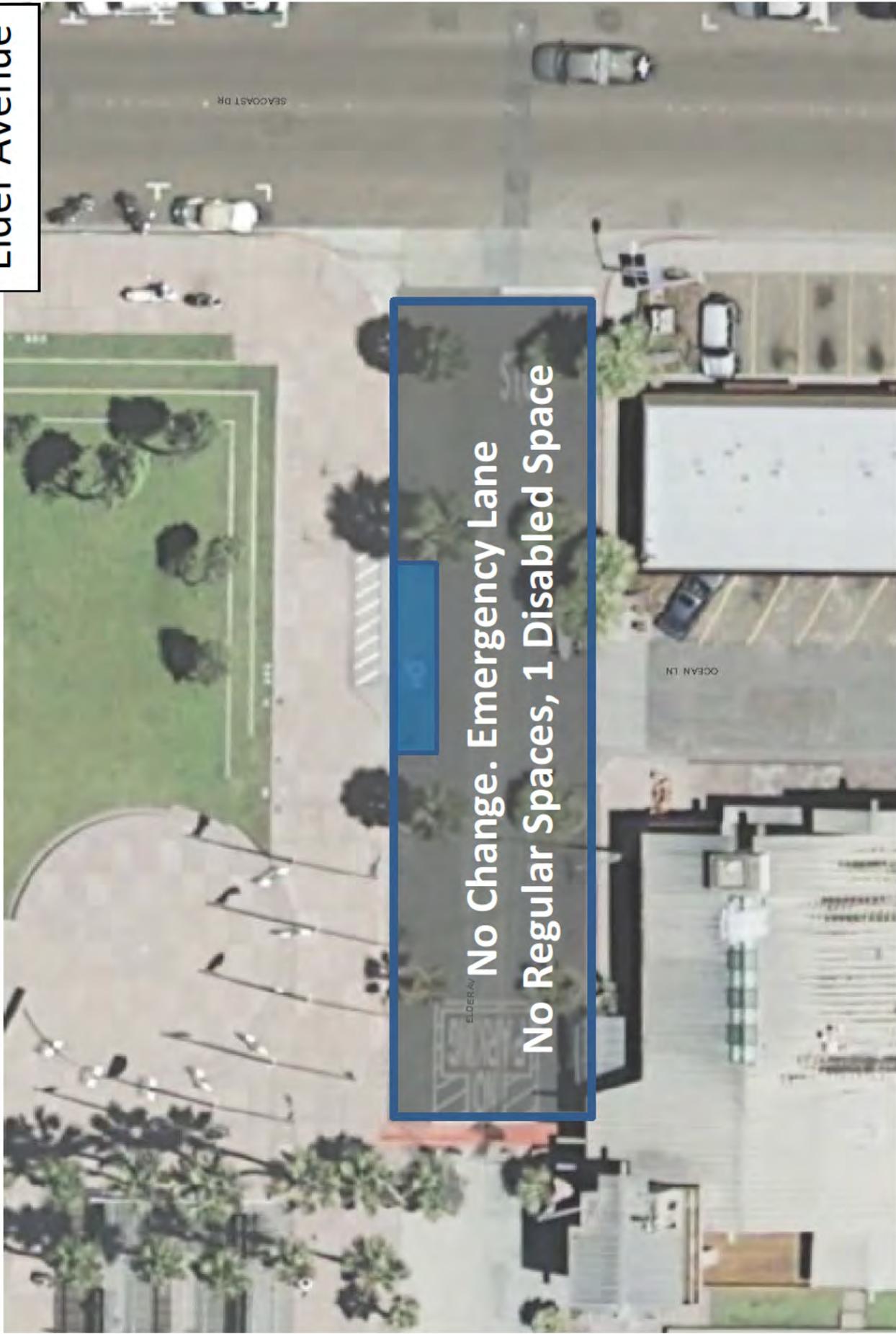


Elm Avenue



Change From 10p-7a to Midnight to 5a
9 Regular Spaces

Elder Avenue



No Change. Emergency Lane
No Regular Spaces, 1 Disabled Space

Elkwood
Avenue

Change From 10pm-7am to Midnight - 5am
7 Regular Spaces



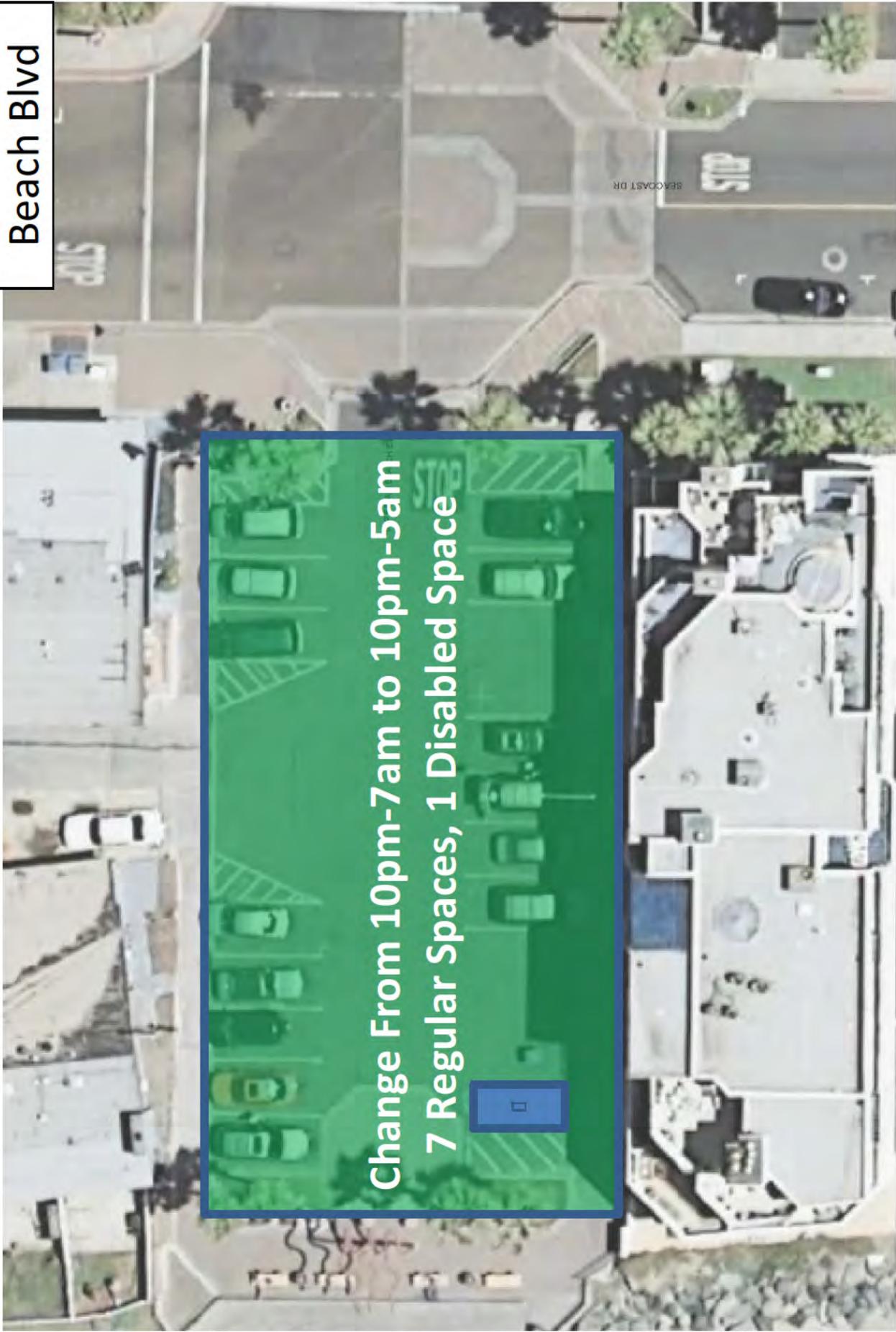
Ebony
Avenue



Change From 10pm-7am to Midnight - 5am
7 Regular Spaces

Imperial
Beach Blvd

Change From 10pm-7am to 10pm-5am
7 Regular Spaces, 1 Disabled Space



Admiralty
Way



Change From 10pm-7am to 10pm - 5am
7 Regular Spaces, 1 Disabled Space

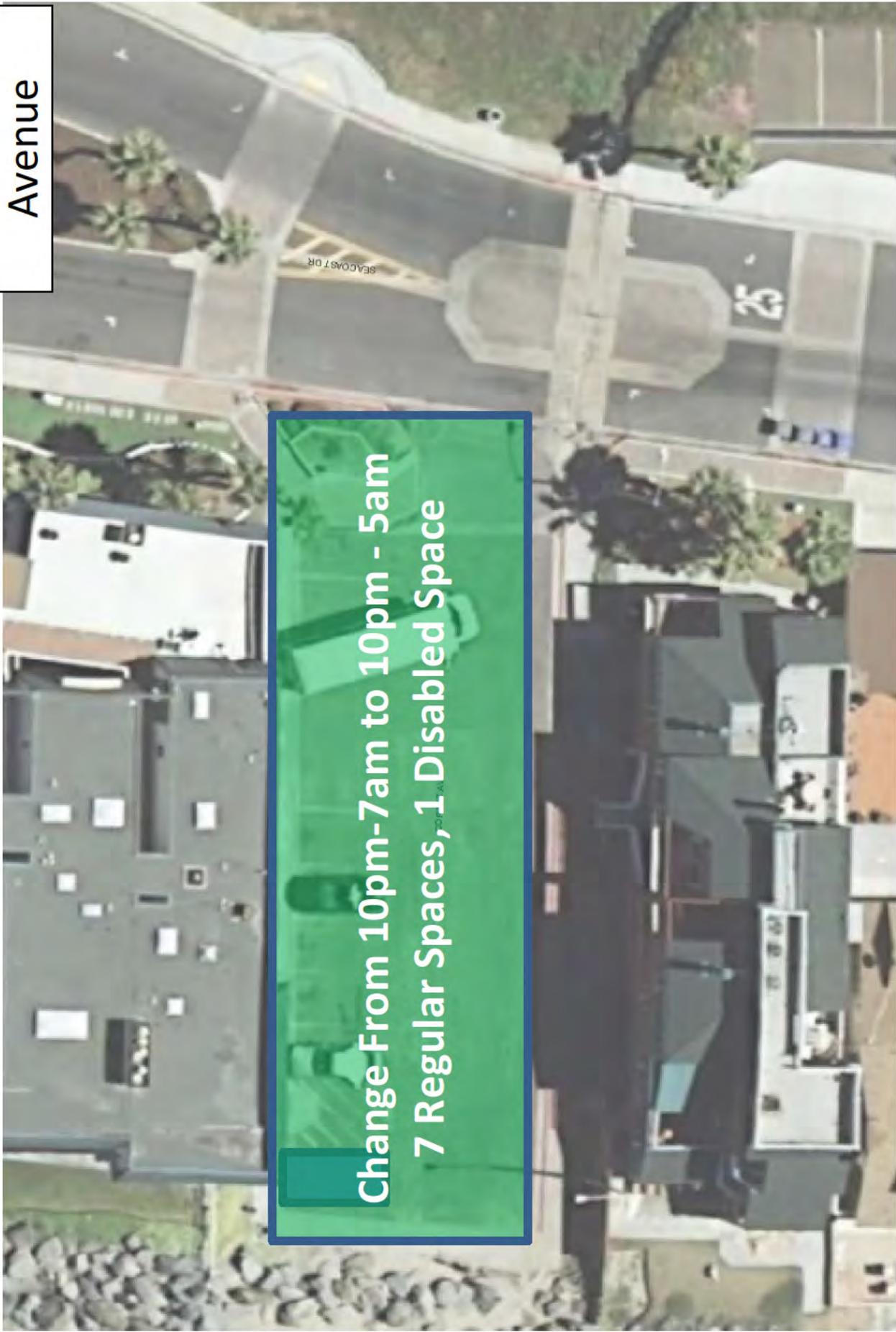
Beach
Avenue

Change From 10pm-7am to 10pm - 5am
7 Regular Spaces, 1 Disabled Space



Cortez
Avenue

Change From 10pm-7am to 10pm - 5am
7 Regular Spaces, 1 Disabled Space



Descanso
Avenue



Change From 10pm-7am to Midnight-5am
7 Regular Spaces, 1 Disabled Space

ITEM 5.1

**THE STAFF REPORT WILL BE
PROVIDED AT OR PRIOR TO THE
CITY COUNCIL MEETING**



STAFF REPORT
CITY OF IMPERIAL BEACH

TO: HONORABLE MAYOR AND CITY COUNCIL
FROM: ANDY HALL, CITY MANAGER *AH*

MEETING DATE: SEPTEMBER 17, 2014
ORIGINATING DEPT.: COMMUNITY DEVELOPMENT DEPARTMENT
GREG WADE, ASSISTANT CITY MANAGER/ COMMUNITY DEVELOPMENT DIRECTOR *GW*
JIM NAKAGAWA, AICP, CITY PLANNER

SUBJECT: REPORTS: NAVAL BASE CORONADO (NBC) COASTAL CAMPUS ENVIRONMENTAL IMPACT STATEMENT (EIS) COMMENTS

EXECUTIVE SUMMARY:

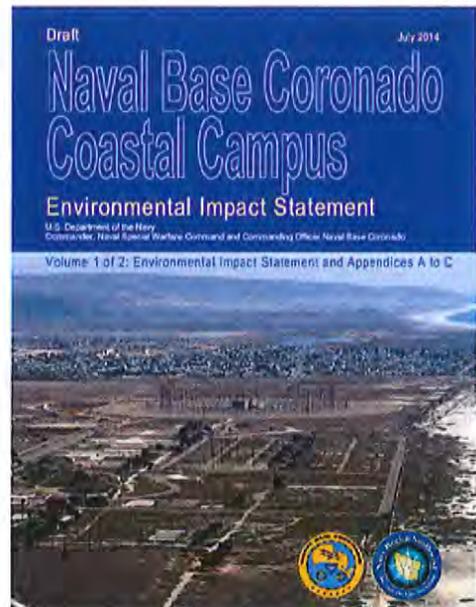
The U.S. Navy has released the draft Environmental Impact Statement (EIS) for the Naval Base Coronado (NBC) Coastal Campus for public review. City Council review and possible modification of the draft letter of comment for this document is requested.

PROJECT DESCRIPTION/BACKGROUND:

The Navy proposes a consolidation of training facilities within the Silver Strand Training Complex (SSTC) area. The U.S. Navy has released the draft Environmental Impact Statement (EIS) for the Naval Base Coronado (NBC) Coastal Campus for public review. A community workshop was held at the Marina Vista Center in Imperial Beach on August 13, 2014.

PROJECT EVALUATION/ DISCUSSION:

A review of the EIS document by city staff has identified a number of significant issues and omissions. The City has a number of projects for which the California Environmental Quality Act (CEQA) requires the preparation of environmental documents. CEQA requires our projects to consider the cumulative environmental effects of other projects, including those by the U.S. Navy. Similarly, the implementing regulations of the National Environmental Policy Act (NEPA) cited in the EIS require the consideration of the cumulative effects of other projects, including those of the City. The data and analysis for the environmental documents for both those of the City and the U.S. Navy should be consistent in



order for them to be legally adequate. The attached draft letter of comment lists a number of issues that need to be addressed in the following issue areas of concern: traffic, utilities, storm water, public safety, climate change/ sea level rise, and cumulative impact analysis.

ENVIRONMENTAL INFORMATION:

This EIS document is being provided for review pursuant to the requirements of the National Environmental Policy Act (NEPA) of 1969.

FISCAL ANALYSIS:

There is no direct fiscal impact on the City with this Navy document other than staff time expended to prepare comments on the draft EIS. However, with the implementation of the Coastal Campus project, significant fiscal impacts to the City are likely to accrue to mitigate for the impacts that are discussed in the EIS and for the impacts identified in the City's letter of comment.

DEPARTMENT RECOMMENDATION:

Staff recommends that the City Council review the draft comment letter regarding the NBC Coastal Campus EIS and provide any additional comments.

CITY MANAGER'S RECOMMENDATION:

Approve Department recommendation.

Attachments:

1. Draft comment letter on the NBC Coastal Campus EIS
2. NBC Coastal Campus EIS Executive Summary
3. City of Coronado comments
4. Caltrans comments

c: file NBC Coastal Campus EIS

Ms. Teresa Bresler, NBC Coastal Campus EIS Project Manager, 2730 McKean Street, Bldg. 291, San Diego, CA 92136 teresa.bresler@navy.mil

Wesley E Bomyea, CIV OASN (EI&E), OPDASN EI&E, Community Plans and Liaison, Naval Base Coronado, Building 3 (Public Works Office), PO Box 357040, Code N46C, San Diego, CA 92135-7040 wesley.bomyea@navy.mil

Rachel A. Hurst, Director of Community Development, and Ann McCaull, Senior Planner, City of Coronado, 1825 Strand Way, Coronado, CA 92118 rhurst@coronado.ca.us amccaull@coronado.ca.us

Mark Delaplaine, Federal Consistency Manager, California Coastal Commission, 45 Fremont St #2000, San Francisco, CA 94105 Mark.Delaplaine@coastal.ca.gov

Arnold Torma, KOA Corporation, 5095 Murphy Canyon Rd., Suite 330, San Diego, CA 92123 jatorma@koacorporation.com

Greg Shields, PE, CEO, Project Design Consultants, 701 B Street Suite 800, San Diego, CA 92101 greg@projectdesign.com

Sarah Strand, Regional Planner SANDAG, 401 B Street, Suite 800, San Diego, CA 92101 sarah.strand@sandag.org



Attachment 1

City of Imperial Beach, California

OFFICE OF THE MAYOR

825 Imperial Beach Blvd., Imperial Beach, CA 91932 Tel: (619) 423-8303 Fax: (619) 628-1395

September 17, 2014

NBC Coastal Campus EIS Project Manager
Attn: Ms. Teresa Bresler
2730 McKean Street, Bldg. 291
San Diego, CA 92136

RE: Naval Base Coronado (NBC) Coastal Campus Draft Environmental Impact Statement (EIS)

Dear Ms. Bresler:

Thank you for the opportunity to provide comments on the Environmental Impact Statement (EIS) prepared for the proposed Coastal Campus located at Naval Base Coronado. The City of Imperial Beach (the "City") values its partnership with the Navy in addressing the occasional issues that arise in relation to Navy activities. Imperial Beach is proud to be home to a number of Navy service personnel that have found much needed housing in our community and it is anticipated that the proposed Coastal Campus will have positive economic benefits to the City with increased business patronage and activity.

The Coastal Campus will potentially influence Imperial Beach more than any other recent Navy instigated activity. The impacts identified in the EIS and the issues raised in this letter are important to Imperial Beach and need to be addressed to ensure the construction of the Coastal Campus will yield positive results while minimizing negative impacts. From past experience, Imperial Beach has learned that environmental documents need to be complete and consistent. It is with this perspective that the City offers its comments.

Based upon the City's review of the EIS and the impacts it identifies, the City's primary concerns pertain to Traffic and Circulation and Utilities impacts and the proposed Mitigation Measures and Impact Avoidance and Minimization Measures identified in the EIS in these respective areas. As such, please accept the following general comments and requests regarding these areas of concern:

Traffic

1. The EIS identifies a number of intersections within and outside the City that will be impacted by development of the Coastal Campus. The City requests that the Navy work with the City and the City's traffic engineering consultant (KOA Corporation) to develop a mitigation program to address the traffic impacts identified in the EIS. One focus of primary concern to the City is the intersection of Silver Strand Boulevard and Palm Avenue that is already impacted by traffic from the existing entry gate to the Naval Radio Receiving Facility. Even though the EIS does not identify significant impacts due to the project at this intersection, the City anticipates that additional traffic generated from the proposed Coastal Campus, both during and post-construction, would impact this and other nearby local streets and intersections. Mitigation measures for this intersection that

should be considered include: the relocation of the southern gate at the north end of Silver Strand Boulevard farther north onto Navy property to provide increased queuing distance to reduce the amount of traffic congestion that might otherwise occur on Silver Strand Way; installing appropriate traffic control mechanisms at Silver Strand Way to control east/west traffic on Palm Avenue; instituting modified hours of operation (i.e. between 10:00 a.m. to 2:00 p.m.) for the southern gate; and consideration of restricting the southern gate to pedestrian and bicycle traffic only.

2. The City requests that all identified traffic impact Mitigation Measures and specifically identified traffic Impact Avoidance and Minimization Measures of the EIS be funded and implemented prior to and/or with construction of the proposed Coastal Campus and completed by 2024. The design and construction of these improvements should be developed in close coordination with Caltrans and the cities of Imperial Beach, San Diego and Coronado. This would help to address the significant traffic impacts identified in the EIS that could otherwise adversely impact the cities of Imperial Beach, San Diego and Coronado.
3. The City requests that the Navy work with Caltrans, the City, and the City of San Diego to design and implement traffic signalization modifications and/or signal rephasing throughout the entire Palm Avenue/SR-75 corridor from I-5 to the proposed northern entry gate of the proposed Coastal Campus. City staff has been evaluating and analyzing the traffic and circulation along Palm Avenue/SR-75 as part of its Palm Avenue Mixed Use and Commercial Corridor Master Plan and has made preliminary determinations that, through signal timing and phasing modifications, significant improvements to levels of service can result. Given the significant number and increase of average daily vehicle trips of 8,886 with the proposed Coastal Campus, along with the significant and potentially significant impacts to traffic that the proposed Coastal Campus will create, the City requests that the Navy fund the study and implementation of a comprehensive traffic signalization re-timing and re-phasing effort for the Palm Avenue/SR-75 corridor from I-5 to the proposed Coastal Campus to help mitigate the identified significant traffic impacts.
4. As part of the above-requested traffic signalization modifications, the traffic signals along SR-75 should be upgraded to incorporate the latest technology that provides more effective signal synchronization and efficiency in order to deal with the increased traffic that would result from the project. The Navy should work with the City's traffic engineering consultant to examine signal lights that can react to current traffic conditions without "timed delays" at intersections when no traffic is present and examine preferential treatment of east/west traffic during peak hour traffic, and limit turning movements onto Palm Avenue/SR-75 during peak traffic periods.
5. The City does not have a traffic impact fee system and, therefore, does not have the funds programed to install street and traffic signal improvements and/or to maintain the improvements as a result of this project. As mitigation, the City believes that the Navy should participate in and/or fund the cost of completing the necessary upgrades to the intersections and traffic signals along Palm Avenue/SR-75 to accommodate the traffic increases clearly resulting from the proposed project. Rather than simply identify the problem, there should be actual mitigation of the impacts based on the fact that the City will be significantly impacted by a decision wholly outside of its jurisdictional purview.
6. Underground conduits for a traffic light at the Palm Avenue and Rainbow Drive intersection have already been installed. As the EIS identifies potential impacts to this

- intersection resulting from the proposed project, the City requests that the Navy install this signal as a part of the Coastal Campus project.
7. To further mitigate traffic impacts identified in the EIS, the City requests that the Navy develop and implement a Transportation Demand Management (TDM) Plan that would encourage and promote the use of alternate modes of transportation to and within all Navy facilities including the proposed Coastal Campus. Such TDM measures should include but not be limited to: 1) promoting and implementing carpools and vanpools and the use of public transit to and from Navy installations including the Coastal Campus; 2) designing the Coastal Campus to be pedestrian and bicycle friendly and providing bicycle parking and/or a shared bicycle program within the Coastal Campus; 3) implementing shuttle service between Navy facilities in Imperial Beach and Coronado; 4) implementing an internal shuttle service within the Coastal Campus to transport military and non-military Navy personnel within the Campus and to the southern gate and promoting walk-in or bike-in traffic into Imperial Beach to minimize vehicle trips through the southern gate during lunch hours; and 5) providing north- and south-bound bus stops at the northern entry gate along SR-75.
 8. If resulting traffic and observed traffic speeds of vehicles leaving the southern gate and along Palm Avenue warrant it, the City requests that the Navy install Radar Speed Signs at the midpoint of Silver Strand Blvd. on the east and west sides of the street and at the midpoint of Palm Avenue between Rainbow Drive and Silver Strand Blvd. on the north and south sides of the street.
 9. Through existing mutual aid agreements, the City's emergency services will be partly responsible for responding to emergencies along Highway 75 from the Imperial Beach/Coronado city limits to the Coronado Cays entrance. The City notes that the EIS may not adequately address the provision of these services and the mutual aid agreements regarding public safety services that may arise from the proposed Coastal Campus.

In addition to the foregoing general comments, the City has the following comments regarding specific sections of the EIS:

Executive Summary (pages ES-1 through ES-57)

10. The Traffic and Access Improvements discussion of the Executive Summary (page ES-15, lines 13 through 17) state that the "existing southern controlled access gate would remain open; however, use of this gate would be limited to current traffic volumes with construction of the exposed entry control point." This seems to imply that only the amount of traffic that currently uses the southern gate, which is very minimal, will continue to use the gate upon project completion. However, if it "remains open" it is unclear how the Navy would limit access to the gate only to "current traffic volumes." The City requests, therefore, that the southern gate be restricted to access only during non-commuting hours (i.e., access only between 10:00 AM and 2:00 PM) to mitigate the identified traffic impacts in the primarily residential neighborhood surrounding the southern gate. The City also requests that the entry control point of the southern gate be located several hundred feet within Navy property to allow for adequate (and overflow) vehicle queuing and/or stacking that would otherwise occur on Silver Strand Way.

11. The Traffic and Access Improvements discussion (page ES-15, lines 23 through 33) of the Executive Summary identifies future traffic improvements (P-991) for several intersections in and near Imperial Beach. For the Rainbow/SR-75 intersection, this includes “restriping of the traffic lanes on Rainbow Drive and adjusting the intersection traffic signal phasing” by 2024. However, mitigation measures of the EIS itself and the Summary of Effects Table ES-3 of the Executive Summary do not include adjusting the traffic signal phasing. The City requests that the Navy work with Caltrans and the cities of Imperial Beach and San Diego to modify the traffic signal phasing of all intersections within the Palm Avenue/SR-75 Corridor between the proposed Coastal Campus and I-5 to address the significant increase of 8,886 ADT expected with the implementation of Alternative 1. This would also help facilitate the implementation of the Palm Avenue Mixed Use and Commercial Corridor Master Plan on which the City has been working since 2007 and is currently being designed and prepared for environmental analysis.
12. The Cumulative Impacts discussion of the Executive Summary (page E-19, lines 18 through 23) states that “traffic generation associated with military and civilian projects that are completed, in progress, or planned for development in Coronado and Imperial Beach have been factored into SANDAG’s traffic forecasts” and that “regional-level planning has taken place to consider associated traffic levels” such that “when added to the impacts from other potentially cumulative projects, the Proposed Action (Alternative 1) would not result in significant cumulative impacts to traffic and circulation.” SANDAG, however, has advised the City and its traffic engineering consultant that this not accurate. Additionally, the EIS itself identifies significant impacts to traffic and circulation for several intersections in Imperial Beach and San Diego along Palm Avenue/SR-75 with Levels of Service at D and F for several intersections both in 2024 and in 2040 which will adversely impact the City of Imperial Beach, its residents, workers and visitors.
13. The Mitigation Measures and Impact Avoidance and Minimization Measures of the Executive Summary (Section ES.6.11, page ES-20) states that NEPA requires that the Federal Agency (the Navy) provide the means to mitigate adverse environmental impacts of the Proposed Action alternatives and goes on to state that mitigation measures “are proposed for implementation during the design, construction, and postconstruction states of the Proposed Action to minimize and avoid potential significant impacts.” Given that significant impacts to traffic and circulation are identified in the EIS and mitigation and impact avoidance and minimization measures are proposed, the City requests that the Federal Government (Navy) fund the implementation of these measures during design and construction of the project by 2024.
14. The Summary Table of the Executive Summary (Table ES-3, page ES-44) states that “significant and unmitigable temporary traffic impacts may occur during the construction phase of the project along the transportation route between the Proposed Action footprint and I-5 in Imperial Beach.” The City requests that all reasonable measures be taken to reduce or mitigate these impacts in the City to the maximum extent possible.
15. The Mitigation Identification and Implementation Table of the Executive Summary (Table ES-4, page ES-56) identifies mitigation measures, their benefit, evaluation criteria, implementation, responsible command and date of implementation. For the significant Traffic and Circulation impacts, Table ES-4 identifies the Responsible Party as “Host or Tenant Command, as appropriate and Caltrans and the City of Imperial Beach.” As noted above, since the identified impacts are a direct result of the proposed project, the

City believes and, therefore requests, that the Navy be responsible for the costs to design and implement the required mitigation measures.

Chapter 3.9 – Traffic and Circulation

16. The City opposes the “Construction North, Operations South” Construction Scenario as this would create “significant and unmitigable impacts” at several intersections in Imperial Beach (Chapter 3.9.2.4, page 3.9-35 lines 5 and 6) and requests, therefore, that the “North Only” Construction Scenario be utilized. Although the “North Only” Construction Scenario would also have significant and unmitigable traffic impacts, they would occur primarily along Palm Avenue/SR-75 which already carries most traffic through Imperial Beach and would minimize adverse impacts to the residential neighborhood surrounding the southern entry gate. However, all construction-related impacts should be minimized to the maximum extent possible.
17. Depending upon the year and number of Nuclear-Powered Aircraft Carriers (referred to as “Carrier Vessel Nuclear or CVN’s”) that are in port, the EIS identifies significant traffic impacts at anywhere from 5 to 8 intersections in Imperial Beach. Given these significant impacts, the City requests that the Navy work with the City of Imperial Beach to implement all of the identified mitigation and improvement measures as well as other Palm Avenue/SR-75 corridor improvements currently being designed by the City to mitigate these impacts. All such measures should be funded and implemented by the Federal Government/Navy by 2024.
18. Chapters 3.9.2.2.1 (Existing with 1 CVN Conditions) and 3.9.2.2.2 (Existing with 2 CVNs Conditions) of the EIS identify existing levels of service (LOS) for the “7th Street and Silver Strand Blvd (SR-75)” (Intersection 19) at “F” for the AM peak period and “E” for the PM peak period. The City has reviewed several traffic analyses recently for projects proposed along Palm Avenue/SR-75 and has also conducted its own traffic analysis for the Palm Avenue Mixed Use and Commercial Corridor Master Plan all of which show substantially better results for this intersection. The City requests and recommends that the Navy’s traffic engineering consultants revisit the intersection delay/LOS for this location for accuracy since the results do not replicate actual conditions in the other recent studies the City has reviewed for this location which show better results.

Chapter 3.12 – Utilities and Public Services

19. The City supports the Navy’s proposal to connect to and provide significant upgrades to the City’s sanitary sewer system, however, because the sewer capacity model that the Navy may have discussed with the City’s consultant RBF may be outdated, an updated analysis of the project impacts to the City’s sewer conveyance system should be performed. The Navy should work with the City on modeling the sewer capacity and identify areas where upgrades are needed in the system. Additionally, the City does not have funding allocated for the proposed sewer upgrades and, therefore, would need to have them designed and constructed by the Navy as part of and to adequately serve the proposed Coastal Campus project. The EIS identifies replacing the entire sewer line to pump station 5 and along Imperial Beach Blvd between 4th and East Lane. Again, the City does not have funds to perform these upgrades and there may be other impacts to the City’s wet wells and downstream conveyance system that are not considered in the EIS analysis.

20. It is our understanding that the Navy's stated proposal in the EIS to connect to the City's sanitary sewer/wastewater system may require Local Agency Formation Commission (LAFCO) approval. Although the City supports and would assist with the Navy's proposed connection to its sanitary sewer system, it is the City's expectation that the Navy will be exclusively responsible for all procedural, design, and construction costs associated with this stated intention including all costs associated with obtaining any required LAFCO or other jurisdictional approvals.
21. A natural gas line enters the Coastal Campus site through the southern gate and will likely be routed from that point to the new buildings. The EIS did not state whether that gas line entering the base from the City is sized sufficiently to meet their needs. If not, then the upsizing of that gas line will come through the City disrupting the community and potentially damaging City streets. The City requests that the Navy analyze this gas line to determine its adequacy to serve the proposed Coastal Campus and to identify any needed upsizing of this line.

Chapter 3.5 – Water Quality and Hydrology

22. In the mitigation chart, item 3.5 Water Quality and Hydrology, the EIS states that the alternatives would create new impervious surfaces that would alter on-site and off-site drainage patterns. The EIS also states that Alternative 1 proposes improvements to the existing storm water drainage system to accommodate increases in runoff. However, this facility is immediately adjacent to the Camp Surf detention pond that accepts runoff from the surrounding areas including drainage from the City of Imperial Beach. During the winter, this pond fills up such that the installed pump on Camp Surf needs to discharge water from the pond into the ocean. At times, the pond gets so full that water will not run off City streets and the City ends up with significant ponding (two feet or more) near the intersection of Carnation Avenue and Seacoast Drive. Until the Camp Surf pond discharge pump can pump out enough water, the ponding will remain on City Streets sometimes for several days. The runoff, surface or subsurface, from this development into the detention pond will only make the ponding in the City streets more severe and of a longer duration. Flooding of the adjacent residences at Carnation Avenue and Seacoast Drive is a concern. The City requests, therefore, that the Navy consider alternatives to mitigate this existing flooding condition that will likely be exacerbated with construction of the proposed Coastal Campus.

Chapter 4.0 – Cumulative Impacts

23. Projects Not Identified in Table 4-1:

The following projects were not and should be identified, analyzed and considered in the preparation and analysis of cumulative environmental impacts in the proposed Coastal Campus project EIS:

- a. **City of Imperial Beach Commercial Zoning Review/Update** – this was a comprehensive update of all three of the City's commercial zones approved by the City Council in August 2012. The Zoning Update included the C/MU-1 Zone extending along Palm Avenue/SR-75 from the boundary with the City of Coronado to the City of San Diego. The City's environmental consultant, AECOM, prepared a Draft Program Environmental Impact Report (PEIR) in

accordance with CEQA for this General Plan/Local Coastal Plan (LCP) and Commercial Zoning Amendments Project. A 45-day public review and comment period was provided pursuant to CEQA Guidelines Section 15105 for the Draft PEIR (SCH# 2011041048) from April 19, 2012 to June 4, 2012. The PEIR identified potentially significant and significant impacts that were not included, considered, or analyzed in the Navy's EIS. The PEIR determined that the proposed project could have potentially significant environmental effects in the areas of Air Quality, Paleontological Resources, and Noise with mitigation measures identified that would reduce the potential environmental impacts to these resource areas to below a level of significance. The PEIR also found that Implementation of the proposed project would result in significant and unavoidable impacts to Greenhouse Gas (GHG) Emissions, Hydrology and Water Quality, and Transportation and Traffic. Implementation of the mitigation measures identified in the PEIR would reduce impacts to an extent; however, even with the proposed mitigation, the GHG emissions, hydrology and water quality, and transportation and traffic impacts would remain significant and unavoidable requiring the adoption of a Statement of Overriding Considerations. Transportation Demand Management (TDM) strategies are also proposed as mitigation.

- b. Bikeway Village Project & General Plan/LCP Amendment** – On May 2, 2012, the City Council adopted approved a General Plan/Local Coastal Program Amendment, the final Mitigated Negative Declaration (MND/SCH# 2012031034) and discretionary permit approval for the Bikeway Village project. Bikeway Village proposes the conversion/adaptive reuse of two approximately 15,000 square foot warehouse structures at 535 Florence, 536 13th Streets and on vacant parcel at the northern terminus of 13th Street in Imperial Beach.
- c. Breakwater Shopping Center** – this project, which is expected to begin construction in early 2015, will be a 46,200 square feet retail shopping center at the southwest corner of 9th Street and Palm Avenue/SR-75. The project was approved by the City Council on December 14, 2011 (for which a State-mandated automatic three-year extension of time will be granted this December) along with approval of a Mitigation Negative Declaration (SCH #2011111018). The Mitigated Negative Declaration (MND) prepared for this project and was routed for public review from November 7, 2011 to December 7, 2011 and submitted to the State Clearinghouse for agency review. The City conducted an Environmental Initial Study (IS) that determined the proposed project could have a potentially significant environmental effect in the areas of Aesthetics, Biological Resources, Cultural Resources, Geology and Soils, Hazards and Hazardous Materials, Hydrology /Water Quality, Noise, and Transportation/Traffic. However, the MND identified mitigation measures that will avoid or reduce all potentially significant environmental effects to below a level of significance.
- d. Bicycle Transportation Plan and Eco Route Bikeway Project & General Plan/LCP Amendment** – on April 1, 2009, the City Council approved an Amendment to the Circulation Element of the General Plan/Local Coastal Plan and certification of an Environmental Impact Report (EIR, SCH# 2007101061), adoption of a Bicycle Transportation Plan (BTP) with policies for bicycle facilities and route designations, and an Administrative Coastal Development Permit for a traffic calming improvement plan from 7th Street to 3rd Street within the Palm Avenue right-of-way. Among other proposed projects, these actions resulted in

the reduction of Palm Avenue between 7th and 3rd streets from a four-lane street to a two-lane street with Class II Bike Lanes and on-street parking. Given that significant and potentially significant impacts to traffic and circulation have been identified in the EIS as a result of the proposed Coastal Campus project, this EIR and the Eco Route Bikeway Project should be included and analyzed in the cumulative impact analysis of the EIS.

- e. **Palm Avenue and Carnation Avenue Street End Project** – this is a Port of San Diego project for the improvement of two City street ends, Palm Avenue and Carnation Avenue, between Seacoast Drive and the beach. The project was designed and intended to provide improved and enhanced coastal access to and along the beach. The Palm Avenue Street End portion of the project has been constructed, however, the Carnation Avenue portion is dependent upon the granting of a 20-foot easement by and from the Navy and this portion of the project is, as yet, unfunded. An environmental impact report was prepared, circulated for public review and approved by the City for this project in August of 2006 (SCH# 200231106).
- f. **Palm Avenue Mixed Use & Commercial Corridor Master Plan** – the City completed and the City Council approved the Palm Avenue Commercial Corridor Master Plan Study in February 2009 which proposed right-of-way improvements for the Palm Avenue/SR-75 corridor focused on improving pedestrian safety and walkability, enhancing the corridor's overall aesthetics and appearance, and improving functionality of the vehicular corridor while maintaining acceptable traffic levels of service in order to create a "main street" environment. In July 2013, SANDAG awarded grant funding to the City to prepare design drawings for the Palm Avenue Mixed Use and Commercial Corridor Master Plan and to prepare an environmental review document for this project. Work began on this effort in January 2014 and continues today. City staff has been closely coordinating with Caltrans and SANDAG and with the City's residents and businesses in the preparation of the drawings. The City has also engaged the Navy in the design and outreach effort. Given that significant and potentially significant impacts to traffic and circulation have been identified in the EIS as a result of the proposed Coastal Campus project, this EIR and the Palm Avenue Mixed Use and Commercial Corridor Project should and must be included in the cumulative impact analysis of the EIS.
- g. The indoor shooting range (MILCON P-876) that was given a Categorical Exclusion but details of the project were not provided to the City. The Court ruled in the street ends lawsuit against the City of Imperial Beach and the San Diego Port District that separate projects need to be cumulatively analyzed in the environmental document and cannot be excluded. The City needs details of the project in order to conduct an adequate analysis of cumulative impacts in the City's environmental documents.

Climate Change/Sea Level Rise

- 24. The City and the Tijuana River National Estuarine Research Reserve (TRNERR) are currently each underway with sea level rise (SLR) studies to assess the potential impacts that sea level rise may have on the City and the Tijuana Estuary. There have also been sea level rise studies done for Naval Base Coronado (through SPAWAR) and for other jurisdictions in the vicinity of the proposed Coastal Campus. The City requests,

therefore, that the Navy carefully consider the potential impacts to Imperial Beach that SLR may have as in connection with the development of the proposed Coastal Campus.

Chapter 5.0 Mitigation Measures and Impact Avoidance and Minimization Measures

25. The City firmly believes that all Mitigation Measures T-1 through T-6 identified on pages 5-20, 5-21 and 5-24, in Chapter 5.9.1.1 of the EIS should include traffic signal modification and rephasing for each identified affected intersection as stated in the Executive Summary (page ES-15). The City requests that the traffic signalization for all other signalized intersections in the Palm Avenue/SR-75 corridor between Interstate 5 and the northern entry gate of the Coastal Campus also be modified and rephased to mitigate the identified significant traffic impacts. The Navy should work closely with Caltrans and the cities of Imperial Beach, Coronado and San Diego to implement these traffic signal and intersection modifications.
26. Mitigation Measure T-4 on page 5-21 of the EIS calls for the elimination of the eastern crosswalk at the intersection of 13th Street and Palm Avenue/SR-75 in Imperial Beach. The City does not support the elimination of this crosswalk. The Palm Avenue Commercial and Mixed Use Corridor Master Plan on which the City has been working since 2007 is now in the design and environmental review phase and is intended to promote pedestrian safety through the corridor and, therefore, proposes enhanced and clearly delineated crosswalks at all intersections along Palm Avenue/SR-75. Additionally, there is an existing eastbound MTS bus stop directly adjacent to this eastern crosswalk at 13th Street and MTS has indicated that this bus stop must have direct crosswalk access from this stop to the north. The City believes that the traffic impacts identified in the EIS for this intersection can be mitigated by signalization modification and rephrasing and other measures without having to eliminate this crosswalk.
27. The City supports Impact Avoidance and Minimization Measure t-1 (EIS Section 5.9.1.1, page 5-20, lines 4 through 8) to accelerate implementation of the new entry control point (P-947) for SSTC-South "as early as possible" to "prevent traffic in Imperial Beach from becoming excessive and to provide appropriate capacity and security facilities to process the increasing number of vehicles accessing SSTC-South." The North Gate should be completed, with ample queuing on Navy property, prior to any occupation of the new facility to avoid the creation of an unintentional traffic pattern where Navy commuters habituate to using the southern gate in Imperial Beach. Consideration should also be given to possibly installing an overpass for left turns (subject to our traffic consultant and Caltrans review) rather than a center left turn lane for north bound SR-75 traffic onto the base.

Once again, the City greatly appreciates the opportunity to be able to offer comments on the Navy's Coastal Campus EIS and the City looks forward to cooperating with the Navy, in collaboration with the cities of Coronado and San Diego and Caltrans, in resolving these issues.

Sincerely,

James C. Janney
Mayor

C: file US Navy Coastal Campus EIS
City Council
Honorable Mayor Kevin L. Faulconer, City of San Diego
Laurie Berman, Director, Caltrans District 11
Andy Hall, City Manager
Greg Wade, Assistant City Manager/Community Development Director
Jim Nakagawa, AICP, City Planner
Arnold Torma, KOA Corporation
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Sarah Strand, SANDAG

Draft

Attachment 2

July 2014

Naval Base Coronado Coastal Campus

Environmental Impact Statement

U.S. Department of the Navy
Commander, Naval Special Warfare Command and Commanding Officer Naval Base Coronado

Volume 1 of 2: Environmental Impact Statement and Appendices A to C



Draft
Naval Base Coronado Coastal Campus
Environmental Impact Statement

Lead Agency:
Department of the Navy

Action Proponents:
Commander, Naval Special Warfare Command and
Commanding Officer Naval Base Coronado

Volume 1 of 2: Environmental Impact
Statement and Appendices A to C

Point of Contact: Teresa Bresler
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July 2014

1 **COVER SHEET**
2 **NAVAL BASE CORONADO COASTAL CAMPUS**
3 **ENVIRONMENTAL IMPACT STATEMENT**
4
5

6 Lead Agency for the EIS: U.S. Department of the Navy (Navy)
7 Title of the Proposed Action: Naval Base Coronado (NBC) Coastal Campus
8 Affected Jurisdiction: County of San Diego, Cities of Coronado and Imperial Beach
9 Designation: Draft Environmental Impact Statement (DEIS)
10
11

12 **ABSTRACT**

13 This Environmental Impact Statement (EIS) was prepared by the U.S. Navy in compliance with the
14 National Environmental Policy Act (NEPA) of 1969 (42 United States Code 4321 et seq.), the Council on
15 Environmental Quality Regulations for Implementing the Procedural Provisions of NEPA (Title 40 Code of
16 Federal Regulations [C.F.R.] Parts 1500–1508), and Navy Procedures for Implementing NEPA (32 C.F.R.
17 775). Four alternatives are analyzed in this EIS. The No Action Alternative would maintain the existing
18 land uses and training facilities as currently utilized at NBC and would continue to have limited space for
19 current and future training support for the Naval Special Warfare Command (NSWC), as well as an
20 inability to cope with Congressionally mandated expanding training needs.
21

22 Alternative 1 – Silver Strand Training Complex-South (SSTC-South) Bunker Demolition Alternative would
23 consolidate NSWC facilities to one location on SSTC-South. This alternative would include design and
24 construction of logistical support buildings, equipment use and maintenance training facilities, classroom
25 and tactical skills instruction buildings, storage and administrative facilities, utilities, fencing, roads, and
26 parking. A new controlled entry point would be provided for immediate access to/from State Route 75 and
27 a historic bunker would be demolished to facilitate campus construction. Alternative 2 – SSTC-South
28 Bunker Retention Alternative would include all the components of Alternative 1 within the same footprint
29 but would include retention of a historic bunker, thereby resulting in a smaller developable footprint.
30 Alternative 3 – Multi-Installation Alternative would site necessary NSWC facilities at more than one
31 location to include Naval Amphibious Base Coronado and Naval Air Station North Island, in addition to
32 SSTC-South. This alternative would include retention of the historic bunker similar to Alternative 2.
33

34 This EIS addresses the potential environmental impacts that could result from activities under the No
35 Action Alternative, Alternative 1, Alternative 2, and Alternative 3. Environmental resource topics evaluated
36 include land use and recreation; geology and soils; air quality; hazardous materials and waste; water
37 quality and hydrology; noise; biological resources; cultural resources; traffic and circulation;
38 socioeconomics and environmental justice; public health and safety; utilities and public services; coastal
39 uses and resources; and aesthetics.
40

41 **Prepared by:** Department of the Navy
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46

EXECUTIVE SUMMARY

ES.1 INTRODUCTION AND BACKGROUND

The United States (U.S.) Department of the Navy (Navy) prepared this Environmental Impact Statement (EIS) to evaluate the potential environmental effects of developing an academic campus to support the current and future operational readiness of personnel with the Naval Special Warfare Command (NSWC) on Naval Base Coronado (NBC) in San Diego County, California. This EIS was prepared in compliance with the National Environmental Policy Act (NEPA) of 1969, which is found at 42 U.S. Code (U.S.C.) §§ 4321–4370h. The Regulations for Implementing NEPA, which are promulgated by the President’s Council on Environmental Quality (CEQ), are found at 40 Code of Federal Regulations (C.F.R.) §§ 1500–1508. The Navy’s Procedures for Implementing NEPA are found at 32 C.F.R. § 775. The Commanding Officer, NBC and the Commander, NSWC are joint action proponents for this EIS.

NBC comprises the following eight Navy installations in San Diego and Los Angeles counties: Naval Air Station North Island (NASNI); Naval Amphibious Base (NAB) Coronado; the Silver Strand Training Complex (SSTC); Naval Outlying Landing Field Imperial Beach (NOLFIB); Naval Auxiliary Landing Field San Clemente Island (NALF SCI); Camp Michael Monsoor; Remote Training Site Warner Springs; and Camp Morena.

The proposed NBC Coastal Campus would include a mix of instructional and administrative facilities that would support logistics, operations, training, and administration. Specific proposed actions within the NBC Coastal Campus proposal are (1) evaluation of current land use and available facilities; (2) augmentation by design and construction of new facilities to support logistics, equipment use (and equipment maintenance) training, classroom and tactical skills instruction, storage, and administration; and (3) design and build of related site improvements that may include new infrastructure (e.g., upgraded utilities, fencing, roads, and parking). Site preparation for construction, such as demolition of existing infrastructure (e.g., buildings and roads) and site grading and leveling, would also be included. All facilities and infrastructure would be maintained as necessary after development. Details of the Proposed Action and alternatives are presented in Chapter 2.

Outdoor training at SSTC was previously analyzed under NEPA in the Silver Strand Training Complex EIS (U.S. Navy 2011b), and related in-water training was previously analyzed under NEPA in the Southern California Range Complex EIS/Overseas EIS (U.S. Navy 2009a) and the Final Hawaii-Southern California Training and Testing EIS/Overseas EIS (U.S. Navy 2013a). The type of training proposed for the NBC Coastal Campus would include equipment use and equipment maintenance training, classroom and tactical skills instruction, and physical conditioning.

The Global War on Terrorism, following the events of 11 September 2001, signaled the need for, and ultimately led to, an increase in the demand for Special Operations Force (SOF) capabilities, including Naval Special Warfare, the maritime component of the U.S. Special Operations Command (USSOCOM). The Navy was directed to support an increase in Special Warfare Operators or Sea, Air, and Land (SEAL) team personnel and to develop riverine (river-type environments) warfare capabilities. NSWC experienced substantial growth to meet the global operational demands for special operatives, which resulted in the need for new facilities to support logistics, operations, training, and administration.

1 NSWC is located at Naval Amphibious Base (NAB) Coronado and directs the Navy's SOF. It is the lead
2 maritime component of USSOCOM, headquartered at MacDill Air Force Base in Tampa, Florida. The
3 NSWC mission is to organize, train, man, equip, educate, sustain, and maintain combat readiness, and
4 deploy NSW forces to carry out special warfare missions worldwide. NSW forces operate independently
5 or in conjunction with other SOF, joint forces, allied units, and coalition forces.
6

7 NSWC supports training strategy, doctrine, tactics, and requirements of Commander, USSOCOM by
8 ensuring that NSW special operators, combat support, combat service support, and other personnel
9 involved with performing NSW missions are maintained in an optimum state of readiness, discipline, and
10 morale. NSWC further ensures that the component units formed by these personnel are ready to meet
11 the operational requirements of Combatant Commanders to whom they will be assigned upon
12 deployment. The Combatant Commanders organize, assign functions to, and direct subordinate
13 commands and forces necessary to carry out missions, including authoritative direction over all aspects of
14 military operations, joint training, and logistics. Other personnel involved with performing NSW missions
15 include Naval Construction Battalion (Seabees); explosive ordnance disposal (EOD) technicians; and
16 personnel with expertise in military specialties such as intelligence, communications, cryptology, and
17 logistics. NSWC provides direction to seven NSW Groups (NSWGs) and the NSW Center (NSWCEN).
18

19 NSWGs train, equip, command, and deploy components of NSW Squadrons to meet the exercise,
20 contingency, and wartime requirements of the regional Combatant Commanders, theater special warfare
21 commands, and numbered fleets located around the world. Additionally, they receive support from
22 permanently deployed NSW units in Guam, Bahrain, and Germany.
23

24 NSWCEN, located at NAB Coronado, provides basic and advanced instruction and training in maritime
25 Special Operations to U.S. military and government personnel and members of select foreign armed
26 forces. NSWCEN is responsible for oversight of all courses that lead to individual SEAL and SWCC
27 qualifications or certifications (U.S. Navy 2010a), and for producing operators.
28

29 The NSW organization structure is based on various echelons/levels of command. Echelon I is
30 USSOCOM, Echelon II is NSWC, and Echelon III includes the NSWGs and the NSWCEN. Echelon IV
31 commands are operational and logistical units and training commands including SEAL teams, Support
32 Activity (SUPPACT), Mobile Communications Detachments (MCD), Training Detachment (TRADET), and
33 Logistics Support Unit (LOGSU), as well as both Basic Training Command (BTC) and Advanced Training
34 Command (ATC). All Echelon IV training commands as well as operational and logistical units share
35 similar missions and resources (e.g., space, personnel, equipment, civilian support staff, and medical
36 resources). The training commands as well as operational and logistical units (Echelon IV) report to the
37 NSWGs (Echelon III) for command and control.
38

39 **ES.2 PURPOSE OF AND NEED FOR THE PROPOSED ACTION**

40
41 The purpose of the Proposed Action is to (1) provide adequate facilities to support growth of NSWC on
42 the west coast and (2) maintain the required levels of operational readiness of special warfare forces, as
43 mandated by Title 10 U.S.C. § 167.
44

45 NSWC and its subordinate commands are located at five separate installations of NBC (NASNI, NAB
46 Coronado, NOLFIB, NALF SCI, and SSTC) and the current locations of NSW facilities on NBC
47 installations do not support efficient NSW operations and training, as mandated. Many NSW facilities on

1 NBC installations are functionally obsolete and would not meet current or would not meet future
2 requirements with expansion and renovation. Many of these facilities were built during the World War II
3 (WWII) era as temporary or pre-engineered facilities designed to meet a specific and immediate need,
4 while others were built over 30 years ago for a very different force structure and are now functionally
5 obsolete. On NAB Coronado alone, NSWC and subordinate commands are spread throughout 60
6 facilities that are divided by State Route 75 (SR-75), negatively impacting the potential to achieve
7 effective Command and Control and organizational synergy.

8
9 These facilities include temporary, pre-engineered structures, tension fabric structures, and modular
10 structures built or procured only as a short-term solution to ongoing needs. In addition, several NSW units
11 are temporarily utilizing space in Bachelor Enlisted Quarters (BEQs). A lack of adequate, climate-
12 controlled gear storage facilities has resulted in increased gear degradation and/or maintenance
13 requirements. A lack of dynamic shooting and close quarters combat training facilities is resulting in west
14 coast SEALs traveling to private sector ranges in the midwest and southeast, increasing time away from
15 home and family. Basic Facility Requirements for NSWC units at NBC are currently not being met. Space
16 deficiencies and fragmentation of the force result in inefficiencies in mission planning and execution and
17 jeopardize operational readiness of NSWC.

18
19 The Proposed Action is needed due to the lack of sufficient facilities and space to support NSWC's
20 administrative, logistics, and classroom and tactical instruction functions. As identified in the NSW
21 Strategic MILCON Development Plan at NBC, use of existing facilities would prove challenging and costly
22 (U.S. Navy 2010b). The Proposed Action would meet this need by optimizing both facilities and use of
23 space, including synchronistic site improvements, within the existing NBC footprint. This would allow
24 NSWC to support their mandated mission requirements in an efficient manner. The Proposed Action
25 would also consolidate the following command elements into one geographic location for efficient
26 operations and training:

- 27
- 28 • Naval Special Warfare Group ONE (NSWG-1)
- 29 • SEAL Teams ONE, THREE, FIVE, SEVEN (SEAL Teams 1, 3, 5, 7)
- 30 • Logistics Support Unit (LOGSU) ONE
- 31 • Training Detachment (TRADET) ONE
- 32 • Naval Special Warfare Group TEN Detachment Coronado
- 33 • Naval Special Warfare Support Activity One
- 34 • Naval Special Warfare Mission Support Center
- 35 • Naval Special Warfare Group TEN Regional Cultural Engagement Unit
- 36 • Naval Special Warfare Group TEN Regional Support Troop ONE
- 37 • Naval Special Warfare Group TEN Training Troop ONE
- 38 • Naval Special Warfare Group TEN METOC Troop ONE
- 39 • Naval Special Warfare Group TEN Unmanned Aerial System (UAS) Troop ONE
- 40 • Naval Special Warfare Group ELEVEN (NSWG-11)
- 41 • SEAL Team SEVENTEEN (SEAL Team 17)
- 42 • Naval Special Warfare Center Advanced Training Command (ATC)
- 43

1 **ES.3 PROJECT LOCATION**

2
3 NBC comprises the following eight Navy installations in San Diego and Los Angeles counties: NASNI;
4 NAB Coronado; SSTC; NOLFIB; NALF SCI; Camp Michael Monsoor; Remote Training Site Warner
5 Springs; and Camp Morena. Three NBC installations—NASNI, NAB Coronado, and SSTC—are
6 considered as locations to support this Proposed Action. All three are located within 10 miles of each
7 other.

8
9 **NASNI**

10
11 NASNI is bounded by San Diego Bay on the north and west, the Pacific Ocean on the south, and
12 developed portions of the City of Coronado to the east and south. Primary on-base access is via Third
13 Street, by way of the Coronado Bay Bridge (SR-75). NASNI has three nuclear-powered aircraft carrier
14 berths, with two carriers currently homeported with more than 230 permanent and deployable aircraft.
15 NASNI is the largest naval aviation industrial complex on the west coast and serves as the master
16 helicopter base for NBC. NASNI is currently home to approximately 25,000 active duty military, reserve,
17 and civilian personnel. The majority of facilities on NASNI are dedicated to both air and water/port
18 operations and personnel support.

19
20 **NAB CORONADO**

21
22 NAB Coronado is bounded by San Diego Bay on the north, east, and south and the Pacific Ocean on the
23 west. NAB Coronado is a primarily developed area with access provided via SR-75, which bisects the
24 installation into two separate locations (bayside and oceanside). NAB Coronado's mission is to provide
25 on-base facilities and services for the support of U.S. and allied forces engaged in amphibious,
26 expeditionary, and special warfare training and operations. NAB Coronado is home to nearly 6,000 active
27 duty, selected reserve military, and civilian personnel and is the only naval amphibious installation on the
28 west coast and one of two amphibious installations in the U.S. NAB Coronado serves as the base of
29 operations for Commander, NSWC.

30
31 **SSTC**

32
33 SSTC is bordered by a developed portion of the City of Coronado to the north and the City of Imperial
34 Beach to the south, with San Diego Bay to the east and the Pacific Ocean to the west. SSTC is divided
35 into two noncontiguous areas: SSTC-North and SSTC-South. SSTC-North includes land areas on the
36 northern half of the Silver Strand peninsula, while SSTC-South includes land areas on the southern end
37 of the peninsula; both include adjacent nearshore waters of the Pacific Ocean. SSTC-North and SSTC-
38 South are separated by Silver Strand State Beach, which is owned by the California Department of Parks
39 and Recreation.

40
41 The mission of SSTC is to support the Navy and Marine Corps amphibious, expeditionary, and special
42 warfare training by providing local land, sea, and airspace support services, material, and training
43 facilities that will help Naval and Marine Corps forces achieve and maintain the highest level of
44 operational readiness.

SSTC-North

SSTC-North is used for maritime and field training only and includes 10 oceanside beach and boat training lanes, ocean anchorage areas, bayside water training areas, and bayside beaches. The anchorages lie offshore of SSTC-North in the Pacific Ocean and overlap a portion of the boat training lanes. SSTC-North consists of 745 acres of land owned by the Federal government and includes approximately 2.6 nautical miles of coastline.

SSTC-South

SSTC-South is primarily used for maritime and field training but does provide limited infrastructure for classrooms, structures, administration, and storage to support military training. It extends approximately 1.3 nautical miles along the Pacific Coast and encompasses approximately 548 acres of land owned by the Federal government from the mean high tide line on the bayside to the mean high tide line on the oceanside. SSTC-South also includes oceanside beach and boat training lanes, and inland training areas and facilities inside a fenced area. Regional access to SSTC-South is provided by Interstate 5 (I-5); local access is provided by SR-75. SSTC-South also includes areas of sensitive natural and cultural resources. Natural resources include an area of wetlands and vernal pools in the southeast portion of the site. There are several federally listed wildlife species on SSTC-South including San Diego fairy shrimp, California Least Tern, Western Snowy Plover, and Light-footed Clapper Rail and federally listed plant species include the salt marsh bird's beak. Ten World War II-era buildings/structures are located on SSTC-South. Seven of the building/structures were recommended as eligible for the National Register of Historic Places (NRHP), including the Wullenweber Antenna Array and the six building/structures recommended as contributors to the discontinuous Fort Emory Coastal Battery Historic District. Fort Emory Coastal Battery Historic District includes Building 98, Building 99, Building 100, Building 911, Building 912 fuel tank pits, and Battery Imperial. The Wullenweber Antenna Array has been approved for demolition with the exception of a segment that would be preserved for historic purposes.

ES.4 ENVIRONMENTAL REVIEW PROCESS

NEPA requires Federal agencies to examine the environmental effects of their proposed actions. The first step in the NEPA process for an EIS is to publish a Notice of Intent (NOI) to prepare an EIS, which provides an overview of the proposed action and the scope of the EIS. Scoping is an early and open process for developing the "scope" of issues to be addressed in an EIS and for identifying significant issues related to a proposed action. The scoping process for an EIS is initiated by publication of the NOI in the *Federal Register* and local newspapers. On 29 June 2012, the NOI to prepare this EIS was published in the *Federal Register* (Appendix A). The NOI invited agencies, organizations, and the general public to provide written comments about the Proposed Action and issues to be addressed in the EIS. The NOI also announced two public meetings, which were held on 17 July 2012 at the Marina Vista Community Center in Imperial Beach, California, and 18 July 2012 at the Coronado Public Library in Coronado, California. The scoping period was originally planned for 30 days but was extended for another 15 days to conclude on 14 August 2012 due to a request by the City of Coronado. Advertisements announcing the scoping meetings were placed in four local and regional newspapers: San Diego Union-Tribune, Enlace (Spanish newspaper), Coronado Eagle and Journal, and the Imperial Beach Eagle and Times. Advertisements regarding the notice of extension of the scoping period were placed in the same newspapers.

1 A summary of the public involvement process is also contained in Appendix A. Public scoping comments
 2 received during the scoping process are addressed throughout this EIS. Subsequent to the scoping
 3 process, a Draft EIS is prepared to assess potential impacts of a proposed action and alternatives on the
 4 environment. When completed, the U.S. Environmental Protection Agency publishes a Notice of
 5 Availability is published in the *Federal Register* and notices are placed in local or regional newspapers
 6 announcing the availability of the Draft EIS. The Draft EIS is circulated for review and comment; public
 7 meetings are also held. The Final EIS addresses all public comments received on the Draft EIS.
 8 Responses to public comments may include correction of data, clarifications of and modifications to
 9 analytical approaches, and inclusion of new or additional data or analyses. Finally, the decision maker will
 10 issue a Record of Decision no earlier than 30 days after a Final EIS is made available to the public.

11
 12 **ES.5 REQUIRED REGULATORY COORDINATION**

13
 14 As part of the NEPA compliance process, coordination and consultation with appropriate government
 15 agencies will be initiated to obtain regulatory input and guidance related to the Proposed Action and
 16 alternatives. The Proposed Action may require specific regulatory decisions and approvals from Federal
 17 and state agencies, as summarized in Table ES-1 below.

18
 19
 20 **Table ES-1**
 21 **Regulatory Coordination Status**

Regulation	Agency/Organization	Coordination Status
Endangered Species Act (1973, as amended)	U.S. Fish and Wildlife Service (USFWS)	Biological Assessment submittal to the USFWS on 28 April 2014
National Historic Preservation Act of 1966, as amended (1994); Archaeological Resources Protection Act of 1979; National Register of Historic Places (1977); and Native American Graves Protection and Repatriation Act of 1990	California Historic Preservation Officer, Native American Tribes	Consultation and coordination with California State Historic Preservation Officer and the Tribes is expected to be initiated in 2014
Clean Water Act (1972, as amended); Executive Order 11990 (Protection of Wetlands 1977)	U.S. Army Corps of Engineers (USACE) and California Regional Water Quality Control Board	Depending on the permit needs, coordination with USACE would occur in 2014
Coastal Zone Management Act (1972, as amended)	California Coastal Commission	Coordination would be initiated in 2014
Clean Air Act (1970 and Amendments of 1977 and 1990)	U.S. Environmental Protection Agency	General Conformity, Record of Non-Applicability drafted 7 March 2014

22
 23
 24 **ES.6 PROPOSED ACTION AND ALTERNATIVES**

25
 26 The Proposed Action would include 24 projects constructed over a 10-year period at a cost of
 27 approximately \$700 million, providing nearly 1.5 million square feet of facilities. Details of the Proposed
 28 Action and alternatives are presented in Chapter 2.

1 **ES.6.1** Development of Alternatives

2
3 Guidance for the development of alternatives is provided in CEQ regulations (40 C.F.R. § 1502.14).
4 Analysis of the environmental impacts of the alternatives is the focal point of an EIS and is intended to
5 provide the decision maker and the public with a clear understanding of relevant issues and the basis for
6 choice among identified courses of action. NEPA requires that an EIS be prepared to evaluate the
7 environmental consequences of a range of reasonable alternatives. The alternatives in this EIS were
8 developed using the following Federal and military land use policies and procedures:

- 9
- 10 • Assessment of the current and projected needs for future military land use, nonmilitary land use,
11 and environmental resources management at NBC;
 - 12 • Identification of public concerns through a public scoping process and consideration of comments
13 received during this process regarding the Navy's new development, land utilization, and
14 environmental resources management; and
 - 15 • Consideration of limited nonmilitary uses of Navy real estate and training areas at NBC
16 components (including U.S. Border Patrol and YMCA Camp Surf) that are compatible with military
17 uses and the Navy's stewardship goals for natural and cultural resources, and that do not create
18 a fiscal, compliance, security, or public health and safety risk.
- 19

20 **ES.6.2** Reasonable Alternative Selection Criteria

21
22 Consistent with the purpose and need identified in Chapter 1, alternatives selection criteria were
23 developed to help identify viable and reasonable alternatives to carry forward for analysis and to eliminate
24 unreasonable alternatives from further consideration in the EIS. The reasonable alternative selection
25 criteria for this EIS include the following:

- 26
- 27 1. Location of the Proposed Action in proximity to existing Federal facilities and military lands used
28 by NSWC within the existing footprint of NBC. NSWC is located at NBC, the largest naval
29 complex in the U.S., and will not be relocating. NBC provides a full spectrum of Navy SEAL
30 training inclusive of sea, air, and land components, which make NBC the critical present and
31 future center for NSWC. NSWC directs the Navy's SOF from NAB Coronado, while SSTC is the
32 premier west coast special warfare training area for the Navy; both are a part of NBC. A major
33 concern for NSWC is the time required by the SEALs for deployment or training away from home,
34 referred to as personnel tempo (PERSTEMPO) and individual tempo (ITEMPO). PERSTEMPO
35 refers to the total time an individual is deployed versus non-deployed, and ITEMPO refers to the
36 total time an individual is at home. Efficient location of commands, equipment, facilities, and
37 infrastructure that support NSW within the NBC footprint would minimize the amount of time
38 SEALs spend away from home for their training and would also meet OPNAV 3000.13C (U.S.
39 Navy 2007a) PERSTEMPO requirements.
 - 40 2. Avoid adversely affecting current Navy missions. Adding new facilities for NSWC, to other military
41 installations would require other Commands to reorganize and relocate, and would thereby
42 impede their missions.
 - 43 3. Co-location of NSW facilities to the extent feasible to optimize efficiency and primacy of use. Co-
44 locating the proposed NSWC facilities to a single installation would optimize efficiency and

1 provide NSWC with first priority or exclusive use of the required facilities. Co-location would
2 centralize operations and minimize organizational redundancies, integrate siting to improve
3 mobility of deployments and training evolutions, maximize resource availability, resolve critical
4 facility shortfalls, and replace inadequate and undersized facilities.
5

6 The specific geographic placement of the Coastal Campus on NBC is pivotal to providing shore
7 installation support to NSWC. Identification of NSWC's role and function, and existing geographic
8 relationship to NBC, to include land, facilities, infrastructure, and access to local ranges, has generated
9 the set of selection criteria that funnels possible approaches into a reasoned evaluation whose ultimate
10 purpose is to determine whether the examined alternatives fulfill the objective of this Proposed Action;
11 that is to say, fulfillment of the purpose and need. Co-location of NSWC components provides synergy,
12 optimizes functional and geographic relationships, and maximizes funds available for modernization.
13

14 Since 11 September 2001, USSOCOM manpower has nearly doubled, the budget has nearly tripled, and
15 overseas deployments have quadrupled. Shore forces support provided by NBC must include
16 predictability, that is, the ability of SOF personnel to use local facilities to receive necessary knowledge
17 and training "in their backyard." "Traveling to train" means more days away from home when in a non-
18 deployed status. The NBC Coastal Campus would be a modernization effort that not only increases
19 operational skills and proficiency, but also provides "days at home," thereby fulfilling the NBC mission to
20 support Fleet, Fighter and Family.
21

22 **ES.6.3 Alternatives Considered but Not Carried Forward for Detailed Analysis**

23
24 Fourteen alternatives, including the No Action Alternative, were initially considered while preparing this
25 EIS. Further analysis resulted in a determination that 12 action alternatives would not meet the
26 reasonable alternative selection criteria and, thus, would not meet the Navy's operational readiness
27 needs in Southern California. A brief description of these alternatives and reasons for their elimination are
28 provided in the following sections.
29

30 **Naval Air Station North Island**

31
32 NASNI is located on Coronado Island approximately 10 miles northwest of SSTC-South. Due to its
33 location within the NBC footprint, this alternative would meet criterion 1. NASNI is the Designated
34 Helicopter Master Base for west coast helicopters. Mission-essential transient aircraft, including various
35 helicopter, propeller, and jet aircraft, operate in and out of NASNI. NASNI is nearly fully developed in
36 areas not otherwise constrained by restrictions on runway clearances, and construction of the 1.5-million-
37 square-foot NBC Coastal Campus would require substantial relocation of uses, thereby impeding current
38 Navy activities and missions. Due to a lack of available land at NASNI, co-location of NSWC facilities
39 would not be feasible and optimizing efficiency and primacy of use would not occur. Therefore, this
40 alternative would not meet criteria 2 and 3 and was eliminated from further analysis.
41

42 **Naval Amphibious Base Coronado**

43
44 NAB Coronado is located between NASNI and SSTC-South. Due to its location within the NBC footprint,
45 NAB Coronado would meet criterion 1. NAB Coronado is nearly fully developed, and construction of the
46 1.5-million-square-foot NBC Coastal Campus would require substantial relocation of uses, constraining
47 the spaces of remaining uses and users, and thereby impeding current Navy activities and missions. Due

1 to a lack of available land at NAB Coronado, co-location of NSWC facilities would not be feasible and
2 optimizing efficiency and primacy of use would not occur. Therefore, this alternative would not meet
3 criteria 2 and 3 and was eliminated from further analysis.

4 **NOLF Imperial Beach**

5
6
7 NOLFIB is located 1 mile southeast of SSTC-South, 10 miles south of downtown San Diego, and
8 adjacent to the City of Imperial Beach. Due to its location within the NBC footprint, NOLFIB would meet
9 criterion 1. NOLFIB operates as an extension of NASNI, providing a practice airfield for helicopter
10 operations, with miscellaneous support facilities serving the military population in the Imperial Beach area
11 (U.S. Navy 2011d). Construction of the 1.5-million-square-foot NBC Coastal Campus exclusively at
12 NOLFIB would expand development and/or require substantial relocation of uses, constraining the
13 spaces of remaining uses and users, and thereby impeding current Navy activities and missions. Due to a
14 lack of available land at NOLFIB, co-location of NSWC facilities would not be feasible and optimizing
15 efficiency and primacy of use would not occur. If air operations were relocated from NOLFIB to
16 accommodate new development, the air training would need to be located elsewhere. Due to the air
17 traffic volume at NOLFIB, NASNI would not have the capacity to absorb these additional air operations
18 (U.S. Navy 2009b). Therefore, this alternative would not meet criteria 2 and 3 and was eliminated from
19 further analysis.

20 **Naval Base Point Loma**

21
22
23 Naval Base Point Loma, located approximately 9 miles northwest of SSTC-South, is one of the Navy's
24 premier west coast submarine bases. Due to its location outside the NBC footprint, this alternative would
25 not meet criterion 1. Naval Base Point Loma is nearly fully developed in areas not otherwise constrained
26 by restrictions on submarine access, and construction of the 1.5-million-square-foot NBC Coastal
27 Campus would require substantial relocation of uses, thereby impeding current Navy activities and
28 missions. Due to a lack of available land at Naval Base Point Loma, co-location of NSWC facilities would
29 not be feasible and optimizing efficiency and primacy of use would not occur. Therefore, this alternative
30 would not meet criteria 2 and 3 and was eliminated from further analysis.

31 **Naval Base San Diego**

32
33
34 Naval Base San Diego is located approximately 5 miles northeast of, and across San Diego Bay from,
35 SSTC-South. Due to its location outside the NBC footprint, this alternative would not meet criterion 1.
36 Naval Base San Diego is the principal homeport of the Pacific Fleet, consisting of 49 Navy ships, two
37 Coast Guard cutters, five Military Sealift Command logistical support platforms, and several research and
38 auxiliary vessels. Similar to Naval Base Point Loma, Naval Base San Diego is nearly fully developed in
39 areas not otherwise constrained by restrictions on ship homeporting, and construction of the 1.5-million-
40 square-foot NBC Coastal Campus would require substantial relocation of uses, thereby impeding current
41 Navy activities and missions. Due to a lack of available land at Naval Base San Diego, co-location of
42 NSWC facilities would not be feasible and optimizing efficiency and primacy of use would not occur.
43 Therefore, this alternative would not meet criteria 2 and 3 and was eliminated from further analysis.

44 **Marine Corps Base Camp Pendleton**

45
46
47 Marine Corps Base Camp Pendleton, located 45 miles north of San Diego, is the Marines' premier
48 amphibious training base and their only west coast amphibious training base. Due to its location outside

1 the NBC footprint, this alternative would not meet criterion 1. Marine Corps Base Camp Pendleton has
2 numerous environmentally sensitive (biological and cultural) resources that currently limit and constrain
3 Marine Corps training. Construction of the 1.5-million-square-foot NBC Coastal Campus would further
4 constrain training and would impede current Marine Corps activities and mission. This alternative would
5 not meet criterion 2. NSWC would share coastal training areas with Marine Corps Base Camp Pendleton
6 users and would not have primacy of use, which would not optimize efficiency of use. This alternative
7 would not meet criterion 3. Therefore, this alternative was eliminated from further analysis.

8 9 **Naval Auxiliary Landing Field San Clemente Island**

10
11 NALF SCI is located 67 miles west of San Diego and within the NBC footprint. This alternative would
12 meet criterion 1. The main mission of NALF SCI is to support research and development of many of the
13 Navy's weapon systems and it is also one of the few remaining live fire ranges available. A number of
14 constraints, including threatened and endangered species and unexploded ordnance concerns, currently
15 limit and constrain Navy training. Construction of the 1.5-million-square-foot NBC Coastal Campus would
16 further constrain training and would impede current Navy activities and mission. This alternative would not
17 meet criterion 2. In addition, the distance from assets, facilities, and land used by NSWC would be
18 problematic for integration of new NSWC facilities, which would not optimize efficiency and primacy of
19 use. This alternative would not meet criterion 3. Therefore, this alternative was eliminated from further
20 analysis.

21 22 **Camp Michael Monsoor**

23
24 Camp Michael Monsoor (formerly known as the La Posta Mountain Warfare Training Facility) is located
25 60 miles east of San Diego within the NBC footprint. This alternative would meet criterion 1. Camp
26 Michael Monsoor is one of the few places that allows SOF to conduct mountain warfare training in a real
27 life environment with limited encroachment problems. Construction of the 1.5-million-square-foot NBC
28 Coastal Campus would reduce the amount of training lands and would impede current Navy activities and
29 mission. This alternative would not meet criterion 2. In addition, the distance from assets, facilities, and
30 land used by NSWC would be problematic for integration of new NSWC facilities, which would not
31 optimize efficiency and primacy of use. This alternative would not meet criterion 3. Therefore, this
32 alternative was eliminated from further analysis.

33 34 **Remote Training Site Warner Springs**

35
36 Remote Training Site Warner Springs (RTSWS) is located approximately 45 miles northeast of San Diego
37 within the NBC footprint. This alternative would meet criterion 1. The primary purpose for the RTSWS is to
38 conduct Survival, Evasion, Resistance, and Escape (SERE) training, with a secondary purpose of
39 supporting training activities. Any new development on this land would need to be reviewed and
40 authorized by other landholders, including the Bureau of Land Management, U.S. Forest Service, and
41 Vista Irrigation District, as the Navy does not have exclusive ownership or use rights to any land at
42 RTSWS. Construction of the 1.5-million-square-foot NBC Coastal Campus would reduce the amount of
43 current SERE training lands and would impede current Navy activities and mission, thereby, not meeting
44 criterion 2. In addition, the distance from assets used by NSWC would be problematic for integration of
45 new NSWC facilities, which would not optimize efficiency and primacy of use. This alternative would not
46 meet criterion 3. Therefore, this alternative was eliminated from further analysis.

1 **Naval Air Facility El Centro**

2
3 Naval Air Facility El Centro, located 110 miles east of San Diego, is a key naval aviation training facility.
4 Due to its location outside the NBC footprint, this alternative would not meet criterion 1. Naval Air Facility
5 El Centro is developed and also has areas constrained by restrictions on runway clearances.
6 Construction of the 1.5-million-square-foot NBC Coastal Campus would expand development and/or
7 require substantial relocation of uses, constraining the spaces of remaining uses and users, and thereby
8 impede current Navy activities and missions. This alternative would not meet criterion 2. In addition, the
9 distance from assets used by NSWC would be problematic for integration of new NSWC facilities, which
10 would not optimize efficiency and primacy of use. This alternative would not meet criterion 3. Therefore,
11 this alternative was eliminated from further analysis.
12

13 **Naval Air Station Fallon**

14
15 Naval Air Station Fallon is located in the Lahontan Valley of Churchill County in west-central Nevada,
16 about 70 miles east of Reno and 540 miles north of San Diego. Due to its location outside the NBC
17 footprint, this alternative would not meet criterion 1. Naval Air Station Fallon is the Navy's premier tactical
18 air warfare training center. Construction of the 1.5-million-square-foot NBC Coastal Campus would
19 expand development and/or require substantial relocation of uses, constraining the spaces of remaining
20 uses and users, and thereby impede current Navy activities and missions. This alternative would not meet
21 criterion 2. In addition, the distance from assets used by NSWC would be problematic for integration of
22 new NSWC facilities, which would not optimize efficiency and primacy of use. This alternative would not
23 meet criterion 3. Therefore, this alternative was eliminated from further analysis.
24

25 **Naval Air Weapons Station China Lake**

26
27 Naval Air Weapons Station China Lake is located in the western Mojave Desert of Southern California,
28 approximately 225 miles north of San Diego. Due to its location outside the NBC footprint, this alternative
29 would not meet criterion 1. Naval Air Weapons Station China Lake supports the Navy's research,
30 development, acquisition, testing, and evaluation of cutting-edge weapons systems for the warfighter.
31 Construction of the 1.5-million-square-foot NBC Coastal Campus would expand development and/or
32 require substantial relocation of uses, constraining the spaces of remaining uses and users, and thereby
33 impede current Navy activities and missions. This alternative would not meet criterion 2. In addition, the
34 distance from assets used by NSWC would be problematic for integration of new NSWC facilities, which
35 would not optimize efficiency and primacy of use. This alternative would not meet criterion 3. Therefore,
36 this alternative was eliminated from further analysis.
37

38 **ES.6.4 No Action Alternative**

39
40 The No Action Alternative would maintain the existing land uses and training facilities currently at NBC.
41 None of the Proposed Action construction or improvements would occur. Current programmed levels of
42 use (type, tempo, location), including requirements for planned force growth, would continue. Use of
43 existing facilities would prove challenging and costly, as documented by the NSW Strategic MILCON Plan
44 at NBC, which identified the need for additional operational resources (U.S. Navy 2010b). As a result,
45 NSWC would continue to have limited space for current and future training and operations support, as
46 well as an inability to undertake Congressionally mandated growth. Geographically dispersed assets and
47 continued use of temporary facilities would continue to cause inefficiencies in mission planning and

1 execution as well as logistical support. Commands would not be consolidated, and inefficiencies in
 2 command and control functions would continue. By limiting facilities and land use support to
 3 accommodate NSWC growth and expansion, the No Action Alternative would not achieve the mission of
 4 NSWC or the purpose and need of the Proposed Action. The No Action Alternative is used in this EIS as
 5 an analytical baseline that establishes the current facilities and land use framework. It provides this
 6 analytical baseline upon which other alternatives may be compared.

7
 8 **ES.6.5 Alternative 1 – SSTC-South Bunker Demolition Alternative**

9
 10 Alternative 1 (SSTC-South Bunker Demolition Alternative) would consist of (1) consolidation of the
 11 necessary NSWC facilities to one location on SSTC-South; (2) design and construction of logistical
 12 support buildings, equipment use (and equipment maintenance) training facilities (including an
 13 approximately 50-foot-long by 80-foot-wide by 120-foot-tall parachute drying tower or paraloft), classroom
 14 and tactical skills instruction buildings, storage, and administrative facilities; infrastructure; utilities;
 15 fencing; roads; and parking; and (3) construction of a new entry control point providing immediate access
 16 to SSTC-South from SR-75. Also included would be a food service facility, fuel dispensing facility, and a
 17 “mini-mart” type of store. With the exception of the paraloft at 120 feet tall, all other buildings would be
 18 limited in height to 45 feet or the height of the largest bunker, Building 99. Under this alternative Building
 19 99 would be demolished along with up to 20 other existing structures. An existing Navy facility along with
 20 its associated cabling would need to be relocated north of its current location within the Alternative 1
 21 footprint. Site preparation would potentially also include demolition of infrastructure and site grading and
 22 leveling. Sustainable design would be used for all facilities as is practicable. Off-site traffic, access, and
 23 utility improvements would also be required.

24
 25 Alternative 1 would be composed of general facility requirements and proposed military construction
 26 (MILCON) projects, as included in Table ES-2.

27
 28
 29 **Table ES-2**
 30 **NBC Coastal Campus Facility Requirements Summary**

Facility Requirements¹	MILCON Projects	Estimated Square Footage
Administration	P-200, P-912, P-951	90,000
Operational Units	P-797, P-889, P-890, P-892, P-893, P-904, P-915, P-919, P-964	737,000
Logistics and Community Support	P-776, P-870, P-920, P-921, P-965	292,000
Training (Indoor and Physical Training)	P-911, P-918, P-949, P-950, P-952, P-966, P-967	340,000
TOTAL	24 MILCONS	1,459,000

31 ¹ These are general facility types, but similar uses (i.e., administrative and storage) could be included
 32 within multiple facility types. Not included in this summary are the proposed entry control point (P-947),
 33 infrastructure improvements (P-991), food service, fuel dispensing, or “mini-mart” facilities that are also a
 34 part of the Proposed Action.
 35
 36

1 The following discussion will address the proposed land uses and improvements and the interrelationship
2 of the proposed facility requirements, the demolition of Building 99, traffic and access improvements, and
3 utility improvements.

4 **Relationship Between Facility Requirements**

6
7 The guiding planning element of Alternative 1 is the clustering of interrelated uses, functions, and facilities
8 on a single, contiguous campus to facilitate multiple types of efficiencies as described below.

9 Administration

10
11 Administrative uses include command-and-control for oversight of subordinate commands. NSWG-1
12 Operations Support Facility (P-200), NSWG-11 Operations Support Facility (P-912), and the ATC
13 Operations Support Facility (P-951) would be co-located to support effective command and control.

14 15 Operational Units

16
17 Operational units including SEAL Teams 1 (P-889), 3 (P-890), 5 (P-964), and 7 (P-892) would be the core
18 of the proposed NBC Coastal Campus. These are active SEAL teams participating in a common, ongoing
19 24-month inter-deployment training cycle in preparation for their next deployment, itself a 6-month event.
20 Operational units have day-to-day interaction with their own support elements for mission planning,
21 instructions, and coordination, requiring operational adjacencies and synergies. SEAL Team 17 (P-904),
22 a reserve team on a differing training and deployment cycle, but with similar types of support needs,
23 would be co-located with the active SEAL teams.

24
25 SEAL team support elements include SUPPACTs and Mobile Communications Detachments (MCD).
26 Both have regular and frequent interaction and deploy with SEAL teams. SUPPACT (P-797, P-893, and
27 P-919), is an operational unit providing intelligence, surveillance, and reconnaissance support to SEAL
28 teams. MCD (P-915) is an operational unit providing communications support to the SEAL teams.

29 30 Logistics/Community Support

31
32 Logistics encompasses a number of functions, including Unmanned Aerial Vehicle (UAV) maintenance
33 and storage (P-870); supply (warehousing), small craft engineering (repair and maintenance of small
34 craft), and Combat Services Support (P-920); tactical ground mobility (maintenance and repair of military
35 vehicles) and air operations (cleaning, storing/hanging, and maintaining parachutes) (P-921); and dive
36 operations (repair and maintenance of dive equipment) and armory (weapons cleaning, storing, and
37 maintenance) (P-776). The Resiliency Center (P-965), a resource available to SOF personnel and their
38 families to proactively address many of the mental, physical, spiritual, and financial challenges they face,
39 would also be located on the NBC Coastal Campus.

40 41 Training (Indoor and Physical Training)

42
43 With the operational units and the logistics/community support uses clustered together, physical training
44 components are needed in proximity for efficiency of day-to-day training support. The Tactical Athlete
45 Center (TAC) (P-952) is a wellness facility for physical fitness, nutrition, alternative medicine,
46 rehabilitation and physical therapy, and spiritual healing. The purpose of the TAC is to reduce injury, aid

1 in recovery, and educate the SEALs on proper biomechanics to become stronger and more resilient. The
2 TRADET facility (P-966) includes classrooms providing a variety of courses of instruction in Land
3 Warfare, Assaults, Mobility, and Waterborne (Surface and Subsurface) Training, and supports combatives
4 training prior to deployment. The individual SEAL also spends a good deal of time at this facility when
5 preparing for deployment.

6
7 The other multiple training and training support facilities with synergies gained from co-location with the
8 elements described above would include ATC Applied Instruction (P-949); TRADET Training Tank, ATC
9 Dive Operations, and Obstacle Course and Turf Field (P-966); ATC Operations and Support and ATC
10 Communications (P-950); Close Quarters Combat (P-918); NSWG-1 Multi-Purpose Canines Complex (P-
11 967); and SERE (P-911) facilities.

12
13 Based on the descriptions above, the indoor training facilities, the operational and logistics facilities, and
14 the respective administrative facilities are operationally linked and would need to be co-located with the
15 SEAL teams to maximize operational efficiencies and to optimize organizational synergies.

16
17 If these various elements, along with their associated personnel, would be concentrated in one place, a
18 food service facility, which is a “service common” element not specific to NSWC, would be needed at the
19 project site. Without this facility, there would be no food service provided on SSTC-South. Also included
20 in the Proposed Action would be an entry control point (P-947) that would involve construction of a base
21 main gate with sentry house and anti-terrorism/force protection (AT/FP) improvements including new
22 traffic lanes for approach, queue, vehicle inspection, denial, and exit, plus reinforced fencing, a wall,
23 traffic barrier systems, pedestrian gates, a security office, utilities, paving and site improvements, and
24 parking; a fuel dispensing facility with capacity for approximately 3,000 gallons of gasoline (87 octane),
25 2,000 gallons of Diesel #2, 300 gallons of liquid petroleum, liquid propane, and 300 gallons of
26 compressed natural gas; and a “mini-mart” type of store. For the purposes of analysis in this EIS, it is
27 assumed that up to 20 existing structures (not including Building 99, discussed separately below) and
28 associated utilities and infrastructure at SSTC-South would need to be demolished to facilitate the new
29 development proposed under Alternative 1.

30 31 **Demolition of Building 99**

32
33 The existing NRHP-eligible historic bunker complex (Building 99) at SSTC-South would be demolished
34 (P-991) under this alternative, and is being reviewed under the National Historic Preservation Act (NHPA)
35 Section 106 process. The Building 99 area, approximately 4.6 acres in size, is located in the central
36 portion of the developable northern area of SSTC-South. With the removal of Building 99, this 4.6-acre
37 area would be usable for the proposed NBC Coastal Campus development. Demolition of Building 99
38 would be conducted with the use of small commercial explosives and/or diamond saws to initially break
39 up the structure followed by drilling and hammering to further break up the materials. Abatement of lead-
40 based paint/asbestos-containing materials surveyed would be conducted before demolition. The
41 demolished concrete and steel would be either reused as part of the construction material for the Coastal
42 Campus or removed to a local landfill. Assuming a worst-case scenario of no reuse, removal of the debris
43 would result in approximately 5,400 truck (round trip) trips from SSTC-South to I-5 via the Palm Avenue
44 portion of SR-75. Complete demolition would last approximately 24 months; however, demolition debris
45 would be stockpiled adjacent to the demolition site and the majority of the debris removal would occur
46 over a 2- to 3-month period.

1 Traffic and Access Improvements

2
3 Primary access to the site would be provided from SR-75 in the northern portion of SSTC-South. This
4 intersection and access would be improved with additional turn lanes on SR-75, improved ingress and
5 egress from SR-75, and a new entry control point (P-947). The ingress/egress to SR-75 would require
6 signalization. The proposed improvements to SR-75 would include a new southbound right-turn lane and
7 a new northbound left-turn lane into the proposed Coastal Campus. These improvements would occur
8 within the California Department of Transportation (Caltrans) right-of-way. The entry control point would
9 provide standard vehicle identification checks, personal identification checks, and truck inspection
10 checks, along with parking. An entry control facility, including a 600-square-foot sentry house, would
11 ensure the proper level of access control for all traffic to the Coastal Campus.
12

13 The existing southern controlled access gate would remain open; however, use of this gate would be
14 limited to current traffic volumes with construction of the proposed entry control point. To prevent
15 demolition and construction traffic from traveling through the southern controlled access gate and
16 residential areas of Imperial Beach, temporary northern access would be provided until a permanent
17 northern entry control point can be constructed. Improvements to the temporary northern access could
18 include a traffic signal, a left-turn lane on northbound SR-75 into the site, and a right-turn lane on
19 southbound SR-75 into the site. These improvements would be within the Caltrans SR-75 right-of-way.
20 Future traffic improvements (P-991) would also be required at five intersections on Palm Avenue (SR-75).
21 These improvements are described below:
22

- 23 • Rainbow Drive/Palm Avenue (SR-75) – restriping of the traffic lanes on Rainbow Drive and
24 adjusting the intersection traffic signal phasing. These improvements would be needed by 2024.
- 25 • Palm Avenue (SR-75)/9th Street – adjusting the intersection traffic signal phasing. This
26 improvement would be needed by 2040.
- 27 • Palm Avenue (SR-75)/13th Street – adjusting the intersection traffic signal phasing. This
28 improvement would be needed by 2040.
- 29 • Palm Avenue (SR-75)/19th Street/Saturn Boulevard – street widening on Palm Avenue (SR-75)
30 to change the westbound approach to include a second westbound left-turn lane. This
31 improvement would be needed by 2040.
- 32 • I-5 southbound exit ramp/Palm Avenue (SR-75) – extend the southbound right-turn lanes on the
33 exit ramp. This improvement would be needed by 2040.
34

35 Utility Improvements

36
37 Utility improvements (P-991) would be required to serve the Coastal Campus. A 16-inch water line within
38 a 30-foot-wide easement extends through the site north to south. The water easement is with California
39 American Water Company. The existing 16-inch line would be tapped into at two locations to provide
40 redundancy for the 10-inch fire main, as well as an additional tap for a 6-inch line for potable water
41 service to the new MILCONs. California American Water Company has recommended that 200,000
42 gallons of on-site water storage along with booster pumps be included to handle peak flows. The water
43 storage would be located in one or more water storage tanks proposed to be constructed within the
44 project footprint.

1 The 30-foot California American Water Company water easement may need to be relocated within the
2 Alternative 1 footprint. It currently extends through the proposed Coastal Campus footprint, and
3 constructing new facilities over the pipeline would hinder future pipeline maintenance and/or repair. If
4 relocation is required, the pipeline would be replaced from this point south to the SSTC-South boundary
5 within the existing California American Water Company easement. The replaced portion(s) of the existing
6 pipeline would be abandoned in place and filled with a material (i.e., slurry-type of material) to prevent
7 pipeline collapse.

8 Wastewater service would be provided by the City of Imperial Beach system. Service would be connected
9 to the City of Imperial Beach's 6-inch wastewater line south of SSTC-South. A new wastewater
10 conveyance system along with a wastewater storage facility and a proposed 450 gallon per minute pump
11 station would be included on-site. A new 6-inch-diameter sewer force main would be proposed extending
12 approximately 4,000 feet from the center of the existing Wullenweber Antenna Array within Hooper
13 Boulevard to the connection to the Imperial Beach system. Operational redundancy during emergency
14 conditions would be provided by equipping the new pump station with an emergency storage facility
15 capable of accommodating up to 6 hours of average sewer inflow.

16
17 Off-site improvements to the City's system may be required to accommodate the additional wastewater
18 demand. It is assumed that the City's entire sewer main to Pump Station 5 (east of the intersection of
19 Dahlia Avenue and Seacoast Drive) would be replaced. This would include upgrades to the sewer lines
20 within Silver Strand Boulevard, Calia Avenue, and Seacoast Drive to Pump Station 5. Improvements to
21 the sewer line within Imperial Beach Boulevard from 4th Street to East Lane may also be required. The
22 proposed improvements would increase the 6-inch line to an 8-inch or 10-inch line.

23
24 Electrical and natural gas service would be provided by San Diego Gas and Electric. Existing electrical
25 service is available at the central, eastern boundary of SSTC-South along SR-75. The proposed electrical
26 upgrades needed to serve the proposed Coastal Campus would be installed within the four existing 4-
27 inch conduits on the eastern edge of SSTC-South. The existing switchgear building (Building S) has
28 sufficient space to accommodate the electrical upgrades. These improvements would not require any
29 ground disturbance.

30
31 On-site, the electrical system would be placed underground. A new natural gas line would need to be
32 installed from the center of the existing Wullenweber Antenna Array south within the existing road to the
33 connection at the SSTC-South/Imperial Beach boundary. Communication services would be provided on-
34 site by the Navy.

35
36 **ES.6.6 Alternative 2 – SSTC-South Bunker Retention Alternative**

37
38 Alternative 2 (SSTC-South Bunker Retention Alternative) would include all of the components of
39 Alternative 1, except Building 99 would be retained rather than demolished and would be preserved in
40 place or adaptively reused. All other existing structures on SSTC-South proposed for demolition under
41 Alternative 1 would also be proposed for demolition under Alternative 2.

42
43 The existing NRHP-eligible historic Building 99 at SSTC-South would be retained and preserved in place
44 or adaptively reused under Alternative 2, subject to review under the NEPA and NHPA Section 106
45 process. Due to the central location and the areal extent of the bunker, the portion of the Alternative 2

1 footprint that could be developed for the Coastal Campus itself would be smaller (by 4.6 acres) than
2 under Alternative 1.

3 4 **ES.6.7 Alternative 3 – Multi-Installation Alternative**

5
6 Alternative 3 (Multi-Installation Alternative) would include all of the components described for Alternative 1
7 (SSTC-South Bunker Demolition Alternative), but these components would be located on three separate
8 Navy installations: NAB Coronado, NASNI, and SSTC-South. As discussed in Section ES.7.3, neither
9 NAB Coronado nor NASNI alone could accommodate the entire 1.5-million-square-foot Coastal Campus
10 development; however, these installations could accommodate separate proposed uses, with the
11 remaining proposed uses located at SSTC-South.

12
13 Under Alternative 3, the MILCONs included in the plan would be the same as those included under
14 Alternative 1 and would provide the necessary operational resources for NSW. Similar to Alternative 1,
15 Alternative 3 would be composed of general facility requirements, as described in Table ES-2.

16
17 Alternative 3 differs from Alternative 1 in that four facilities included in the Proposed Action would not be
18 clustered with the other uses at SSTC-South. Specifically, SEAL Team 17 (P-904), NSWG-11 Operations
19 Support Facility (P-912), and the Resiliency Center (P-965) would be located at NAB Coronado, and the
20 maintenance and logistics portion of the UAV facility (P-870) would be located at NASNI. All other
21 proposed components would be located at SSTC-South, similar to Alternative 1, and the SSTC-South
22 portion of the Alternative 3 footprint would be the same as that of Alternative 2. While Alternative 1
23 describes the advantages of including these facilities in an integrated campus with the rest of the facilities
24 described above, below are potential reasons for taking a multi-installation approach with alternative
25 siting of these facilities.

26
27 For the purposes of analysis in this EIS, it is assumed that up to 10 existing structures and associated
28 utilities and infrastructure at NAB Coronado would need to be demolished to facilitate the new
29 development proposed under Alternative 3. Given the existing and planned status of all buildings in the
30 area identified at NAB Coronado, no compensatory construction would be required. No demolition would
31 be required at NASNI.

32
33 The configuration of Alternative 3 would still provide the adjacency and synergy required to support the
34 functionality of the various echelons/levels of command within the NSW organizational structure. Under
35 Alternative 3, Building 99 would be retained as proposed in Alternative 2. Demolition of up to 20 other
36 existing structures on SSTC-South was proposed for Alternative 1, and would also be proposed for
37 Alternative 3. Site preparation for construction, such as demolition of existing infrastructure (e.g., roads)
38 and site grading and leveling, would also be included.

39
40 All traffic and access improvements as well as utility improvements for Alternative 3 would be the same as
41 those described for Alternative 1. No additional access or utility improvements would be proposed at NAB
42 Coronado or NASNI as a part of the Proposed Action, but routine maintenance and periodic system
43 upgrades would continue to occur. Existing utilities at NAB Coronado and NASNI would be able to
44 accommodate the proposed MILCONs at those installations.

45

1 **ES.6.8 Affected Environment and Environmental Consequences**

2
3 This EIS describes existing environmental conditions and assesses the environmental effects of the
4 Proposed Action alternatives. The affected environment and environmental consequences are described
5 and analyzed according to categories of resources. In the environmental impact analysis process, the
6 resources analyzed are identified and the expected geographic scope of potential impacts for each
7 resource, known as the resource's region of influence, is defined.

8 **ES.6.9 Summary of Effects**

9
10 Environmental effects that may result from implementation of the Navy's proposed NBC Coastal Campus
11 are summarized in Table ES-3.

12
13 **ES.6.10 Cumulative Impacts**

14
15 Cumulative impacts were analyzed by following NEPA, CEQ regulations, and CEQ guidance. This
16 cumulative impacts analysis studies each impacted resource area and determines the level of impact that
17 results from the incremental addition of the Coastal Campus proposal, when added to past, present, and
18 reasonably foreseeable future actions. Identifiable effects of actions occurring in the past and present
19 were analyzed, along with reasonably foreseeable future actions to assess additive impacts of the NBC
20 Coastal Campus.

21
22 The NBC Coastal Campus project would not significantly change or impact current or planned nonmilitary
23 land use, recreation, or public access. Cumulative effects to geology and soils would be negligible relative
24 to the scale of natural processes operating in the region of influence. The Proposed Action would conform
25 to the State Implementation Plan and would not require a conformity determination. The Proposed Action
26 would not substantially contribute to global climate change and greenhouse gas emissions. Generation of
27 hazardous materials and wastes would be managed as part of the overall hazardous waste stream, and
28 existing physical capacities would be sufficient to handle cumulative additions to the existing waste
29 stream. Compliance with state and Federal regulations would limit the release of pollutants to minimal
30 amounts, which would not result in substantial cumulative effects to water resources.

31
32 The NBC Coastal Campus, along with other anticipated projects and activities, could result in minor
33 increases in intrusive noise, traffic noise, and operational noise, but cumulative effects would not be
34 significant.

35 All Federal activities within SSTC-South potentially affecting federally protected species and habitats
36 would be subject to ESA Section 7 consultation and would require the issuance of a BO by USFWS with
37 reasonable and prudent measures, terms and conditions, and conservation recommendations. In
38 addition, NBC, in concert with USFWS and as identified in several BOs issued by USFWS, training and
39 operations guidelines, and the NBC Integrated Natural Resources Management Plan (INRMP), has
40 established plans and conditions throughout SSTC-South to protect, preserve, and conserve natural
41 resources to minimize significant cumulative impacts. A Biological Assessment was submitted to the
42 USFWS on 28 April 2014, initiating formal consultation. The NBC Coastal Campus is not anticipated to
43 contribute to cumulative impacts to federally listed plants or wildlife because no occupied habitat would be
44 permanently, directly impacted. Although permanent and temporary indirect impacts are associated with

1 the NBC Coastal Campus, these are not anticipated to contribute to the loss of federally listed species or
2 occupied habitat, and, therefore, would not contribute to cumulative impacts.

3
4 The NBC Coastal Campus would adversely affect a WWII-era historic property. No nonmilitary projects in
5 the area would have the potential to disturb WWII-era historic military resources, and there would be no
6 cumulative effects from those projects. One historic structure (Building 99) eligible for listing in the NRHP
7 as a contributor to the Fort Emory Coastal Defense Historic District would be demolished under
8 Alternative 1. This would constitute an adverse effect to the NRHP-eligible Historic District. Other military
9 projects in the area that are not a part of the Proposed Action could also adversely affect WWII-era
10 historic Navy resources.

11
12 As dictated by the NHPA, the Navy is obligated to protect historic properties under its ownership in a way
13 that emphasizes preservation and minimizes the impact of undertakings that might individually or
14 cumulatively adversely affect such properties. Therefore, while individual effects may be adverse, by the
15 Navy meeting its protection obligations, the Proposed Action, when added to the impacts from other
16 potentially cumulative projects, would not result in significant cumulative impacts to cultural resources.

17
18 Traffic generation associated with military and civilian projects that are completed, in progress, or planned
19 for development in Coronado and Imperial Beach have been factored into San Diego Association of
20 Government's traffic forecasts. Therefore, while individual projects would contribute to traffic generation
21 on roadways affected by the NBC Coastal Campus, regional-level planning has taken place to consider
22 associated traffic levels. As such, when added to the impacts from other potentially cumulative projects,
23 the Proposed Action would not result in significant cumulative impacts to traffic and circulation.

24 The NBC Coastal Campus would have a minimal effect on regional employment, income, housing, and
25 infrastructure and would not contribute to cumulative socioeconomic effects in the region.

26 The Navy has specific and documented procedures in place to ensure the public health and safety from
27 Navy operational actions. The incremental impacts of the NBC Coastal Campus would not represent any
28 appreciable contribution to cumulative health and safety risks. Therefore, when added to the impacts from
29 other potentially cumulative projects, the Proposed Action would not result in significant cumulative
30 impacts to public health and safety.

31
32 The Proposed Action in combination with other developments and projects in the area would increase the
33 demands for utilities and public services. Each project would coordinate with the service's providers to
34 ensure adequate service is available and to avoid a significant cumulative impact.

35
36 The NBC Coastal Campus would be visually compatible with the existing building heights. No structures
37 would be taller than 45 feet above grade with the exception of the proposed 120-foot-tall paraloft. Partial
38 removal of the Wullenweber Antenna Array would improve the existing visual landscape of SSTC-South
39 by providing increased opened views of the natural environment. The NBC Coastal Campus would not
40 change public or coastal access. Construction effects on water quality would be temporary and would not
41 be significant, provided there was successful compliance with the water quality conservation measures.
42 Thus, cumulative aesthetic impacts and impacts to the coastal environment would not be significant.

1 **ES.6.11 Mitigation Measures and Impact Avoidance and Minimization Measures**

2

3 NEPA regulations require that the Federal agency provide the means to mitigate adverse environmental
4 impacts of the Proposed Action alternatives. These mitigation measures are proposed for implementation
5 during the design, construction, and postconstruction stages of the Proposed Action to minimize and
6 avoid potential significant impacts. Mitigation measures to address specific impacts from the proposed
7 Coastal Campus are included in Table ES-4.

8

9 As part of the Navy's commitment to sustainable use of resources and environmental stewardship, the
10 Navy incorporates into all of its activities measures that are protective of the environment. These impact
11 avoidance and minimization measures include employment of best management practices, employment
12 of standard operating procedures, and adoption of other measures that avoid or minimize the impacts of
13 Navy activities on the environment. Chapter 5 provides a list of which mitigation measures and impact
14 avoidance and minimization measures are proposed for each resource area.

15

1
2

**Table ES-3
Summary of Effects**

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
3.1 Land Use and Recreation	<p><u>Impacts:</u> No effects on existing land uses; no incompatibility with existing land uses.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 1 would expand the density and area of developed uses on SSTC-South but would not introduce incompatible land uses or be incompatible with existing land uses. Land use effects would not be significant. All off-site improvements (traffic and access and utility) would occur within infrastructure (roadways and utility) rights-of-way and corridors and would not have a significant land use impact. No recreational facilities on or off the installation would be adversely affected.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 2, similar to Alternative 1, would expand the density and area of developed uses on SSTC-South but would not introduce incompatible land uses or be incompatible with existing land uses. Land use effects would not be significant. All off-site improvements (traffic and access and utility) would occur within infrastructure (roadways and utility) rights-of-way and corridors and would not have a significant land use impact. No recreational facilities on or off the installation would be adversely affected.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 3, similar to Alternative 2, would expand the density and area of developed uses on SSTC-South but would not introduce incompatible land uses or be incompatible with existing land uses. The proposed facilities at NAB Coronado (P-904, P-912, and P-965) and NASNI (portion of P-870) would be developed in the footprints of existing buildings, consistent with the existing land use. Land use effects would not be significant. All off-site improvements (traffic and access and utility) would occur within infrastructure (roadways and utility) rights-of-way and corridors and would not have a significant land use impact. No recreational facilities on or off the installation would be adversely affected.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>
3.2 Geology and Soils	<u>Impacts:</u>	<u>Impacts:</u>	<u>Impacts:</u>	<u>Impacts:</u>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
	<p>No effects on geology and soils; no effect from geological hazards.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p>Changes in topography would be relatively minor involving construction site leveling. SSTC-South possesses highly erodible soils. Strong seismically induced ground motion and associated ground shaking could occur. Adverse effects attributable to liquefaction and settlement are considered minor. Alternative 1 development would mostly occur outside the tsunami inundation area. No significant risk of seiches and landslides occurring. No significant geology and soils impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Prepare a detailed demolition plan for Building 99. • Compliance with the seismic design criteria identified in Uniform Building Code, the Naval Facilities Engineering Command (NAVFAC) P-355 Seismic Design Manual, and the design specifications criteria of the Structural 	<p>The geology and soils impacts would be the same as Alternative 1.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Compliance with the seismic design criteria identified in Uniform Building Code, the NAVFAC P-355 Seismic Design Manual, and the design specifications criteria of the Structural Engineering Association of California. • Prepare and comply with geotechnical studies that would be conducted for the Coastal Campus overall and/or all MILCON construction sites during project design. • Implement erosion control measures after construction. • Prepare a project-specific NPDES General Construction Permit and a SWPPP. 	<p>The geology and soils impacts at SSTC-South would be the same as Alternative 1. The construction of the MILCONs on NAB Coronado and NASNI would occur on flat already developed areas with similar geology and soils impacts as described for SSTC-South. No significant geology and soils impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> These measures would be the same as for Alternative 2.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Engineering Association of California.</p> <ul style="list-style-type: none"> • Prepare and comply with geotechnical studies that would be conducted for the Coastal Campus overall and/or for all MILCON construction sites during project design. • Implement erosion control measures after construction. • Prepare a project-specific National Pollutant Discharge Elimination System (NPDES) General Construction Permit and a Stormwater Pollution Prevention Plan (SWPPP). 		
3.3 Air Quality	<p><u>Impacts:</u> No new construction or operational pollutant emissions sources would be generated; therefore, local and regional air quality would not be affected.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Annual emissions would be less than <i>de minimis</i> levels in the San Diego Air Basin (SDAB); therefore, Alternative 1 would conform to the State Implementation Plan (SIP), and a formal conformity determination would not be required.</p> <p>The estimated annual Proposed Action emissions of all pollutants (volatile organic compounds [VOCs], nitrogen oxide [NO_x], carbon monoxide</p>	<p><u>Impacts:</u> Annual emissions would be less than <i>de minimis</i> levels in the SDAB; therefore, Alternative 2 would conform to the SIP, and a formal conformity determination would not be required.</p> <p>The estimated annual Proposed Action emissions of all pollutants (VOCs, NO_x, CO, SO_x, PM₁₀, and PM_{2.5}) for Alternative 2 in 2015 through 2024 would be less than the PSD emissions rate thresholds. The air quality impacts would not be</p>	<p><u>Impacts:</u> Annual emissions would be less than <i>de minimis</i> levels in the SDAB; therefore, Alternative 3 would conform to the SIP, and a formal conformity determination would not be required.</p> <p>The estimated annual Proposed Action emissions of all pollutants (VOCs, NO_x, CO, SO_x, PM₁₀, and PM_{2.5}) for Alternative 3 in 2015 through 2024 would be less than the PSD emissions rate thresholds. The air quality impacts would not be</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>[CO], oxides of sulfur [SO_x], and particulate matter [PM₁₀ and PM_{2.5}]) for Alternative 1 in 2015 through 2024 would be less than the Prevention of Significant Deterioration (PSD) emissions rate thresholds. The air quality impacts would not be significant.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To control fugitive dust and exhaust emissions and to minimize dust during grading and earthwork operations:</p> <ul style="list-style-type: none"> • Implement best available control measures (BACM) in accordance with Chief of Naval Operations Instruction (OPNAVINST) 5090.1D, and applicable state (i.e., APCD) regulations. • Water all active construction areas at least twice daily. • Cover all trucks hauling soil, sand, and other loose materials, or require all trucks to maintain at least 2 feet of freeboard. 	<p>significant.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To control fugitive dust and exhaust emissions and to minimize dust during grading and earthwork operations:</p> <ul style="list-style-type: none"> • Implement BACM in accordance with OPNAVINST 5090.1D, and applicable state (i.e., APCD) regulations. • Water all active construction areas at least twice daily. • Cover all trucks hauling soil, sand, and other loose materials, or require all trucks to maintain at least 2 feet of freeboard. • Pave, apply water twice daily, or apply (nontoxic) soil stabilizers on all unpaved access roads, parking areas, and staging areas at construction sites. • Sweep streets daily (with water sweepers) if visible soil material is carried onto adjacent paved streets. • Prepare a detailed demolition plan to identify measures to break up, reuse to the maximum extent practicable, and 	<p>significant.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To control fugitive dust and exhaust emissions and to minimize dust during grading and earthwork operations, the measures proposed for Alternative 2 would also apply to Alternative 3.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<ul style="list-style-type: none"> • Pave, apply water twice daily, or apply (nontoxic) soil stabilizers on all unpaved access roads, parking areas, and staging areas at construction sites. • Sweep streets daily (with water sweepers) if visible soil material is carried onto adjacent paved streets. • Prepare a detailed demolition plan to identify measures to break up, reuse to the maximum extent practicable, and haul away the debris from the demolition of Building 99 and other structures. • Incorporate abatement measures if asbestos-containing building materials or lead-based paint is determined to be present during demolition. 	<p>haul away the debris from the demolition of structures.</p> <ul style="list-style-type: none"> • Incorporate abatement measures if asbestos-containing building materials or lead-based paint is determined to be present during demolition. 	
<p>3.4 Hazardous Materials and Waste</p>	<p><u>Impacts:</u> No changes to hazardous materials or hazardous waste use, transport, storage, or disposal would occur. No hazardous materials and hazardous waste impacts would occur under the No Action Alternative.</p>	<p><u>Impacts:</u> The quantity of hazardous materials transported to SSTC-South and the hazardous materials at SSTC-South would increase. However, the maximum quantities of these materials stored on-site would not increase, because the use increase</p>	<p><u>Impacts:</u> The Alternative 2 hazardous materials, hazardous waste, USTs and IR sites impacts would be the same as Alternative 1. Overall, Alternative 2 would not result in any significant hazardous materials and waste impacts.</p> <p><u>Mitigation Measures:</u></p>	<p><u>Impacts:</u> The amount of hazardous materials used and the quantity of hazardous materials transported to SSTC-South, NAB Coronado, and NASNI along SR-75 would increase. However, the maximum quantities of these materials stored on-site would not</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
	<p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p>would not trigger the need for expanded storage facilities.</p> <p>There would be a temporary increase in production of hazardous waste due to demolition and construction activities, however, contractors would be required to properly store, transport, and dispose of their hazardous waste so that there would be a minimal risk to human health or the environment. Although all former underground storage tanks (UST) have received regulatory closure, Alternative 1 has the potential to disturb the subsurface in the area of the former USTs which increases the risks to human health and the environment during excavation, transportation, and disposal. There are two Installation Restoration (IR) sites (IR Sites 10 and 11) at SSTC-South. IR Site 10 (rubble disposal area), located northeast of the Wullenweber Antenna Array, was granted No Further Action by the Regional Water Quality Control Board. IR Site 11 (asbestos), located near</p>	<p>None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>	<p>increase, because the use increase would not trigger the need for expanded storage facilities.</p> <p>Wastes from demolition and construction activities at SSTC-South, NAB Coronado, and NASNI include waste from petroleum products, coolants, water, and residual petroleum contamination in soil at former USTs and IR Sites. Alternative 3 would include retention of Building 99 similar to Alternative 2. Therefore, under Alternative 3, the impacts with regard to hazardous waste would be the same as Alternative 2. Although all former UST have received regulatory closure, Alternative 3 has the potential to disturb the subsurface in the area of the former USTs which increases the risks to human health and the environment during excavation, transportation, and disposal. Similar to Alternative 1, IR Sites 10 and 11 at SSTC-South pose minimal risk to human health or the environment under Alternative 3. There are five IR sites (IR Sites 1 through 5) at NAB Coronado; and 12 sites (IR Sites 1 through 12)</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Building 100, was recommended for No Further Action and it has been closed. IR Sites 10 and 11 pose minimal risk to human health or the environment under Alternative 1. Alternative 1 would not result in any significant hazardous materials and waste impacts.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Comply with Navy's general instructions (e.g., OPNAVINST 5100.23) to ensure that hazardous materials and hazardous waste are stored and handled appropriately. • Compliance with the Navy's current mitigation measures including Hazardous Waste Management Plan, NBC Hazardous Substance Release Integrated Contingency Plan (U.S. Navy 2008a), and Regional Explosive Hazardous Waste Management Plan (U.S. Navy 2004). 		<p>at NASNI. Only IR Sites 1 through 4 for NAB Coronado and IR Site 10 for NASNI are near the proposed Alternative 3 development. At NAB Coronado, IR Site 1 (Building 603 disposal pit) is located along the oceanside shore on the northwestern corner of NAB Coronado with current status of No Further Action. IR Site 2 (Old Refuse Disposal and Burn Area) is located near the bayside shore of NAB Coronado and overlaps geographically with IR Site 4. This site is undergoing further investigation. IR Site 3 (New Paint Shop Site) is located near the northern boundary of NAB Coronado and is undergoing further investigation. IR Site 4 (Sandblast Grit Disposal Area) is located near the bayside shore of main base NAB Coronado and overlaps geographically with IR Site 2. Further investigation is being conducted for IR Site 4. At NASNI, IR Site 10 (Property Disposal Area) is located at the west side of NASNI in the vicinity of Building 805. Removal action was completed in April 2005 and further actions are still being conducted. IR Sites 1</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<ul style="list-style-type: none"> • Field screen (e.g., air monitoring) during construction to identify potential residual petroleum contamination. • Manage and dispose of disturbed soil or debris in the event that residual contamination is encountered in accordance with Navy guidance, and applicable state and Federal regulations. • Prior to the start of any demolition activities, contractors shall perform hazardous building materials surveys in order to identify and implement appropriate control measures during demolition to protect human health (both worker and public) and the environment. Appropriate control measures may include preparation and implementation of demolition plans, lead compliance plans, and/or asbestos abatement plans, as necessary, depending upon the results of the hazardous materials 		<p>through 4 at NAB Coronado pose minimal risk to human health and the environment because of their locations relative to the proposed improvements under Alternative 3. IR Site 10 at NASNI is currently under investigation and precautions should be taken during planning and construction to prevent exposure of workers and the environment to site contaminants. Alternative 3 would not result in any significant hazardous materials and waste impacts.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>building surveys.</p> <ul style="list-style-type: none"> • A plan or guidance for the contractor should be in place in the event that unforeseen materials are discovered during demolition and construction. This would include communication and follow-on action protocol. • Where possible, avoid disturbing areas of known historical UST releases and/or IR sites. 		
<p>3.5 Water Quality and Hydrology</p>	<p><u>Impacts:</u> No new construction or operational activities would occur; therefore, water quality would not be affected.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 1 would create new impervious surfaces that could alter on-site and off-site drainage patterns, which could cause undesirable increases in surface runoff flow rates or discharge volumes. Construction could result in erosion, off-site sediment transport, pollution, and construction material spills that could impact receiving water quality. Operation could increase the potential for pollutant loading into surrounding water bodies.</p> <p>Alternative 1 proposes improvements to the existing storm water drainage system to</p>	<p><u>Impacts:</u> Alternative 2 would not result in a greater amount of impervious surfaces and associated increased runoff than Alternative 1. Similar to Alternative 1, there could be an increase in construction-related impacts to receiving water quality and the amount of pollutants entering water resources within the area. Alternative 2 proposes improvements to the existing storm water drainage system to accommodate increases in runoff. No significant water quality and hydrology impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p>	<p><u>Impacts:</u> The water quality and hydrology impacts at SSTC-South would be the same as Alternative 1. Development at NAB Coronado and NASNI would occur in developed areas and would not create new impervious surfaces. Similar for Alternative 1, construction at NAB Coronado and NASNI could result in erosion, off-site sediment transport, pollution, and construction material spills that could impact receiving water quality. With the incorporation of the below measures, no significant water quality impacts would occur.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>accommodate increases in runoff. Improvements could result in construction-related impacts to receiving waters. No significant water quality and hydrology impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Impacts would be avoided by implementation of a project-specific SWPPP with BMPs. • All new facilities construction would include sustainable designs (i.e., Low Impact Development [LID], energy efficient design, and integrated layout). • Construction and postconstruction activities would adhere to Federal, state, and local standards, as well as the measures specified in Section 5.5. By successfully complying with these measures, runoff during construction and postconstruction operations would be minimized and treated 	<p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>	<p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		through LID, site design, and/or structural BMPs mandated by these measures.		
3.6 Noise	<p><u>Impacts:</u> No new construction or operational noise sources would be generated; therefore, ambient noise levels would not be affected and no noise impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Under Alternative 1, demolition of existing facilities and infrastructure and the construction and operations of new facilities and infrastructure would add to the noise levels of the existing activities on SSTC-South and the area's ambient noise levels, which are characteristic of the urban environment and transportation activities (port and aviation) of the area. Alternative 1 would include the demolition of Building 99 in 2015–2016, which would generate noise from concrete drilling and sawing, blasting, concrete breaking, stockpiling, and truck hauling off-site. Temporary worst-case 8-hour averaged construction noise would be approximately 62 dBA at the Coronado Cays and 60 dBA at Imperial Beach. U.S. Navy and City of Imperial Beach regulations do not limit decibel levels of construction noise; however, the City of</p>	<p><u>Impacts:</u> Alternative 2 would retain Building 99; therefore, the associated demolition and hauling noise described for Alternative 1 would not occur. All other construction and operation noise would be similar to Alternative 1. Therefore, Alternative 2 would not have a significant impact to noise.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To reduce noise impacts associated with project-related demolition activities, a detailed demolition plan would be prepared including public notification and complaint protocol.</p>	<p><u>Impacts:</u> Under Alternative 3, construction and operations of new facilities would be similar to Alternatives 1 and 2. Alternative 3 would include retention of Building 99 generating noise levels similar to Alternative 2. Construction and operations of Alternative 3 would not result in any significant noise impacts at NAB Coronado or NASNI. Therefore, Alternative 3 would not have a significant impact to noise.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To reduce noise impacts associated with project-related demolition activities, a detailed demolition plan would be prepared including public notification and complaint protocol.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Coronado (Coronado Cays) limits daytime construction noise levels to 75 dBA L_{eq}. The City of Imperial Beach prohibits construction noise at night between 10:00 PM and 7:00 AM. Nighttime construction is not likely to occur.</p> <p>Operation of Alternative 1 (i.e., facilities use and vehicle traffic) would increase ambient noise levels on SSTC-South; however, the increase would not result in a substantial increase in ambient noise levels; result in incompatible land use; or violate Federal, Navy, state, regional, or local noise standards or requirements. Therefore, Alternative 1 would not have a significant impact to noise.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> To reduce noise impacts associated with project-related demolition activities, blasting activities for Building 99 would be performed by a licensed</p>		

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
3.7 Biological Resources	<p><u>Impacts:</u> No impacts to biological resources.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 1 would result in permanent direct impacts to 100 percent (166.7 acres) of the plant communities and cover types within the Proposed Action footprint. An additional 10.1 acres would be temporarily impacted through utility easements, of which 0.02 acre are jurisdictional waters. There is 0.01 acre of San Diego fairy shrimp (<i>Branchinecta sandiegonensis</i>)-occupied vernal pool habitat that may utilize trenchless technology construction to avoid permanent direct impacts to vernal pool 10 from the proposed water line. Additionally, there would be a loss of 0.15 acre of critical habitat for the Western Snowy Plover (<i>Charadrius nivosus nivosus</i>) from construction of the proposed entry control point and supporting road improvements. Alternative 1 has the potential to result in significant impacts to</p>	<p><u>Impacts:</u> Alternative 2 would result in permanent direct impacts to 100 percent (162.1 acres) of the plant communities and cover types within the Proposed Action footprint. An additional 10.1 acres would be temporarily impacted through utility easements, of which 0.02 acre are jurisdictional waters. There is 0.01 acre of San Diego fairy shrimp-occupied vernal pool habitat that may utilize trenchless technology construction to avoid permanent direct impacts to vernal pool 10 from the proposed water line. Additionally, there would be a loss of 0.15 acre of critical habitat for the Western Snowy Plover from construction of the proposed entry control point and supporting road improvements. Alternative 2 has the potential to result in significant impacts to biological resources due to a loss of critical habitat for the Western Snowy Plover. Alternative 2 will have no</p>	<p><u>Impacts:</u> Alternative 3 would result in permanent direct impacts to 100 percent (171.05 acres) of the plant communities and cover types within the Proposed Action footprint. An additional 10.1 acres would be temporarily impacted through utility easements, of which 0.02 acre are jurisdictional waters. There is 0.01 acre of San Diego fairy shrimp-occupied vernal pool habitat that may utilize trenchless technology construction to avoid permanent direct impacts to vernal pool 10 from the proposed water line. Additionally, there would be a loss of 0.15 acre of critical habitat for the Western Snowy Plover from construction of the proposed entry control point and supporting road improvements. Alternative 3 has the potential to result in significant impacts to biological resources due to a loss of critical habitat for the Western Snowy Plover. Since</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>biological resources due to a loss of critical habitat for the Western Snowy Plover. Alternative 1 will have no effect on the following species: California Least Tern (<i>Sternula antillarum browni</i>), Least Bell's Vireo (<i>Vireo bellii pusillus</i>), Coastal California Gnatcatcher (<i>Polioptila californica californica</i>), and Pacific pocket mouse (<i>Perognathus longimembris pacificus</i>). Additionally, there are no anticipated adverse effects to any nonfederally listed rare or sensitive wildlife species, or wildlife corridors,</p> <p><u>Mitigation Measures:</u> Consultation with USFWS will occur regarding potential impacts to listed species and critical habitat. All measures and provisions of the Final USFWS-issued BO would be implemented.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Sections 5.7.1 through 5.7.3.</p>	<p>effect on the following species: California Least Tern, Least Bell's Vireo, Coastal California Gnatcatcher, and Pacific pocket mouse. Additionally, there are no anticipated adverse effects to any nonfederally listed rare or sensitive wildlife species, or wildlife corridors,</p> <p><u>Mitigation Measures:</u> Consultation with USFWS will occur regarding potential impacts to listed species and critical habitat. All measures and provisions of the Final USFWS-issued BO would be implemented.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Sections 5.7.1 through 5.7.3.</p>	<p>no sensitive biological resources occur within or adjacent to the project areas on NASNI or NAB Coronado, there would be no significant impacts to biological resources. Alternative 3 will have no effect on the following species: California Least Tern, Least Bell's Vireo, Coastal California Gnatcatcher, and Pacific pocket mouse. Additionally, there are no anticipated adverse effects to any nonfederally listed rare or sensitive wildlife species, or wildlife corridors,</p> <p><u>Mitigation Measures:</u> Consultation with USFWS will occur regarding potential impacts to listed species and critical habitat. All measures and provisions of the Final USFWS-issued BO would be implemented.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Sections 5.7.1 through 5.7.3.</p>
3.8 Cultural Resources	<p><u>Impacts:</u> No effects to cultural resources.</p> <p><u>Mitigation Measures:</u> None.</p>	<p><u>Impacts:</u> Demolition of Building 99, a contributor to the NRHP-eligible Fort Emory Coastal Defense Historic District would constitute an</p>	<p><u>Impacts:</u> The proposed ground-disturbing off-site traffic, access, and utilities improvements have the potential to impact cultural</p>	<p><u>Impacts:</u> The proposed ground-disturbing off-site traffic, access, and utilities improvements have the potential to impact cultural</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
	<p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p>adverse effect to this historic property.</p> <p>The proposed ground-disturbing off-site traffic, access, and utilities improvements have the potential to impact cultural resources.</p> <p><u>Mitigation Measures:</u> In accordance with 36 C.F.R. 800.6, resolution of the adverse effect to the Fort Emory Coastal Defense Historic District would be defined during the pending Section 106 consultation with SHPO, the Advisory Council on Historic Preservation (should they chose to participate), and other consulting parties through development and execution of a memorandum of agreement. Actions stipulated in the MOA for resolving the adverse effect would be required to be completed in advance of the initiation of the undertaking activities creating the adverse effect.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Alternative 1 would be developed in compliance</p>	<p>resources.</p> <p><u>Mitigation Measures:</u> Mitigation measures would not be required under a finding of no adverse effect.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Alternative 2 would be developed in compliance with NHPA Section 106 under the NBC PA, as implemented through the NBC ICRMP.</p> <p>Potential impacts to cultural resources from proposed ground-disturbing off-site traffic, access, and utilities improvements would be addressed through the following measures:</p> <p>Cultural-1: The Navy would coordinate with State Parks and Caltrans for cultural resources surveys for the proposed ground-disturbing off-site traffic and access improvements.</p> <p>Cultural-2: A Monitoring and Discovery Plan would be prepared and implemented prior to the start of ground-disturbing construction activities.</p> <p>Cultural-3: Cultural resources</p>	<p>resources.</p> <p><u>Mitigation Measures:</u> Mitigation measures would not be required under a finding of no adverse effect.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Alternative 3 would be developed in compliance with NHPA Section 106 under the NBC PA, as implemented through the NBC ICRMP.</p> <p>Potential impacts to cultural resources from proposed ground-disturbing off-site traffic, access, and utilities improvements would be addressed through the following measures:</p> <p>Cultural-1: The Navy would coordinate with State Parks and Caltrans for cultural resources surveys for the proposed ground-disturbing off-site traffic and access improvements.</p> <p>Cultural-2: A Monitoring and Discovery Plan would be prepared and implemented prior to the start of ground-disturbing construction activities.</p> <p>Cultural-3: Cultural resources</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>with NHPA Section 106 under the NBC PA, as implemented through the NBC ICRMP.</p> <p>Potential impacts to cultural resources from proposed ground-disturbing off-site traffic, access, and utilities improvements would be addressed through the following measures:</p> <p>Cultural-1: The Navy would coordinate with State Parks and Caltrans for cultural resources surveys for the proposed ground-disturbing off-site traffic and access improvements.</p> <p>Cultural-2: A Monitoring and Discovery Plan would be prepared and implemented prior to the start of ground-disturbing construction activities.</p> <p>Cultural-3: Cultural resources monitoring would be required during mechanical excavation associated with the off-site traffic, access, and utilities improvements.</p> <p>Cultural-4: The accidental discovery of human remains during mechanical</p>	<p>monitoring would be required during mechanical excavation associated with the off-site traffic, access, and utilities improvements.</p> <p>Cultural-4: The accidental discovery of human remains during mechanical excavation would be addressed under NAGPRA for remains found on military Federal lands, and through consultation with the NAHC for remains found on nonmilitary Federal lands and non-Federal lands.</p>	<p>monitoring would be required during mechanical excavation associated with the off-site traffic, access, and utilities improvements.</p> <p>Cultural-4: The accidental discovery of human remains during mechanical excavation would be addressed under NAGPRA for remains found on military Federal lands, and through consultation with the NAHC for remains found on nonmilitary Federal lands and non-Federal lands.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		excavation would be addressed under NAGPRA for remains found on military Federal lands, and through consultation with the NAHC for remains found on nonmilitary Federal lands and non-Federal lands.		
3.9 Traffic and Circulation	<p><u>Impacts:</u> <u>Construction</u> No significant impacts would occur at the study intersections.</p> <p><u>Year 2024</u> 1 CVN: No significant impacts would occur at the study intersections.</p> <p>2 CVNs: No significant impacts would occur at the study intersections.</p> <p>3 CVNs: An analysis of three-CVN conditions was not performed; however, the staggered work hours required when three CVNs are in port results in conditions similar to or better than the results for two-CVN conditions. As two-CVN conditions have no significant impacts at the study intersections, it can be concluded that no</p>	<p><u>Impacts:</u> <u>Construction</u> The study intersections that would have a significant impact during construction due to the addition of Alternative 1 for a “North Only” scenario are shown in Table 3.9-7 and summarized as follows:</p> <ul style="list-style-type: none"> • Year 2015, 6 locations • Year 2016, 6 locations • Year 2017, 7 locations • Year 2018, 8 locations • Year 2019, 12 locations • Year 2020, 12 locations • Year 2021, 12 locations • Year 2022, 12 locations • Year 2023, 12 locations <p>The number of study intersections that would have a significant impact during construction due to the addition of Alternative 1 for a “Construction North, Operations South” scenario is shown in Table 3.9-8 and summarized as follows:</p> <ul style="list-style-type: none"> • Year 2015, 6 locations 	<p><u>Impacts:</u> <u>Construction</u> The study intersections that would have a significant impact during construction due to the addition of Alternative 2 for a “North only” scenario are shown in Table 3.9-11. The number of study intersections that would have a significant impact during construction due to the addition of Alternative 2 for a “Construction North, Operations South” scenario is shown in Table 3.9-12. The number of intersections impacted by construction traffic for Alternative 2 would be the same as described above for Alternative 1, albeit to a more severe degree.</p> <p><u>Postconstruction Year 2024</u> The significant impacts at the study intersections for Alternative 2 would be identical to the findings for Alternative 1.</p>	<p><u>Impacts:</u> <u>Construction</u> The study intersections that would have a significant impact during construction due to the addition of Alternative 3 for a “North Only” scenario are shown in Table 3.9-13 and summarized as follows:</p> <ul style="list-style-type: none"> • Year 2015, 6 locations • Year 2016, 6 locations • Year 2017, 9 locations • Year 2018, 10 locations • Year 2019, 14 locations • Year 2020, 16 locations • Year 2021, 15 locations • Year 2022, 13 locations • Year 2023, 14 locations <p>The number of study intersections that would have a significant impact during construction due to the addition of Alternative 3 for a “Construction North, Operations South” scenario is shown in Table 3.9-14 and summarized as follows:</p> <ul style="list-style-type: none"> • Year 2015, 6 locations

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
	<p>significant impacts would occur at the study intersections while three CVNs are in port.</p> <p><u>Year 2040</u> The impacts for 2040 would be the same as for 2024.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<ul style="list-style-type: none"> • Year 2016, 6 locations • Year 2017, 9 locations • Year 2018, 10 locations • Year 2019, 14 locations • Year 2020, 16 locations • Year 2021, 16 locations • Year 2022, 14 locations • Year 2023, 14 locations <p><u>Postconstruction Year 2024</u> 1 CVN: Five of the study intersections would have a significant impact in Year 2024 due to the addition of Alternative 1:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 9th St & Palm Ave (SR-75) • 13th St & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) <p>2 CVNs: Six of the study intersections would have a significant impact in Year 2024 due to the addition of Alternative 1:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 7th St & Palm Ave 	<p><u>Postconstruction Year 2040</u> The significant impacts at the study intersections for Alternative 2 would be identical to the findings for Alternative 1.</p> <p><u>Construction Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u> The impact avoidance and minimization measures would be identical to those presented in Alternative 1.</p> <p><u>Postconstruction Years 2024 and 2040</u> <u>Mitigation Measures:</u> The mitigation measures would be identical to those presented in Alternative 1.</p> <p><u>Impact Avoidance and Minimization Measures:</u> The impact avoidance and minimization measures would be identical to those presented in Alternative 1.</p>	<ul style="list-style-type: none"> • Year 2016, 6 locations • Year 2017, 9 locations • Year 2018, 10 locations • Year 2019, 14 locations • Year 2020, 16 locations • Year 2021, 17 locations • Year 2022, 17 locations • Year 2023, 13 locations <p><u>Postconstruction Year 2024</u> 1 CVN: Five of the study intersections would have a significant impact in Year 2024 due to the addition of Alternative 3:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 9th St & Palm Ave (SR-75) • 13th St & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) <p>2 CVNs: Six of the study intersections would have a significant impact in Year 2024 due to the addition of Alternative 3:</p> <ul style="list-style-type: none"> • Orange Ave (SR-75) & Fourth St (SR-75) • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 9th St & Palm Ave (SR-75)

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>(SR-75)</p> <ul style="list-style-type: none"> • 9th St & Palm Ave (SR-75) • 13th St & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) <p>3 CVNs: An analysis of three-CVN conditions was not performed. With the staggered work hours required when three CVNs are in port, the results of the intersection analysis would be similar to or better than the results for two-CVN conditions.</p> <p><u>Postconstruction Year 2040</u></p> <p>1 CVN: Seven of the study intersections would have a significant impact in Year 2040 due to the addition of Alternative 1:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 7th Street & Palm Ave (SR-75) • 9th Street & Palm Ave (SR-75) • 13th Street & Palm Ave (SR-75) • Saturn Blvd/19th St & 		<ul style="list-style-type: none"> • 13th St & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) <p>3 CVNs: An analysis of three-CVN conditions was not performed. With the staggered work hours required when three CVNs are in port, the results of the intersection analysis would be similar to or better than the results for two-CVN conditions.</p> <p><u>Postconstruction Year 2040</u></p> <p>1 CVN: Seven of the study intersections would have a significant impact in Year 2040 due to the addition of Alternative 3:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 7th Street & Palm Ave (SR-75) • 9th Street & Palm Ave (SR-75) • 13th Street & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) • Palm Ave (SR-75) & I-5 SB Exit Ramp <p>2 CVNs:</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Palm Ave (SR-75)</p> <ul style="list-style-type: none"> • I-5 SB Exit Ramp & Palm Ave (SR-75) <p>2 CVNs: Eight of the study intersections would have a significant impact in Year 2040 due to the addition of Alternative 1:</p> <ul style="list-style-type: none"> • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Fiddler’s Cove Dwy • Silver Strand Blvd (SR-75) & Rainbow Dr • 7th Street & Palm Ave (SR-75) • 9th Street & Palm Ave (SR-75) • 13th Street & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) • I-5 SB Exit Ramp & Palm Ave (SR-75) <p>3 CVNs: An analysis of three-CVN conditions was not performed. With the staggered work hours required when three CVNs are in port, the results of the intersection analysis would be similar to or better than the results for two-CVN conditions.</p>		<p>Eight of the study intersections would have a significant impact due to the addition of Alternative 3:</p> <ul style="list-style-type: none"> • Orange Ave (SR-75) & Fourth St (SR-75) • Silver Strand Blvd (SR-75) & Tulagi Rd • Silver Strand Blvd (SR-75) & Rainbow Dr • 7th Street & Palm Ave (SR-75) • 9th Street & Palm Ave (SR-75) • 13th Street & Palm Ave (SR-75) • Saturn Blvd/19th St & Palm Ave (SR-75) • Palm Ave (SR-75) & I-5 SB Exit Ramp <p>3 CVNs: An analysis of three-CVN conditions was not performed. With the staggered work hours required when three CVNs are in port, the results of the intersection analysis would be similar to or better than the results for two-CVN conditions.</p> <p><u>Construction Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u></p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p><u>Construction Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u> t-1: Accelerate implementation of new entry control point at SSTC-South. t-2: Include construction management in the design aspect of the Proposed Action. t-3: Coordinate construction activity with NBC representatives to monitor daily activity levels. t-4: Schedule heavy periods of vehicle activity during non-peak hours. t-5: Encourage carpooling and staggered work hours for construction workers. t-6: Notify public stakeholders of times where abnormal construction activity would occur.</p> <p><u>Postconstruction Year 2024 Mitigation Measures:</u> T-1: Modification of signal operations at Silver Strand Blvd (SR-75) & Tulagi Rd T-2: Modification of eastbound approach configuration at Silver</p>		<p>The impact avoidance and minimization measures would be identical to those presented in Alternative 1.</p> <p><u>Postconstruction Year 2024 Mitigation Measures:</u> The mitigation measures would be identical to those presented in Alternative 1.</p> <p><u>Impact Avoidance and Minimization Measures:</u> t-1: Accelerate implementation of new entry control point at SSTC-South</p> <p>See Section 5.9 for more details on these measures.</p> <p><u>Postconstruction Year 2040 Mitigation Measures:</u> The mitigation measures would be identical to those presented in Alternative 1.</p> <p><u>Impact Avoidance and Minimization Measures:</u> t-1: Accelerate implementation of new entry control point at SSTC-South</p> <p>See Section 5.9 for more details on these measures.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Strand Blvd (SR-75) & Rainbow Drive T-3: Modification of northbound and southbound approach configurations at 9th Street & Palm Avenue (SR-75) T-4: Removal of east leg pedestrian crossing at 13th Street & Palm Avenue (SR-75) T-5: Addition of a second westbound left-turn lane at Saturn Blvd/19th St & Palm Ave (SR-75) T-6: Modification of southbound approach configuration at 7th St & Palm Ave (SR-75)</p> <p><u>Impact Avoidance and Minimization Measures:</u> t-1: Accelerate implementation of new entry control point at SSTC-South. See Section 5.9 for more details on these measures.</p> <p><u>Postconstruction Year 2040 Mitigation Measures:</u> T-1: Modification of signal operations at Silver Strand Blvd (SR-75) & Tulagi Rd. T-2: Modification of eastbound approach configuration at Silver Strand Blvd (SR-75) &</p>		

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>Rainbow Drive. T-3: Modification of northbound and southbound approach configurations at 9th Street & Palm Avenue (SR-75). T-4: Removal of east leg pedestrian crossing at 13th Street & Palm Avenue (SR-75). T-5: Addition of a second westbound left-turn lane at Saturn Blvd/19th St & Palm Ave (SR-75) T-6: Modification of southbound approach configuration at 7th St & Palm Ave (SR-75). T-7: Extend the southbound right-turn lanes at Palm Ave (SR-75) & I-5 SB Exit Ramp. T-8: Restriction of left turns out of Fiddler's Cove Driveway and Silver Strand Boulevard (SR-75).</p> <p><u>Impact Avoidance and Minimization Measures:</u> t-1: Accelerate implementation of new entry control point at SSTC-South. t-2: Monitor westbound left-turn delays and safety at the intersection of Silver Strand Blvd (SR-75) & Fiddler's Cove Dwy.</p>		

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
<p>3.10 Socioeconomics and Environmental Justice</p>	<p><u>Impacts:</u> No effects on socioeconomics. No disproportionately high and adverse human health and environmental effects on minority populations and low-income populations. No environmental health risks and safety risks that disproportionately affect children.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p>See Section 5.9 for more details on these measures.</p> <p><u>Impacts:</u> Effects of the Proposed Action on socioeconomics would be largely beneficial in terms of employment and economic output; no impacts are anticipated to population or housing. Temporary debris removal and construction-related traffic would not have a significant socioeconomic impact. Significant and unmitigable temporary traffic impacts may occur during the construction phase of the project along the transportation route between the Proposed Action footprint and I-5 in Imperial Beach. The U.S. census tracts along this corridor all contain populations with high proportions of minority and/or low-income residents. With the implementation of impact avoidance and minimization measures, however, these construction traffic impacts for Alternative 1 would not be high and adverse. Alternative 1 would not result in disproportionately high and adverse human</p>	<p><u>Impacts:</u> Similar to Alternative 1, with fewer impacts associated with debris removal. Alternative 2 would have no significant socioeconomic impacts, would not result in disproportionately high and adverse human health and environmental effects on minority populations and low-income populations, and would not result in environmental health risks and safety risks that disproportionately affect children.</p> <p><u>Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as for Alternative 1.</p>	<p><u>Impacts:</u> Similar to Alternative 1, with fewer impacts associated with debris removal. Alternative 3 would have no significant socioeconomic impacts, would not result in disproportionately high and adverse human health and environmental effects on minority populations and low-income populations, and would not result in environmental health risks and safety risks that disproportionately affect children.</p> <p><u>Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as for Alternative 1.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>health and environmental effects on minority populations and low-income populations. Similarly, these same census tracts contain a disproportionately large percentage of children, but with the implementation of impact avoidance and minimization measures construction traffic impacts for Alternative 1 would not present disproportionate risks to children. Alternative 1 would not result in environmental health risks and safety risks that disproportionately affect children.</p> <p><u>Mitigation Measures:</u> None</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Pedestrian routes along the transportation corridor would be maintained or temporary alternate routes provided and clearly marked during the construction of traffic and access improvements and during the Proposed Action construction phase when traffic would be heavier than under 		

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>normal conditions.</p> <ul style="list-style-type: none"> Residents in the affected census tracts would be notified of increased construction traffic via direct mail and road signage. Emergency public services and other appropriate law enforcement agencies would be notified of increased traffic and how construction traffic may affect emergency response times. 		
<p>3.11 Public Health and Safety</p>	<p><u>Impacts:</u> No change to any public health and safety concerns.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Demolition of Building 99 could include the use of small commercial explosives and/or diamond saws and drilling and hammering to break up the materials. The demolition debris would either be reused as part of the construction material for the Coastal Campus or removed to a local landfill. A detailed demolition plan would be prepared prior to demolition activities. Construction activities would be typical of military structures, would primarily occur within the footprint of SSTC-South, and would include all standard construction safety</p>	<p><u>Impacts:</u> Impacts would be similar to those for Alternative 1, except Alternative 2 would not include the demolition of Building 99. No significant public health and safety impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>	<p><u>Impacts:</u> Impacts would be the same as Alternative 2, except construction would also occur at NAB Coronado and NASNI. No significant public health and safety impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Same as Alternative 1.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>procedures. Construction activities would not result in a significant public health and safety impact. Post-construction use activities would pose no substantial risk to public health and safety.</p> <p>Terrorist activity, although unlikely, would not be considered a significant impact to public health and safety.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u></p> <ul style="list-style-type: none"> • Compliance with all standard construction safety procedures and applicable subparts of the Occupational Safety and Health Administration standards. • Preparation of a detailed demolition and lead/asbestos abatement plan. • Prior to the start of any demolition activities, contractors shall perform hazardous building materials surveys in order to identify and implement appropriate control measures during 		

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>demolition to protect human health (both worker and public) and the environment. Appropriate control measures may include preparation and implementation of demolition plans, lead compliance plans, and/or asbestos abatement plans, as necessary, depending upon the results of the hazardous materials building surveys.</p> <ul style="list-style-type: none"> Compliance with the NBC Installation Emergency Management Plan and its relevant supporting plans. 		
<p>3.12 Utilities and Public Services</p>	<p><u>Impacts:</u> No change to any utilities and public services would occur and therefore no impacts would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> <i>Water</i> The existing 16-inch/20-inch water line would adequately serve the water demand from Alternative 1 with both domestic and fire services. With the proposed water facility improvements, such as additional water storage tanks and booster pumps, there would not be a significant water supply impact. The existing 16-inch/20-inch water line may need to be relocated.</p>	<p><u>Impacts:</u> <i>Water</i> Similar to Alternative 1, the existing 16-inch/20-inch water line would adequately serve the water demand from Alternative 2 with both domestic and fire services. Also with the proposed water facility improvements, there would not be a significant water supply impact. The existing 16-inch/20-inch water line may need to be relocated.</p> <p><i>Wastewater</i> Similar to Alternative 1, with</p>	<p><u>Impacts:</u> <i>Water</i> Similar to Alternative 1, the existing 16-inch/20-inch water line would adequately serve the water demand from Alternative 3 with both domestic and fire services and with the proposed water facility improvements. There is adequate water at NAB Coronado and NASNI. There would not be a significant water supply impact with Alternative 3.</p> <p><i>Wastewater</i> Similar to Alternative 1, with</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p><i>Wastewater</i> The City of Imperial Beach's wastewater system may not have capacity to handle the additional peak morning flows. With the installation of the required wastewater improvements (upgrades to the City's system within Silver Strand Boulevard, Calia Avenue, and Seacoast Drive to Pump Station 5 and within Imperial Beach Boulevard from 4th Street to East Lane), no significant wastewater impact would occur.</p> <p><i>Electrical</i> Electrical capacity upgrades would be needed to maintain the desired primary/back-up service. The use of renewable energy would be included. With the installation of the required electrical upgrades, there would be no significant impact.</p> <p><i>Natural Gas</i> New natural gas service would be connected to the line at the south gate entrance to serve the demand from Alternative 1. No significant natural gas</p>	<p>the installation of the required wastewater improvements, no significant wastewater impact would occur.</p> <p><i>Electrical</i> Similar to Alternative 1, electrical capacity upgrades would be needed to maintain the desired primary/back-up service. The use of renewable energy would be included. With the installation of the required electrical upgrades, there would be no significant impact.</p> <p><i>Natural Gas</i> New natural gas service would be connected to the line at the south gate entrance with no significant natural gas impacts.</p> <p><i>Communication</i> The site is served by AT&T and a new on-site Navy communication system would be constructed to serve the individual buildings within the Coastal Campus. No communication impacts would be expected for Alternative 2.</p> <p><i>Storm Water</i> Similar to Alternative 1, the Alternative 2 drainage design</p>	<p>the installation of the required wastewater improvements, no significant wastewater impact would occur. There is adequate wastewater capacity at NAB Coronado and NASNI.</p> <p><i>Electrical</i> Similar to Alternative 1, electrical capacity upgrades would be needed to maintain the desired primary/back-up service. The use of renewable energy would be included. With the installation of the required electrical upgrades, there would be no significant impact. There is adequate electrical capacity at NAB Coronado and NASNI.</p> <p><i>Natural Gas</i> New natural gas service would be connected to the line at the south gate entrance. There is adequate natural gas capacity at NAB Coronado and NASNI. There would be no significant natural gas impacts</p> <p><i>Communication</i> The site is served by AT&T and a new private on-site Navy communication system would be constructed to serve the individual buildings</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>impacts would be expected.</p> <p><i>Communication</i> The site is served by AT&T and a new on-site Navy communication system would be constructed to serve the individual buildings within the Coastal Campus. No communication impacts would be expected.</p> <p><i>Storm Water</i> The Alternative 1 drainage design would maintain existing runoff patterns to the maximum extent practicable, and retain all runoff on-site (zero discharge) for treatment. Runoff would be directed to different types of LID storm water treatment and storage facilities to remove various pollutants from the runoff and to store storm water for on-site infiltration and evaporation. These design features would reduce runoff volume, capture runoff pollutants on-site, provide groundwater recharge, and offer a supplemental resource for irrigation and/or graywater use in facility buildings. No</p>	<p>would maintain existing runoff patterns to the maximum extent practicable, and retain all runoff on-site (zero discharge) for treatment. Runoff would be directed to different types of LID storm water treatment and storage facilities to remove various pollutants from the runoff and to store storm water for on-site infiltration and evaporation. These design features would reduce runoff volume, capture runoff pollutants on-site, provide groundwater recharge, and offer a supplemental resource for irrigation and/or graywater use in facility buildings. No significant storm water impacts would result.</p> <p><u>Public Services</u> <i>Police</i> Appropriate safety and security lighting and security fencing would be installed where necessary. No significant police services impact would result.</p> <p><i>Fire</i> Construction of all facilities would meet all applicable fire codes and regulations. Project design would include appropriate and required fire</p>	<p>within the Coastal Campus. There is adequate communication service at NAB Coronado and NASNI. No communication impacts would be expected for Alternative 3.</p> <p><i>Storm Water</i> Storm water impacts for Alternative 3 would be the same as Alternative 1 on SSTC-South. The existing storm water systems that served the previous development at NAB Coronado and NASNI would adequately handle P-904, P-912, and P-965 and a portion of P-870, respectively. There would not be a significant storm water impact at SSTC-South, NAB Coronado, or NASNI as a result of development of Alternative 3.</p> <p><u>Public Services</u> <i>Police</i> Appropriate safety and security lighting and security fencing would be installed where necessary. No significant police services impact would result.</p> <p><i>Fire</i> Construction of all facilities would meet all applicable fire codes and regulations.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>significant storm water impact would occur.</p> <p><u>Public Services</u> <i>Police</i> Appropriate safety and security lighting and security fencing would be installed where necessary. No significant police services impact would result.</p> <p><i>Fire</i> Construction of all facilities would meet all applicable fire codes and regulations. Project design would include appropriate and required fire safety design such as sprinkler systems, fire flow requirements, and all other necessary fire safety features. No significant fire services impact would result.</p> <p><i>Solid Waste</i> Alternative 1 would be compliant with EO 13514 and EO 13423 specific to waste diversion, and with the SSWP and Commander, Navy Region Southwest Instruction 11350.1B requirements regarding C&D debris. C&D debris would be diverted from the landfill</p>	<p>safety design such as sprinkler systems, fire flow requirements, and all other necessary fire safety features. No significant fire services impact would result.</p> <p><i>Solid Waste</i> Alternative 2 would be compliant with EO 13514 and EO 13423 specific to waste diversion, and with the SSWP and Commander, Navy Region Southwest Instruction 11350.1B requirements regarding C&D debris. C&D debris would be diverted from the landfill waste stream to the extent feasible. Materials would either be recycled or reused through a variety of potential measures dependent on type and volume of material. Methods could include a temporary on-site concrete batch plant and/or processing at an off-site industrial recycling facility. No significant solid waste impact would result.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p>Project design would include appropriate and required fire safety design such as sprinkler systems, fire flow requirements, and all other necessary fire safety features. No significant fire services impact would result.</p> <p><i>Solid Waste</i> Alternative 3 would be compliant with EO 13514 and EO 13423 specific to waste diversion, and with the SSWP and Commander, Navy Region Southwest Instruction 11350.1B requirements regarding C&D debris. C&D debris would be diverted from the landfill waste stream to the extent feasible. Materials would either be recycled or reused through a variety of potential measures dependent on type and volume of material. No significant solid waste impact would result.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>waste stream to the extent feasible. Materials would either be recycled or reused through a variety of potential measures dependent on type and volume of material. No significant solid waste impact would result.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>		
<p>3.13 Coastal Uses and Resources</p>	<p><u>Impacts:</u> No effects on existing coastal resources; no changes to public access, views, or any coastal resources.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Construction effects on water quality would be temporary and not significant. Alternative 1 would not change public access and therefore no impacts to public access would result.</p> <p>Alternative 1 would be visually compatible with the existing building heights (up to 45 feet tall), with the exception of a paraloft structure that could be up to 120 feet tall. Existing visual setting would change, but Alternative 1 would not obstruct any scenic public viewsheds. No significant visual impact would result. No significant</p>	<p><u>Impacts:</u> Impacts would be similar to those for Alternative 1. No significant impacts to coastal uses or resources are anticipated with the implementation of Alternative 2.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Implementation of the water quality measures specified in Section 5.5 and summarized in Alternative 1.</p>	<p><u>Impacts:</u> Construction effects on water quality would be temporary and not significant. Alternative 3 would not change public access and therefore no impacts to public access would result. Alternative 3 would be visually compatible with the existing building heights (up to 45 feet tall), with the exception of a paraloft structure on the SSTC-South portion of the footprint that could be up to 120 feet tall. Existing visual setting would change, but Alternative 3 would not obstruct any scenic public viewsheds. No significant visual impact would result. No significant impacts to coastal uses or</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>impacts to coastal uses or resources are anticipated with the implementation of Alternative 1.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Implementation of the water quality measures specified in Section 5.5 and summarized below:</p> <ul style="list-style-type: none"> • Implement project-specific SWPPP with BMPs relative to site-specific needs and conditions. • Include sustainable designs (i.e., LID, energy efficient design, and integrated layout). 		<p>resources are anticipated with the implementation of Alternative 3.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Implementation of the water quality measures specified in Section 5.5 and summarized in Alternative 1.</p>
3.14 Aesthetics	<p><u>Impacts:</u> No effect on aesthetics.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> None.</p>	<p><u>Impacts:</u> Alternative 1 would modify viewsheds from SR-75, the Bayshore Bikeway, the Coronado Cays, and Silver Strand State Beach. It would create a more intense visual appearance, including increased nighttime lighting conditions, primarily from southbound SR-75 approaching the north gated entry control point. Viewshed modifications are not anticipated to be</p>	<p><u>Impacts:</u> Similar to Alternative 1, Alternative 2 would modify viewsheds from SR-75, the Bayshore Bikeway, the Coronado Cays, and Silver Strand State Beach. The Alternative 2 appearance would create a more intense visual appearance, including increased nighttime lighting conditions. Viewshed modifications would be similar to Alternative 1 and the modifications are not anticipated to be perceived</p>	<p><u>Impacts:</u> Similar to Alternative 1, Alternative 3 would modify viewsheds from SR-75, the Bayshore Bikeway, the Coronado Cays, and Silver Strand State Beach. The Alternative 3 appearance would create a more intense visual appearance, including increased nighttime lighting conditions. Viewshed modifications would be similar to Alternative 1 and the modifications are not anticipated to be perceived</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		<p>perceived as substantial, dramatic, adverse, or controversial; no significant aesthetic impact would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Design of the buildings would complement the appearance of surrounding areas by including:</p> <ul style="list-style-type: none"> • Context-sensitive architectural treatments; applied consistently throughout the development; • Low-reflectivity building materials in natural, earth-tone colors; • Shielding of permanent outdoor lighting installed at proposed facilities that limit light trespass and ambient light pollution to achieve dark-sky compliance to the extent possible. (Additional methods to reduce light pollution [e.g., dusk-to-dawn sensor activation, low-lumen or limited-spectrum lighting] applied as possible; light poles and light placement at lowest 	<p>as substantial, dramatic, adverse, or controversial; no significant aesthetic impact would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Design of the buildings would complement the appearance of surrounding areas and include the same measures discussed under Alternative 1.</p>	<p>as substantial, dramatic, adverse, or controversial; no significant aesthetic impact would occur. Modification to views at NAB Coronado and NASNI would be insubstantial as those base locations are currently characterized as nearly built out. Addition of one to two facilities at these locations would not be a change in character or perceptible to the average viewer, and no significant aesthetic impact would occur.</p> <p><u>Mitigation Measures:</u> None.</p> <p><u>Impact Avoidance and Minimization Measures:</u> Design of the buildings would complement the appearance of surrounding areas and include the same measures discussed under Alternative 1.</p>

Resource	No Action Alternative	Alternative 1	Alternative 2	Alternative 3
		height practicable [considering security constraints]); and <ul style="list-style-type: none"> Context- and water-sensitive landscape treatments, including visual buffers consisting of earthen berms, vegetated buffers, screening trees, and right-of-way landscape improvements along public-facing adjacencies; to be approved (by NBC NRO staff). 		

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**Table ES-4
Mitigation Identification and Implementation**

Mitigation Measure	Benefit	Evaluation Criteria	Implementation	Responsible Command	Date Implemented
Biological Resources					
Implement provisions stipulated in the USFWS-issued BO. Compliance with NBC INRMP	Avoid and minimize impacts to, and incidental take of, threatened and endangered species and their habitats.	Yearly review of BO requirements. Reduction of populations, habitats, and/or behavioral changes that could jeopardize species.	Continue monitoring the numbers of species, quality of habitat, and potential behavioral changes.	Host or Tenant Command, as appropriate.	Ongoing.
Cultural Resources					
Compliance with NHPA Section 106 under the NBC Programmatic Agreement, as implemented through the NBC ICRMP.	Reduce or mitigate for potential effects to archaeological and historic resources.	Minimization of potential impacts to cultural resources from demolition and construction.	Implementation of measures in the ICRMP and PA and consultation with SHPO, ACHP, Indian Tribes, and other parties.	Host or Tenant Command, as appropriate.	Prior to construction.
Traffic and Circulation					
Implement the following measures by 2024. <ul style="list-style-type: none"> • Modification of signal operations at Silver Strand Blvd (SR-75) & Tulagi Rd • Modification of eastbound approach configuration at Silver Strand Blvd (SR-75) & Rainbow Dr • Modification of northbound and southbound approach configurations at 9th St & Palm Ave (SR-75) 	Reduce intersection traffic congestion and delays.	The post-implementation level of service for the subject intersections.	Implementation of the mitigation measures prior to the threshold year of need, either 2024 or 2040.	Host or Tenant Command, as appropriate and Caltrans and the City of Imperial Beach.	Prior to 2024 and 2040.

Mitigation Measure	Benefit	Evaluation Criteria	Implementation	Responsible Command	Date Implemented
<ul style="list-style-type: none"> • Removal of east leg pedestrian crossing at 13th St & Palm Ave (SR-75) • Addition of a second westbound left-turn lane at Saturn Blvd/19th St & Palm Ave (SR-75) • Modification of southbound approach configuration at 7th St & Palm Ave (SR-75) <p>Implement the following measures by 2040.</p> <ul style="list-style-type: none"> • Extend the southbound right-turn lanes at Palm Ave (SR-75) & I-5 SB Exit Ramp. • Restriction of left turns out of Fiddler's Cove Driveway and Silver Strand Blvd (SR-75). 					

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Attachment 3

REVIEW AND COMMENT ON THE DRAFT ENVIRONMENTAL IMPACT STATEMENT PREPARED FOR THE NAVAL BASE CORONADO COASTAL CAMPUS

ISSUE: Whether to comment on the Draft Environmental Impact Statement (EIS) prepared for the Naval Base Coronado Coastal Campus.

RECOMMENDATION: Approve the transmittal of the attached letter (attachment 1) on the Draft EIS along with any additional comments suggested by Councilmembers at this meeting or as suggested by the Congestion Relief Working Group of the Coronado Transportation Commission as outlined in the attached memorandum (attachment 2).

FISCAL IMPACT: No impacts have been identified.

STRATEGIC PLAN IMPACT: The proposed project will affect several quality of life factors identified within the Strategic Plan. The opportunity to comment on the project and cause a project change may reduce impacts to Coronado and its residents.

PUBLIC NOTICE: The Navy provided public notice of the availability to review and comment on the Draft Environmental Impact Statement. The public review period lasts 60 days, commencing on July 25, and will conclude September 22. The Navy also held a public "information open house" on the Draft document on August 14 from 5 to 8 p.m. at the Winn Room at Coronado Public Library and August 13 from 5 to 8 p.m. at the Marina Vista Community Center in Imperial Beach.

BACKGROUND: The City has received a Notice of a Draft Environmental Impact Statement prepared for the Naval Base Coronado Coastal Campus. The project involves (1) consolidation of Naval Special Warfare Command facilities to one location at the Southern Silver Strand Training Complex, commonly known as the Naval Radio Receiving Station site; (2) design and construction of logistical support buildings, equipment use (and equipment maintenance) training facilities (including an approximately 50' long x 80' wide by 120' tall parachute drying tower or paraloft), classroom and tactical skills instruction buildings, storage, and administrative facilities totaling approximately 1.5 million square feet (p.3.3-15); infrastructure; utilities; fencing; roads; and parking; and (3) construction of a new entry control point providing immediate access to SSTC-South from SR-75. The project also includes a food service facility, fuel dispensing facility, and a mini-mart-type of store. The Navy asserts that the project will not result in increased personnel, rather the existing personnel at NAB would be shifted to the new Coastal Campus. The project (including demolition and construction) is anticipated to take approximately 10 years, commencing in 2015 and concluding in 2024.

The Draft EIS analyzes the anticipated environmental impacts associated with the project in the following subjects: Land Use and Recreation; Geology and Soils; Air Quality; Hazardous Materials and Waste; Water Quality and Hydrology; Noise; Biological Resources; Cultural Resources; Traffic and Circulation; Socioeconomics and Environmental Justice; Public Health and Safety; Utilities and Public Services; Coastal Uses and Resources; and Aesthetics.

The purpose of this agenda item is for the City Council to review, discuss, and receive public comment regarding the Draft EIS prepared for the project. The draft comment letter attached to the report (attachment 1) identifies comments/concerns related to the environmental document. The Council should review the draft letter and make changes and/or additions as deemed appropriate. An additional comment letter from the Congestion Relief Working Group of the Coronado Transportation Commission is also attached (attachment 2). The Council may want to add some of this group's comments to the City's official letter.

Once the comment period is concluded, a Final EIS will be prepared for the project. The Final EIS should address all the public comments received by providing clarifications, modifications, additional data, and environmental analyses as needed. The Final EIS will be released for the public to review and no earlier than 30 days after, the decision to implement one of the project alternatives analyzed in the EIS is made. The Assistant Secretary of the Navy, Energy, Installations, and Environment has authority to render the final decision. The final decision is documented in a Record of Decision (ROD) issued by the Secretary of the Navy (Page 1-11).

ANALYSIS: In general, the Draft document concludes the project will result in significant adverse direct, indirect or cumulative impacts, and mitigation is needed for the proposed action in the following areas: Biological Resources, Cultural Resources, and Traffic and Circulation. A summary of these impacts and anticipated mitigation efforts are as follows:

1. Biological Impacts: The Biological impacts will require consultation with U.S. Fish & Wildlife Service (USFWS) regarding potential impacts to listed species and critical habitat and all measures and provisions of the Final USFWS issued Biological Opinion (BO) would be implemented.
2. Cultural Impacts: Cultural impacts will require resolution of the adverse effect to the Fort Emory Coastal Defense Historic District, which will be defined during Section 106 consultation with the State Historic Preservation Office (SHPO), and implementation of stipulations outlined in the Memorandum of Agreement (MOA) will be developed prior to activities creating the adverse effect.
3. Traffic and Circulation Impacts: The Traffic and Circulation Impacts will be short-term and long-term. The EIS notes there will be construction impacts to numerous signalized intersections in Years 2015-2023 during construction; there will be post-construction impacts to several signalized intersections in Year 2024 due to project implementation; and post-construction impacts in Year 2040 due to project implementation.

Specifically, page ES-44 of the EIS states, "*Significant and unmitigable temporary traffic impacts may occur during the construction phase of the project along the transportation route between the Proposed Action footprint and I-5 in Imperial Beach.*" To address one impact associated with project construction, temporary northern access to the site would be provided along SR-75 and Hooper Boulevard in Coronado. The EIS indicates, "*Improvements to the temporary northern access could include a traffic signal, a left-turn lane on northbound SR-75 into the site, and a right-turn lane on southbound SR-75 into the site. An acceleration and deceleration lane may also be required.*" Primary, permanent access to the project area will be provided at this same location along with a new, entry

control point. The entry control point would “involve construction of a base main gate with 600 square foot sentry house and anti-terrorism protection improvements including new traffic lanes for approach, queue, vehicle inspection, denial and exit, plus reinforced fencing, a wall, traffic barrier systems, pedestrian gates, security office, utilities, paving and site improvements and parking.” (See Pages 2-22-23 and Figure 2-6 for conceptual site plan of intersection and entry control point. See ES-41 for detailed mitigation measures for both during and post construction).

In addition to the impacts mentioned, there are several “*Impact Avoidance and Minimization Measures*” proposed as part of the project for most of the environmental categories identified above. For traffic and circulation, they include the following: 1) accelerate implementation of new entry control point at SSTC-South; 2) include construction management in the design aspect of the Proposed Action; 3) coordinate construction activity with NBC representatives to monitor daily activity levels; 4) schedule heavy periods of vehicle activity during non-peak hours; 5) encourage carpooling and staggered work hours for construction workers; and 6) notify public stakeholders of times where abnormal construction activity would occur.

For other environmental areas, the *Impact Avoidance and Minimization Measures* are in large part standard operating procedures for construction projects. Measures include storm water and water quality procedures; standard Best Management Practices (BMPs); implementation of OSHA standards; washing down project site/trucks to minimize dust; limiting hours/days/months of construction; and providing alternative pedestrian and bicycle routes as necessary to name a few. Upgrades to existing wastewater, electrical, natural gas, communication, and storm water infrastructure will occur with the proposed project avoiding significant impacts related to these utilities and services.

Additional Environmental Concerns and Comments:

The attached letter identifies areas where staff has determined insufficient information has been provided, more environmental analysis is warranted, or there is disagreement with the conclusions within the EIS. Specific areas (the main areas) that should be analyzed further include 1) Traffic and Circulation; 2) Utilities and Services; 3) Aesthetics; 4) Noise and 5) Cumulative Impacts. A summary of the comments is as follows:

A summary of the comments is as follows:

1. Traffic and Circulation – Staff has identified the need to complete the north entrance; the need for changes to traffic intersections; a lack of information on where to queue traffic; the need to develop a traffic reduction/improvement program—including options for alternative transportation; and the need to offer a shuttle service between SSTC-South and NAB. Additionally, City staff expects the Navy to make traffic designs consistent with the aesthetics of the nearby State Park and to keep the scenic view open. Mitigation will also be needed at the intersection of Fourth and Orange based on anticipated increased traffic, especially during peak afternoon traffic times.

2. Utilities and Services – Unless a current contract exists with Imperial Beach for Wastewater, there will be no expansion of another agency’s services into Coronado. LAFCO must verify and approve any extension of municipal services outside a city’s limits and sphere of influence. Additionally, City staff noted that the analysis of emergency services including dispatch and medical transport is inadequate.
3. Aesthetics – City staff needs confirmation that the impacts on views from SR-75 northbound from the City limits were considered because much more of the campus is visible from that direction. City staff strongly encourages the Navy with its construction to incorporate a unified design and harmonious relationship, specifically for roofing materials. It appears that light and glare of northbound vehicles coming from the elevated portion of the site will impact the residents of the Cays. City staff requests that the Navy analyze and mitigate, if needed. Any changes or mitigation should be consistent with the requirements associated with the Scenic Highway designation for SR 75.
4. Noise – No analysis of the City’s Noise Ordinance was considered in the EIS. City staff noted throughout the EIS how it demonstrated there will be increased activity at the Silver Strand Training South Complex, an area with traditionally minimal traffic and urban noise. City staff recommends that noise impacts be studied and reported regularly to the City and that the Navy takes steps to minimize the noise of construction, operation, and night-time combat activities consistent with the City’s Noise Ordinance.
5. Cumulative Impacts – City staff is concerned with the continued development of Navy Amphibious Base and the segmentation of environmental review. This leads to questions of the use of sites at NAB post development of Coastal Campus. City staff requests assurance from the Navy that additional uses will not be located in existing developed areas of NAB that will result in producing a net increase of population, traffic, and uses after the development of the Coastal Campus complex. The response that future development will be the subject of additional environmental review is inadequate, and again is segmenting the environmental review. Automatic mitigation measures such as redesigned entrances, pedestrian overpasses, improved public transportation internal to NAB should be put into place.

The City Council may elect to add, delete, or modify the letter for further environmental analysis as deemed necessary.

A copy of the Draft EIS is available for review at the Coronado Public Library, at the Community Development Department, and at the Navy’s following link www.NBCCoastalCampusEIS.com

Submitted by Community Development/McCaull

Attachment 1: Comment Letter on Draft EIS

Attachment 2: Memorandum from Congestion Relief Working Group of CTC

n:\all departments\staff reports - drafts\09-02 council meeting\eis for coastal campus\final report.doc

CM	ACM	AS	CA	CC	CD	EPD	F	G	L	P	PS	R
	TR	N/A	JNC	MLC	RAH	JN	MB	NA	NA	JF	CM	NA

DEPARTMENT OF TRANSPORTATION

DISTRICT 11, DIVISION OF PLANNING
4050 TAYLOR ST, M.S. 240
SAN DIEGO, CA 92110
PHONE (619) 688-6960
FAX (619) 688-4299
TTY 711
www.dot.ca.gov

Attachment 4

*Serious drought.
Help save water!*

September 8, 2014

11-SD-75
PM 17.6
Naval Base Coronado DEIS

Ms. Teresa Bresler
NBC Coastal Campus
2730 McKean Street, Bldg. 291
San Diego, CA 92136

Dear Ms. Bresler:

The California Department of Transportation (Caltrans) has reviewed the Draft Environmental Impact Study (DEIS) for the Naval Base Coronado project near State Route 75 (SR-75). Caltrans has the following comments:

Traffic:

- On Figure 7-2, for the SB-75 U-turn pocket at Hooper, the total length should be the deceleration length of 485 feet plus approach bay taper. This change should reflect on Synchro as well. Truck turning template should be ran for this U-turn. If it doesn't meet truck turning requirement, advisory Design Exception will be needed for this U-turn movement.
- At the proposed intersection of SR-75 and Hooper, left turn lanes will narrow down the median width so thrie beams and proper crash cushions will need to be installed.
- SR-75 and Hooper intersection requires an ICE evaluation. The ICE evaluation must be done during the Environmental Phase, and not Design Phase, because it helps to determine all possible alternatives for the intersection of SR-75 & Hooper.

Stewardship/Permits:

The proposed project site is within the Coastal Zone and coordination with the California Coastal Commission and/or the City of Coronado will be needed.

Storm Water Compliance:

For the Caltrans encroachment permit and project elements within the State Highway System, the project shall demonstrate compliance with applicable water quality regulations. This includes consistency with the following State Water Resources Control Board, National Pollutant Discharge Elimination System (NPDES) permits:

- Local MS4 permit regulations (Small Municipal Separate Storm Sewer Systems)
- Caltrans NPDES Permit 2012-011-DWQ (CAS 000003)

- Construction General Permit 2009-0009-DWQ (CAS 000002)

Biological Resources:

Nuttall's Lotus plants occur on roadway shoulder areas along State Route 75. Proposed changes to access the site from State Route 75 may require measures to avoid, minimize and/or mitigate impacts to the plant.

Visual Resources:

The visual review dated March 3, 2014 is still applicable to the project and Draft EIS. This review memo is attached for reference.

Right of Way (R/W):

The R/W for SR-75 within the Federal ownership is not access controlled. The original deed contains a provision that the Navy can construct any ingress/egress at their discretion.

General Comments:

Any work performed within Caltrans right-of-way (R/W) will require discretionary review and approval by Caltrans and an encroachment permit will be required for any work within the Caltrans R/W prior to construction. Current policy allows Highway Improvement Projects costing \$1 million or less to follow the Caltrans Encroachment Permit process. Highway Improvement Projects costing greater than \$1 million but less than \$3 million would be allowed to follow a streamlined project development process similar to the Caltrans Encroachment Permit process. In order to determine the appropriate permit processing of projects funded by others, it is recommended the concept and project approval for work to be done on the State Highway System be evaluated through the completion of a Permit Engineering Evaluation Report (PEER). A PEER should always be prepared, regardless of the cost of improvements, when new operating improvements are constructed by the permittee that become part of the State Highway System. These include but are not limited to, signalization, channelization, turn pockets, widening, realignment, public road connections, and bike paths and lanes. After approval of the PEER and necessary application and supporting documentation an encroachment permit can be issued.

In order to expedite the process for projects sponsored by a local agency or private developer, it is recommended a PEER be prepared and included in the Lead Agency's CEQA document. This will help expedite the Caltrans Encroachment Permit Review process. The PEER document forms and procedures can be found in the Caltrans Project Development Procedures Manual (PDPM).

<http://www.dot.ca.gov/hq/oppd/pdpm/pdpmn.htm>

[http://www.dot.ca.gov/hq/traffops/developserv/permits/pdf/forms/PEER_\(TR-0112\).pdf](http://www.dot.ca.gov/hq/traffops/developserv/permits/pdf/forms/PEER_(TR-0112).pdf)

As part of the encroachment permit process, the applicant must provide an approved final environmental document including the California Environmental Quality Act (CEQA) determination addressing any environmental impacts within the Caltrans' R/W, and any corresponding technical studies. If these materials are not included with the encroachment permit application, the applicant will be required to acquire and provide these to Caltrans before the permit application will be accepted. Identification of avoidance and/or mitigation measures will be a condition of the encroachment permit approval as well as procurement of any necessary

Ms. Teresa Bresler
September 8, 2014
Page 3

regulatory and resource agency permits. Encroachment permit submittals that are incomplete can result in significant delays in permit approval.

Improvement plans for construction within State Highway R/W must include the appropriate engineering information consistent with the state code and signed and stamped by a professional engineer registered in the State of California. Caltrans Permit Manual contains a listing of typical information required for project plans. All design and construction must be in conformance with the Americans with Disabilities Act (ADA) requirements.

The Intersection Control Evaluation (ICE) policy checklist is required to evaluate any intersection modification on state facilities. Attached is the link to the ICE policy.
<http://dot.ca.gov/hq/traffops/liaisons/ice.html>

Additional information regarding encroachment permits may be obtained by contacting the Caltrans Permits Office at (619) 688-6158. Early coordination with Caltrans is strongly advised for all encroachment permits.

If you have any questions, please contact Roger Sanchez of the Development Review branch at (619) 688-6494.

Sincerely,

Signature on file

JACOB ARMSTRONG, Branch Chief
Development Review Branch



**STAFF REPORT
CITY OF IMPERIAL BEACH**

TO: HONORABLE MAYOR AND CITY COUNCIL

FROM: ANDY HALL, CITY MANAGER *AH*

MEETING DATE: SEPTEMBER 17, 2014

ORIGINATING DEPT.: COMMUNITY DEVELOPMENT DEPARTMENT
GREG WADE, ASSISTANT CITY MANAGER/COMMUNITY DEVELOPMENT DIRECTOR *GW*
JIM NAKAGAWA, AICP, CITY PLANNER *JN*
TYLER FOLTZ, SENIOR PLANNER *TF*

SUBJECT: REPORT: SADBERRY-PALM, LLC (APPLICANT); REVIEW DESIGN/FAÇADE AND COMPREHENSIVE SIGN PLAN REVISIONS FOR THE PROPOSED BREAKWATER DEVELOPMENT LOCATED ON THE SOUTHERN PORTION OF PALM AVENUE BETWEEN 7TH AND 9TH STREETS (ALSO KNOWN AS 9TH & PALM). MF 1062.

EXECUTIVE SUMMARY:

Staff is requesting that the City Council review and approve design/facade and comprehensive sign plan revisions for the Breakwater project on the southern portion of Palm Avenue between 7th and 9th Streets. The project was initially approved on December 14, 2011, and it is recommended by staff that the City Council determine that the proposed revisions are in substantial conformance with the approved plans.

BACKGROUND:

An application (MF 1062) for an Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028 and SCH #2011111018) was approved on December 14, 2011 for the redevelopment of a new commercial/retail center (Breakwater) that would locate



on a 4.75-acre project site on the south side of State Route 75/Palm Avenue between 9th & 7th

Streets (APNs 626-250-03-00, 626-250-04-00, 626-250-05-00, and 626-250-06-00). The project includes the construction of seven new commercial buildings with approximately 45,000 square feet of commercial space composed of a market, retail shops, and a financial institution. The project also includes an outdoor plaza, approximately 228 surface parking spaces, on-site and off-site improvements, landscaping, signage, lighting, and utilities. Ingress and egress to and from the site would be provided via two driveways on Highway 75/Palm Avenue, one driveway on 9th Street, and a separate entry off of the alley south of the project site.

ANALYSIS:

Though the project was approved on December 14, 2011 (Resolution 2011-7131), some of the conditions of approval for the project were as follows:

- The design of the corner element of the building at the southwest corner of 9th Street and Palm Avenue shall be modified to provide a more distinctive and attractive focal point of the project. Such design modification may include vertical and horizontal articulation, the use of glass or other materials, a varied façade and/or roofline including the use of a cupola or tower element, a curved façade or a public entrance.
- All building façades either fronting or facing a public street shall be designed to be pedestrian-friendly and shall incorporate clear glass, awnings, display cases, and other architectural treatment that provides visual interest. Blank walls shall be avoided and, where necessary, shall be enhanced with architectural detail and/or landscaping to either screen or add visual interest to the façade.

As such, the applicant has submitted revised plans to address these concerns and also provide alterations that it believes would deliver a higher quality product.

The revised façades incorporate a more modern design approach (see Attachment 2 for new design; note that Buildings F and G are not shown on the revised drawings and will be reviewed by the City during Phase 2 of the project development). Building "A" at the northeast corner of the property now provides pedestrian access from Palm Avenue, tall glass windows, murals on the north and east facades, elevated rooflines, and variations in building materials. It is staff's opinion that the new design provides a more unique and distinctive architectural statement and focal point at the corner of 9th Street and Palm Avenue and the newly proposed materials are more timeless and of a higher quality. Additionally, all buildings, especially those fronting public rights-of-way and pedestrian pathways, incorporate urban design elements that promote and enhance the pedestrian experience, such as transparent windows, awnings, trellises, and other architectural details. In staff's opinion, the proposed mural on the north elevation which depicts pier pilings in black and white or a "sepia" color scheme is attractive and lends itself to the building colors. Staff recommends the same type of mural on the 9th Street (east) elevation with other (non-sandcastle) shoreline elements such as shorebirds.

The project is also proposing a 49'-6" clock tower that the applicant believes is an important design feature for the project. The angled roof line of the proposed clock tower, which is designed to invoke a distinctive lifeguard tower element, would be incorporated into the proposed freestanding signs providing a cohesive and consistent design element for the project. The height limit in the C/MU-1 Zone is 40 feet, though the Municipal Code does provide exceptions to the building height restrictions for utility towers and similar structures (Imperial Beach Municipal Code Section 19.40.020). Given that the clock tower would likely be a suitable location for a stealth wireless provider, it is being proposed to accommodate such a facility at a future date with the increased height providing better opportunity for the wireless antenna

transmittal. The height of the antenna is critical to the facilities performance because the signal must be elevated above ground level at a height that provides a clear line-of-sight to clear any topographical barriers and existing natural and building environment. Wireless providers often have to install freestanding facilities within Imperial Beach because existing buildings do not provide enough height to obtain the optimal coverage. Planning for a location that can provide architectural interest, a functional utility to the public (clock), and also serve as a wireless facility may be viewed as a community benefit. Any wireless facility proposing to locate within this structure would require separate review and approval.

New Design



Previous Design

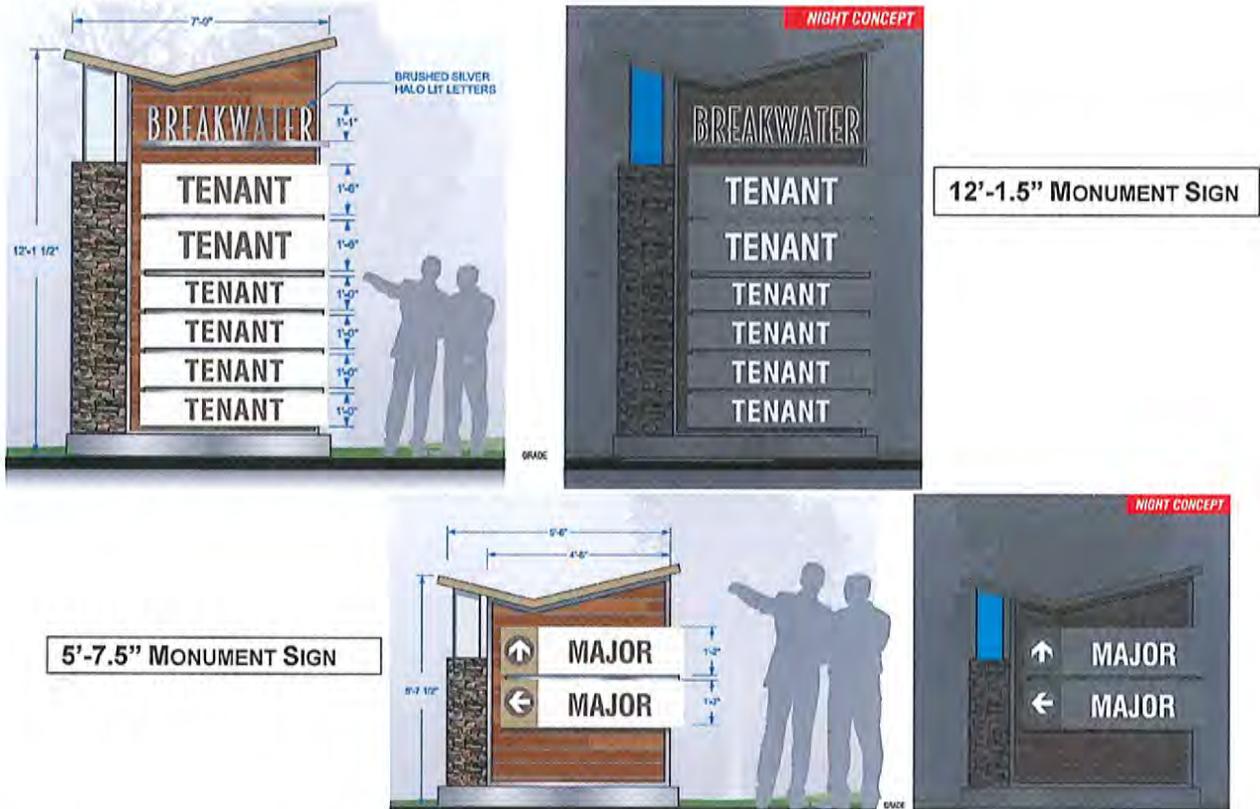


NOVEMBER 7, 2011 A-5

Signage: The applicant received approval for a Comprehensive Sign Plan for the commercial center, allowing for a 25% increase in the maximum total sign area and increased height of ten feet for two monument signs, as opposed to the typical eight foot height limit. The intent of the Comprehensive Signage Plan is to improve the overall aesthetic of a commercial center while providing sufficient signage and would require separate design and zoning review by staff on a case-by-case basis for tenant signage. However, design modifications for the freestanding monument signs should be considered.

As previously noted, two monument signs received an increased height from eight feet to ten feet to reduce the amount of freestanding signs that would locate on the site. The Municipal Code allows for one monument sign per lot frontage, which could have allowed for up to seven eight-foot-tall freestanding signs. The City approved a 25% increase in signage height for the two monument signs since it was determined that increasing the height of the two monument signs would provide a greater aesthetic appeal than a proliferation of freestanding signs. The project is now proposing to increase the height of the two taller monument signs from ten feet to 12'-1.5". The increased height of the signs are due to the distinctive "lifeguard tower" angled architectural element at the top of the sign that matches the roof line of the proposed clock tower. The increased height would not increase the amount of signage area beyond what was originally approved, and is considered by the applicant to be an integral design component for the project. It should also be noted that the "Breakwater" gateway display that would have spanned above the open courtyard is no longer proposed (see below "Previous Design" and Attachment 3). The display was approved as an architectural feature to the project, which may now be expressed through the angled features of the new monument signs. It is staff's opinion that the increased height of two monument signs would be preferable to constructing additional monument signs throughout the site.

New Design



General Plan Consistency: The development was subject to C-1 (General Commercial) zoning requirements at the time of approval (the property has recently been rezoned to C/MU-1, General Commercial & Mixed-Use, though the original approvals still apply). The intent of the commercial zone is to provide areas for business to meet the local demand for commercial goods and services. It is also intended that the dominant type of commercial activity in the commercial zone be community and neighborhood serving retail and office uses. The proposed project will provide seven commercial buildings that will meet the intent of the land use designation.

Design Review: The Design Review Board will review the proposed design revisions at a September 11, 2014 Design Review Board meeting. The Board's recommendations will be provided to the City Council during the September 17, 2014 City Council meeting.

ENVIRONMENTAL IMPACT:

The project complies with the requirements of the California Environmental Quality (CEQA). A Mitigated Negative Declaration (MND) was prepared, routed for public review, and submitted to the State Clearinghouse (SCH #2011111018) for agency review in 2011. The Final MND and Mitigation Monitoring and Reporting Program (MMRP) were adopted by the City Council on December 14, 2011.

COASTAL JURISDICTION:

This project is located in the coastal zone as defined by the California Coastal Act of 1976. The City Council held a public hearing on December 14, 2011 which served as the required coastal permit hearing and the approved the coastal development for project. The project is not located in the Appeal Jurisdiction of the California Coastal Commission and, as such, the decision was not appealable to the California Coastal Commission under Section 30603(a) of the California Public Resources Code.

FISCAL ANALYSIS:

The applicant has deposited approximately \$11,500.00 in Project Account Number 110024 to fund the processing of this application.

RECOMMENDATION:

1. Receive report and entertain public testimony;
2. Consider the design of the project;
3. That the City Council approve the design and façade modifications and determine that the revised plans substantially conform with the approved plans for Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028); Resolution 2011-7131.

Attachments:

1. 2011 Plans – Previously approved
 2. 2014 Plans – Currently proposed
 3. 2011 Signage Plans – Previously approved
 4. 2014 Signage Plans – Currently proposed
 5. Colors and Materials Board
 6. Resolution 2011-7131
- c: file MF 1062

BREAKWATER IMPERIAL BEACH, CALIFORNIA



PROJECT NARRATIVE:

THE PROJECT INCLUDES COMMERCIAL RETAIL, SHOPS, FINANCIAL RETAIL, SURFACE PARKING, HARDSCAPE AND LANDSCAPE AREAS.

ASSESSOR PARCEL NUMBERS:

FROM WEST TO EAST:
628-250-02-00, 628-250-04-00, 628-250-05-00, 628-250-06-00

CONSTRUCTION TYPE / OCCUPANCY:

BUILDING 'A' MARKET (M OCCUPANCY):	TYPE V
BUILDING 'B,C,D,E' SHOPS (M OCCUPANCY):	TYPE V
BUILDING FINANCIAL RETAIL (M OCCUPANCY):	TYPE V
BUILDING 'F' RETAIL (M OCCUPANCY)	TYPE V

ZONING:

C-1 GENERAL COMMERCIAL ZONE

FLOOR AREA RATIO:

SITE AREA:	4.75 ACRES
ACHIEVED FAR:	0.22
PROPOSED MAX HEIGHT:	39 FT
ALLOWED MAXIMUM HEIGHT:	40 FT

PROPOSED USES:

COMMERCIAL (RETAIL, SHOPS, FINANCIAL RETAIL, MARKET)

SETBACKS:

NO FRONT, SIDE OR REAR YARD SETBACKS REQUIRED

SHEET INDEX

- C-S COVER SHEET
- C-1 CIVIL COVER
- C-2 PRELIMINARY GRADING PLAN
- C-3 PRELIMINARY UTILITY PLAN
- C-4 EASEMENT AND ENCUMBRANCE PLAN
- C-4 SR 75 ALIGNMENT AND GRADE EXHIBIT
- L-1 LANDSCAPE PLAN
- A-1 SITE PLAN
- A-2 FLOOR PLANS - BUILDINGS A AND B
- A-3 FLOOR PLANS - BUILDINGS C, D, AND E
- A-4 FLOOR PLANS - BUILDING F AND BANK
- A-5 EXTERIOR ELEVATIONS
- A-6 SECTIONS

DIRECTORY

APPLICANT:

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ESTEAN LENYOUN,

ARCHITECT:

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ANDREW STEVENSON

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BRAD LENAHAN

CIVIL ENGINEER:

PROJECT DESIGN CONSULTANTS
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MARINA WURST

CONSULTING ARCHITECT:

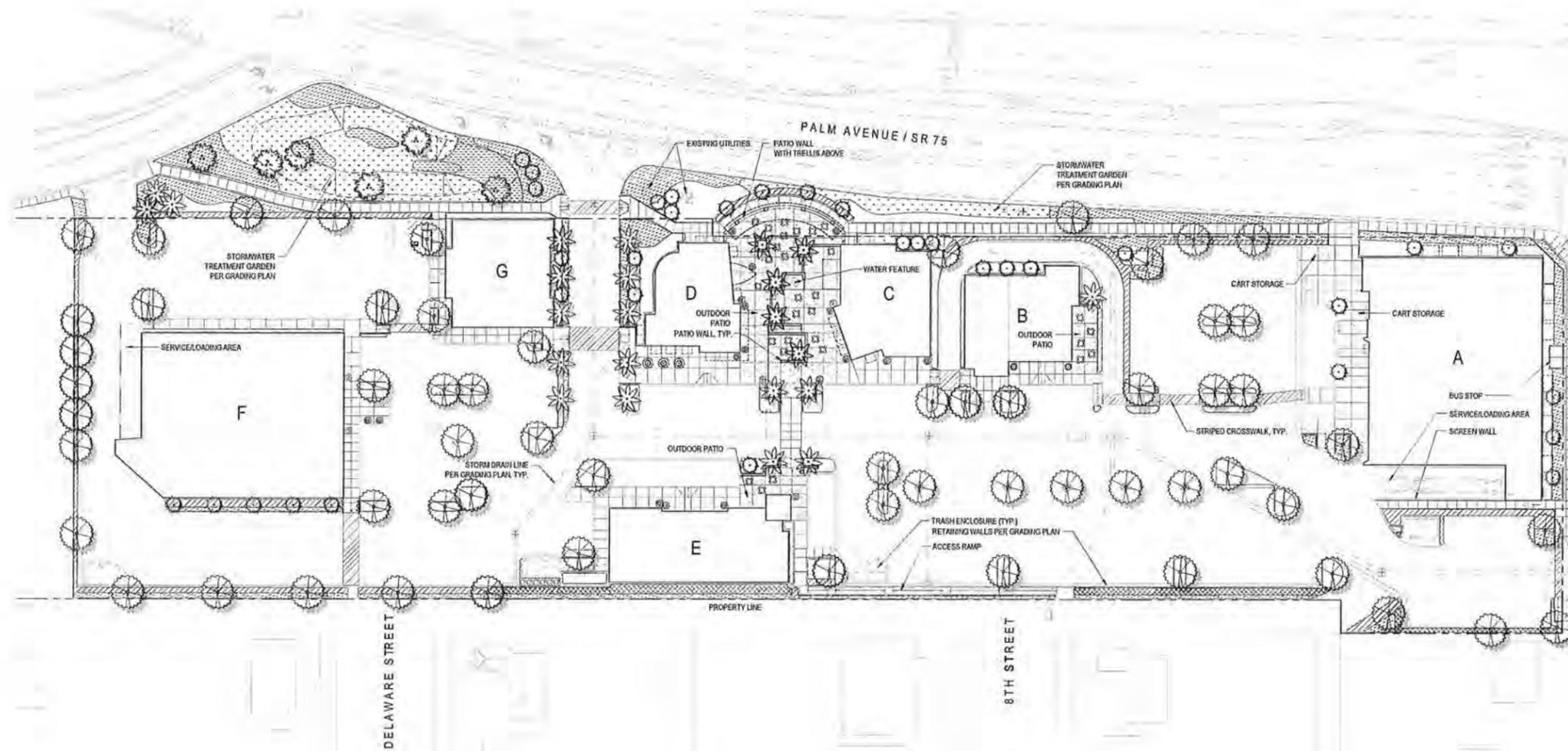
ALBERTI + ALBERTI ARCHITECTURE
2033 SAN ELJO AVENUE, SUITE 113
CARDIFF BY THE SEA, CA 92007
(760) 848-3916 T
(760) 848-3916 F
MARCELO ALBERTI

VICINITY MAP



Cover Sheet

DECEMBER 5, 2011 C-S

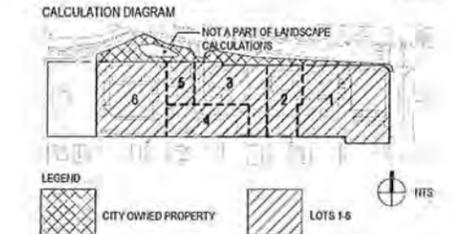


Landscape Notes

- MINIMUM TREE SEPARATION DISTANCE:
 - TRAFFIC SIGNAL, STOP SIGN: 20 FEET
 - UNDERGROUND UTILITY LINES (EXCEPT SEWER): 5 FEET
 - SEWER LINES: 10 FEET
 - ABOVE GROUND UTILITY STRUCTURES: 10 FEET
 - DRIVEWAYS: 10 FEET
 - INTERSECTIONS (INTERSECTING CURB LINES OF TWO STREETS): 25 FEET
- ALL TREES WITHIN PARKING AREAS SHALL BE A MINIMUM OF THREE FOOT BY FIVE FEET, OR 45 SQUARE FOOT (NOT INCLUDING PERIMETER AREA LANDSCAPING) FOR EVERY THREE PARKING SPACES OR THE EQUIVALENT.
- TREE ROOT BARRIERS SHALL BE INSTALLED WHERE TREES ARE PLACED WITHIN 5 FEET OF PUBLIC IMPROVEMENTS INCLUDING WALKS, CURBS, OR STREET PAVEMENT OR WHERE NEW PUBLIC IMPROVEMENTS ARE PLACED ADJACENT TO EXISTING TREES. ROOT BARRIERS WILL NOT BE WRAPPED AROUND THE ROOTBALL.
- WALL THICKNESS IN MODULES 24 INCHES LONG BY 24 INCHES DEEP; MANUFACTURED WITH A MINIMUM 50% POST-CONSUMER RECYCLED POLYPROPYLENE PLASTIC WITH ADDED ULTRAVIOLET INHIBITORS; RECYCLABLE.
- ALL LANDSCAPE AND IRRIGATION SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH THE CITY OF IMPERIAL BEACH LANDSCAPE REGULATIONS.
- ALL REQUIRED LANDSCAPE AREAS SHALL BE MAINTAINED BY THE OWNER. THE LANDSCAPE AREAS SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION.
- TREES SHALL BE MAINTAINED SO THAT ALL BRANCHES OVER PEDESTRIAN WALKWAYS ARE 8 FEET ABOVE THE WALKWAY GRADE AND SO ALL BRANCHES OVER VEHICULAR TRAVEL WAYS ARE 14 FEET ABOVE THE GRADE OF THE TRAVELWAY.
- ALL GRADED, DISTURBED OR ERODED AREAS THAT WILL NOT BE PERMANENTLY PAVED OR COVERED BY STRUCTURES SHALL BE VEGETATED AND IRRIGATED.
- ALL SLOPE AREAS 3:1 OR GREATER SHALL BE REINFORCED WITH STRAW MATS (SC 150 MANUFACTURED BY NORTH AMERICAN GREEN 1-800-473-1965 OR CITY APPROVED EQUAL).
- ALL LANDSCAPED AREAS WILL BE WATERED WITH A PERMANENT BELOW GRADE, FULLY AUTOMATIC IRRIGATION SYSTEM. THIS SYSTEM WILL BE CONTROLLED BY A DUAL PROGRAM ELECTRONIC TIME CLOCK AND REMOTE CONTROL VALVES. POP-UP TYPE HEADS WILL BE USED IN SHRUB AND GROUND COVER AREAS AND WILL BE HELD 24" AWAY FROM IMPERMEABLE SURFACES. SUBSURFACE DRIP IRRIGATION WILL BE USED FOR LANDSCAPED AREAS LESS THAN 8" WIDE. THE SYSTEM WILL BE INSTALLED AS SOON AS POSSIBLE AFTER CONSTRUCTION AND PRIOR TO PLACEMENT OF PLANT MATERIALS. THE SYSTEM SHALL BE IN CONFORMANCE WITH CALIFORNIA'S AB 1881 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE.

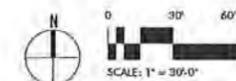
Landscape Calculations

REQUIREMENTS:	
NOT LESS THAN 15% OF THE TOTAL SITE SHALL BE LANDSCAPED	
TOTAL SITE AREA (LOTS 1-6):	206,910 SF, 4.75 ACRES
CITY OWNED PROPERTY AREA (LESS STORMWATER AREA):	13,965 SF, 0.32 ACRES
TOTAL COMBINED AREA (LOTS 1-6 + CITY OWNED PROPERTY, LESS STORMWATER AREA):	220,875 SF, 5.07 ACRES
15% OF TOTAL SITE (LOTS 1-6 + CITY OWNED PROPERTY, LESS STORMWATER AREA):	33,131 SF, 0.76 ACRES
TOTAL LANDSCAPE AREA PROVIDED WITHIN LOTS 1-6:	21,265 SF, 0.49 ACRES
TOTAL LANDSCAPE AREA PROVIDED WITHIN CITY OWNED PROPERTY:	13,965 SF, 0.32 ACRES
TOTAL LANDSCAPE AREA PROVIDED (LOTS 1-6 + CITY OWNED PROPERTY):	35,230 SF, 0.81 ACRES
PERCENTAGE OF LANDSCAPE TO TOTAL SITE AREA (LOTS 1-6 + CITY OWNED PROPERTY):	15.05%



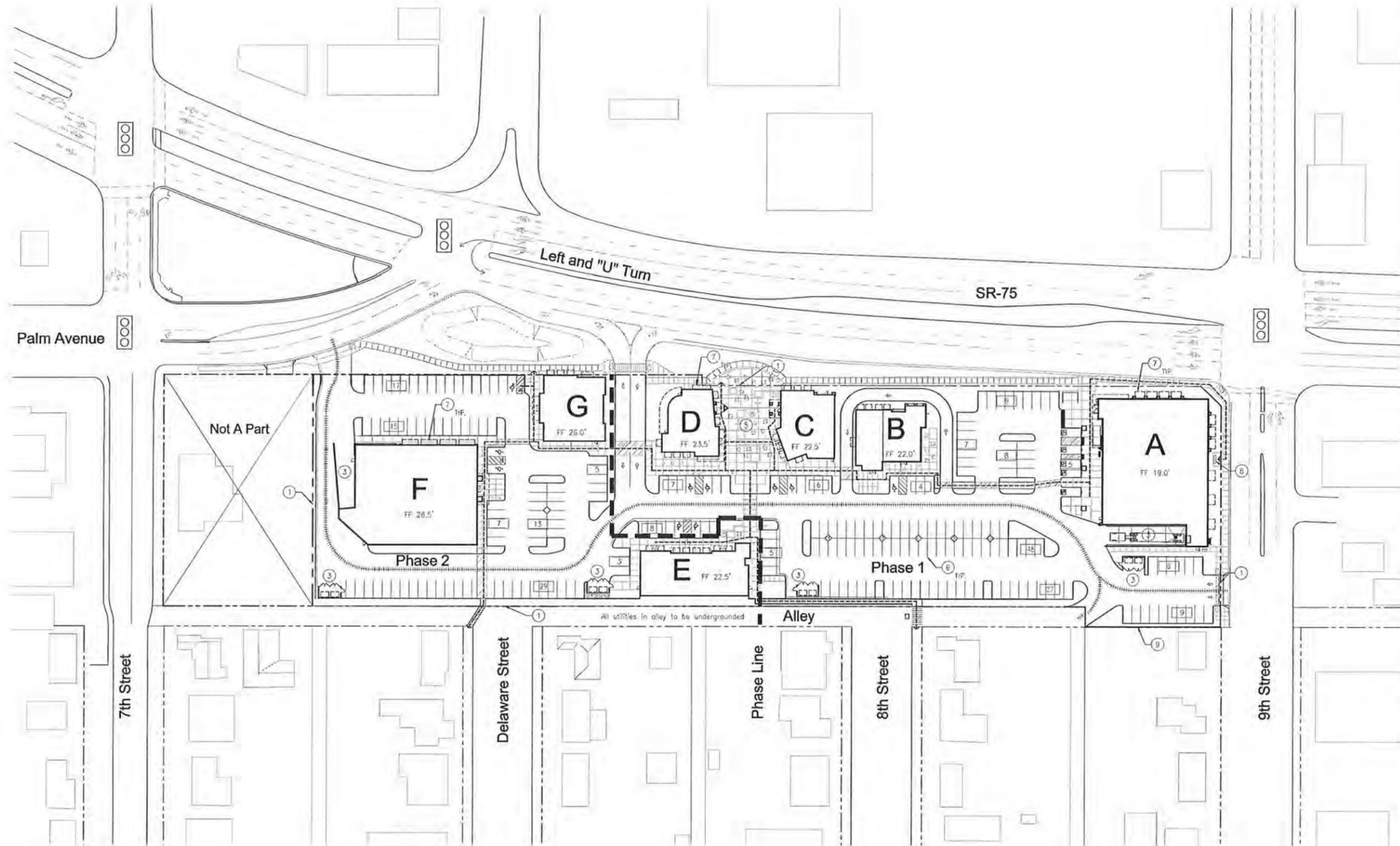
Plant Material Legend

- Palm Trees - such as:**
 - 20" BTH - PHOENIX DACTYLIFERA / MEDJOLA / MEDJOLA DATE PALM
 - 20" BTH - ARCHONTOPHOECA CURRIBRANCHIANA / KING PALM
 - 20" BTH - SYAGRUS ROMANZOFFIANUM / QUEEN PALM
- Evergreen Canopy Trees - 24" box size, such as:**
 - ARBITUS MARINKI / MADROVE
 - METROSDEROS EXCELSA / NEW ZEALAND CHRISTMAS TREE
 - EUCALYPTUS FICIFOLIA / RED FLOWERING GUM
- Small Accent Trees - 24" box size, such as:**
 - ARBITUS UNEDO / STRAWBERRY TREE
 - STRELITZIA NICOLAI / TREE BIRD OF PARADISE
 - ERIOBOTRYA DEFLEXA / BRONZE LOQUAT
 - DRACAENA DRACO / DRAGON TREE
 - LEPTOSPERMUM R. MARINKI'S CHOICE / MARINKI'S CHOICE TEA TREE
 - MELALEUCA NESOPHILA / PINK MELALEUCA
- Medium Height Shrubs - 5 gallon size, such as:**
 - CARISSA MACROCARPA / BOXWOOD BEAUTY / BOXWOOD BEAUTY NATAL PLUM
 - RAPHIOLEPSIS UMBELLATA / MIDWAY HAWTHORN
 - AGAVE ATTENUATA / FOXTAIL AGAVE
 - PHORBIUM SPP. / NEW ZEALAND FLAX
 - ROSA SPP. / ROSE
 - STRELITZIA REGINAE / BIRD OF PARADISE
 - CISTUS SPP. / ROCKROSE
 - ESCALONIA COMPACTA / ESCALLONIA
 - ROSMARRINUS O. / TUSCAN BLUE / TUSCAN BLUE ROSEMARY
- Medium Height Screening Hedge - 5 gallon size, such as:**
 - LEPTOSPERMUM APPLE BLOSSOM / APPLE BLOSSOM LEPTOSPERMUM
 - LIGULSTRUM TEXANUM / TEXAS PRIVET
 - CARISSA MACROCARPA / BOXWOOD BEAUTY / BOXWOOD BEAUTY NATAL PLUM
 - RAPHIOLEPSIS UMBELLATA / MIDWAY HAWTHORN
- Tall Screening Hedge - 5 gallon size, such as:**
 - ARBITUS UNEDO / COMPACTA / DWARF STRAWBERRY TREE
 - PODOCARPUS HEIKELII / LONG LEAFED YELLOW WOOD
 - PITOSPORUM T. / OLIVER TWIST / OLIVER TWIST PITOSPORUM
 - ELAEAGARUS DECIPENS / JAPANESE BLUEBERRY
 - PRUNUS CAROLINIA / BRIGHT & TIGHT / CAROLINA LAUREL
- Potted Plants:**
 - ACCENT PLANTINGS IN FREE-STANDING CONTAINERS; QUANTITY, SIZE, AND MATERIAL TO BE DETERMINED.
- Groundcovers - 1 gallon size, such as:**
 - CALANDRINA SPP. / CALANDRINA
 - SERICEO MANDRALISCAE / BLUE CHALKSTICKS
 - CARUSSA M. / GREEN CARPET / GREEN CARPET NATAL PLUM
 - LANTANA SPP. / LANTANA
- Grasses & Grass-like Plants - 5 gallon and 1 gallon size, such as:**
 - LOMANORA LONGIFOLIA / DWARF MAT RUSH
 - CAREX PANSA / CALIFORNIA MEADOW SEDGE
 - HELICTOTRICHON / SEMPERVIRENS / BLUE OAT GRASS
 - PENNISETUM H. / RED BUNNY TAILS / RED BUNNY TAILS FOUNTAIN GRASS
 - DIETES V. / ORANGE DROPS / ORANGE DROPS FORTNIGHT LILY
- Stormwater Treatment Garden Plantings:**
 - Canopy Tree - 24" box size, such as:**
 - POPULUS FREMOHII / WESTERN COTTONWOOD
 - PLATANUS RACEMOSA / CALIFORNIA SYCAMORE
 - Grasses & Grass-like Plants - see plant sizes below:**
 - CAREX SPISSA / SAWGRASS SEDGE - PLUGS AT 9" O.C.
 - CAREX PANSA / CALIFORNIA MEADOW SEDGE - PLUGS AT 9" O.C.
 - HEMEROCALLIS HYBRID (EVERGREEN) / HYBRID DAY LILY - 1 Gal. size
- 1/4" Decorative Gravel Mulch**



DECEMBER 05, 2011

L-1



LEGEND	
---	EXISTING PROPERTY LINE
---	ACCESSIBLE PATH OF TRAVEL
♿	ACCESSIBLE PARKING STALL
↑ ↓	RAMPED DIRECTIONAL TRAFFIC MARKINGS
----	PIPE LINE

KEYNOTES	
1.	EXISTING PROPERTY LINE
2.	NOT USED
3.	REFUSE & RECYCLING
4.	LOADING & UNLOADING AREA
5.	PLAZA
6.	PARKING STRIPPING
7.	CANOPY OR AWNING ABOVE
8.	BUS STOP TO MEET CITY REQUIREMENTS
9.	CMU WALL, 4' HIGH FOR 1ST 20' FROM PROPERTY LINE, THEN 6' HIGH

NOTES	
1.	THE PROJECT WILL EXCEED THE PARKING RATIO OF 1,250 SF OF NET FLOOR AREA, PLUS ONE SPACE PER 2 EMPLOYEES.
2.	SIDEWALKS SHALL NOT EXCEED 5% IN THE DIRECTION OF TRAVEL AND 2% CROSS SLOPE.

Project Summary	
Site Area	4.75 acres
Building Area	46,200 sf
Parking	238 Stalls (5.1/1,000 sf)

Building Summary	
A) Market	14,800 sf
B) Shops	3,900 sf
C) Shops	3,500 sf
D) Shops	3,000 sf
E) Shops	4,700 sf
F) Retail	12,300 sf
G) Retail / Financial	4,000 sf

LIGHTING DESCRIPTION:
LIGHTING THROUGHOUT THE BREAKWATER PROJECT AREA SHALL BE DESIGNED TO ENHANCE THE ARCHITECTURAL CONCEPTS AND MATERIALS OF THE BUILDINGS, AND PROVIDE A COMFORTABLE ENVIRONMENT FOR WALKING, AND SHOPPING.

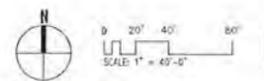
LIGHTING DESIGN CRITERIA

- LIGHTING SHALL CONFORM TO ALL CITY OF IMPERIAL BEACH LIGHTING REQUIREMENTS AND CALIFORNIA STATE CALGREEN ENERGY EFFICIENCY STANDARDS. EFFECTIVE EXTERIOR LIGHTING SHALL BE ARCHITECTURALLY INTEGRATED WITH THE CHARACTER OF THE ADJACENT STRUCTURES. THE CITY OF IMPERIAL BEACH STREET DESIGN CRITERIA, BE APPROPRIATE IN SCALE, HEIGHT, AND INTENSITY FOR THE USE, AND BE MOUNTED DOWNWARD SO AS TO MINIMIZE GLARE.
- EXTERIOR LIGHTING SHALL NOT BURST, FLASH, OR BE OF UNUSUALLY HIGH INTENSITY OR BRIGHTNESS.
- LIGHTING SHALL BE DIRECTED AWAY AND SHIELDED FROM RESIDENTIAL OR LIGHT-SENSITIVE AREAS, AND SHALL NOT PRODUCE GLARE OR LIGHT "SPILL" THAT ADVERSELY AFFECTS NEIGHBORING PROPERTIES OR FUNCTIONS.

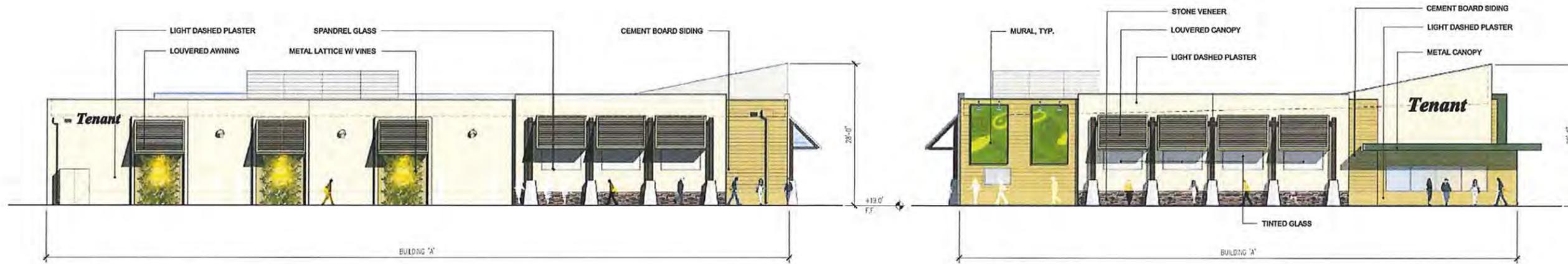
EXTERIOR LIGHTING CONCEPTS

- | | | |
|--|--|--|
| <p>LIGHTING DESIGN CRITERIA</p> <ul style="list-style-type: none"> AFTER HOURS LIGHTING SHALL REMAIN IN OPERATION AS AGREED TO BY THE PROJECT MANAGEMENT AND THE CITY OF IMPERIAL BEACH. | <p>PARKING LOT LIGHTING</p> <ul style="list-style-type: none"> UNIFORMITY OF LIGHTING IN PARKING AREAS WILL BE PROVIDED TO ENHANCE USE AND VISIBILITY. PARKING AREA FIXTURES AND STANDARDS SHALL BE CONSISTENT THROUGHOUT THE PROJECT. LIGHTING LEVELS, LIGHTING SOURCES, AND LAMP COLOR-TEMPERATURE-INDEXES (CRI) SHALL BE SUITABLE FOR HIGH ACTIVITY RETAIL ENVIRONMENTS AND WILL BE CONSISTENT WITH CITY OF IMPERIAL BEACH STANDARDS. | <p>PUBLIC SPACE LIGHTING</p> <ul style="list-style-type: none"> PEDESTRIAN AREAS SHALL BE ILLUMINATED SUCH THAT PATHWAYS ARE HIGHLIGHTED. PLAZA AND GATHERING AREAS SHALL BE ILLUMINATED SUCH THAT THE AREA FUNCTION IS SERVED WITHOUT ADVERSELY IMPACTING SURROUNDING AREAS OR USES. PUBLIC SPACE LIGHTING SHALL PROMOTE PEDESTRIAN USE. FUEL-EFFICIENT LIGHTING MAY BE ACCOMPLISHED THROUGH A VARIETY OF FIXTURE TYPES. |
| <p>LANDSCAPE ACCENT LIGHTING</p> <ul style="list-style-type: none"> LANDSCAPE LIGHTING SHALL BE LOCATED SUCH THAT GLARE IS MINIMIZED FOR VEHICULAR TRAFFIC AND PEDESTRIAN ACTIVITIES. LANDSCAPE LIGHTING SHALL ENHANCE THE LANDSCAPE AREAS AS APPROPRIATE FOR THE PROJECT. FIXTURES MAY BE MOUNTED RECESSED IN-GROUND, ABOVE-GRADE, OR ON TREES. | | |

Site Plan



DECEMBER 5, 2011 A-1



East Elevation

North Elevation



North Elevation

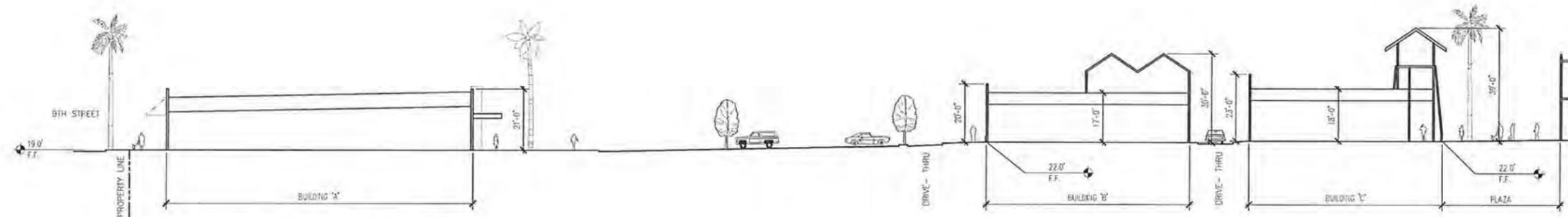


North Elevation



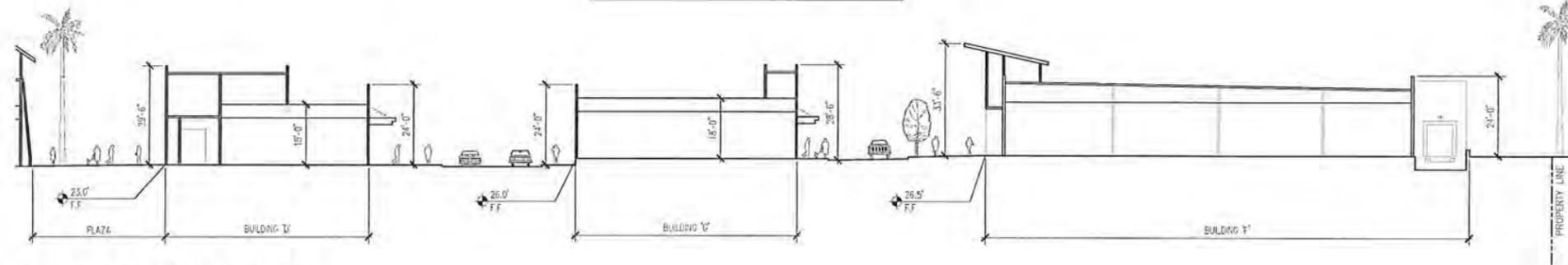
Site - North Elevation

DECEMBER 5, 2011 A-5

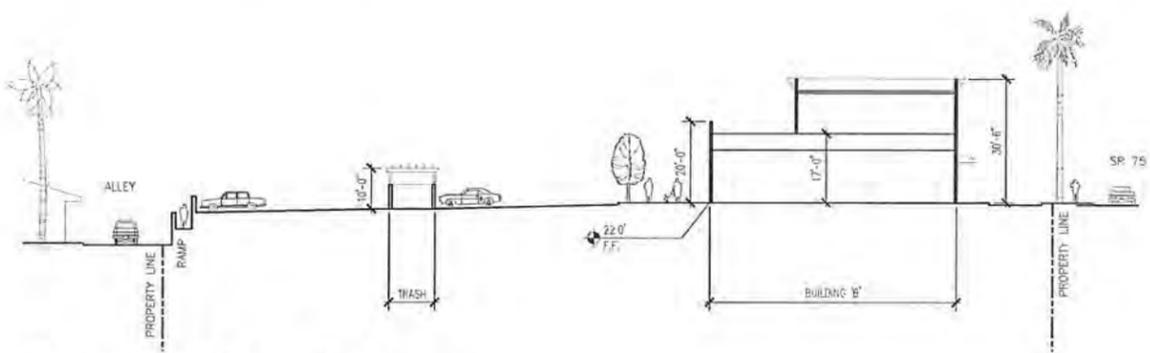


Section A West - East

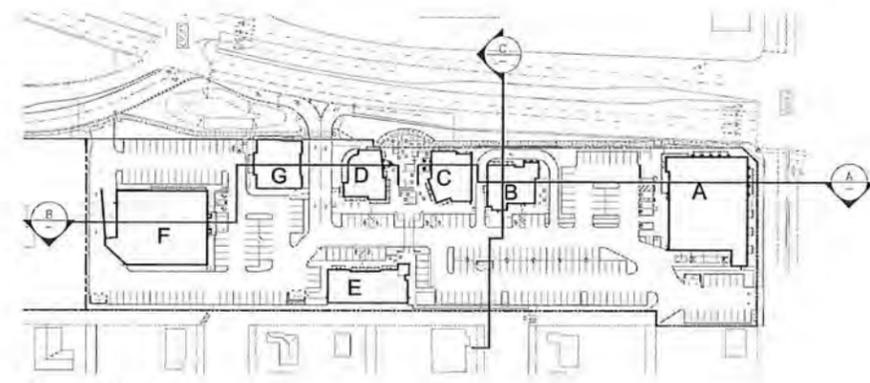
NOTE: SCREENED MECHANICAL ROOFTOP EQUIPMENT, TYP.



Section B West - East



Section C North - South



Key Plan

BREAKWATER IMPERIAL BEACH, CALIFORNIA



PROJECT NARRATIVE:

THE PROJECT INCLUDES COMMERCIAL RETAIL, SHOPS, FINANCIAL RETAIL, SURFACE PARKING, HARDSCAPE AND LANDSCAPE AREAS.

ASSESSOR PARCEL NUMBERS:

FROM WEST TO EAST:
626-250-02-00, 626-250-04-00, 626-250-05-00, 626-250-06-00

CONSTRUCTION TYPE / OCCUPANCY:

BUILDING 'A' MARKET (M OCCUPANCY):	TYPE V
BUILDING 'B,C,D,E' SHOPS (M OCCUPANCY):	TYPE V
BUILDING FINANCIAL RETAIL (M OCCUPANCY):	TYPE V
BUILDING 'F' RETAIL (M OCCUPANCY):	TYPE V

ZONING:

C-1 GENERAL COMMERCIAL ZONE

FLOOR AREA RATIO:

SITE AREA:	4.75 ACRES
ACHIEVED FAR:	0.22
PROPOSED MAX HEIGHT:	49'-5"

PROPOSED USES:

COMMERCIAL (RETAIL, SHOPS, FINANCIAL RETAIL, MARKET)

SETBACKS:

NO FRONT, SIDE OR REAR YARD SETBACKS REQUIRED

SHEET INDEX

- C-S COVER SHEET
- C-1 CIVIL COVER
- C-2 PRELIMINARY GRADING PLAN
- C-3 PRELIMINARY UTILITY PLAN
- C-4 EASEMENT AND ENCUMBRANCE PLAN
- C-4 SR 75 ALIGNMENT AND GRADE EXHIBIT
- L-1 LANDSCAPE PLAN
- A-1 SITE PLAN
- A-2 FLOOR PLANS - BUILDINGS A AND B
- A-3 FLOOR PLANS - BUILDINGS C, D, AND E
- A-4 FLOOR PLANS - BUILDING F AND G
- A-5 EXTERIOR ELEVATIONS
- A-6 SECTIONS

VICINITY MAP



DIRECTORY

APPLICANT:

SUDBERRY DEVELOPMENT
5405 MOREHOUSE DRIVE, SUITE 200
SAN DIEGO, CA 92121
(858) 548-3000 T
(858) 548-3000 F
ESTEAN LEHYOUN

ARCHITECT:

ANDREW HULL STEVENSON ARCHITECTS
5495 MOREHOUSE DRIVE, SUITE 200
SAN DIEGO, CA 92121
(858) 220-7224 T
(858) 546-3009 F
ANDREW STEVENSON

CONSULTING ARCHITECT:

ALBERTI + ALBERTI ARCHITECTURE
2033 SAN ELIJO AVENUE, SUITE 113
CARDIFF BY THE SEA, CA 92007
(760) 846-3916 T
(760) 846-3916 F
MARCELO ALBERTI

LANDSCAPE ARCHITECT:

NOWELL & ASSOCIATES
2605 STATE STREET, SUITE B
SAN DIEGO, CA 92103
(619) 325-1990 T
(619) 325-1997 F
BRAD LENAHAN

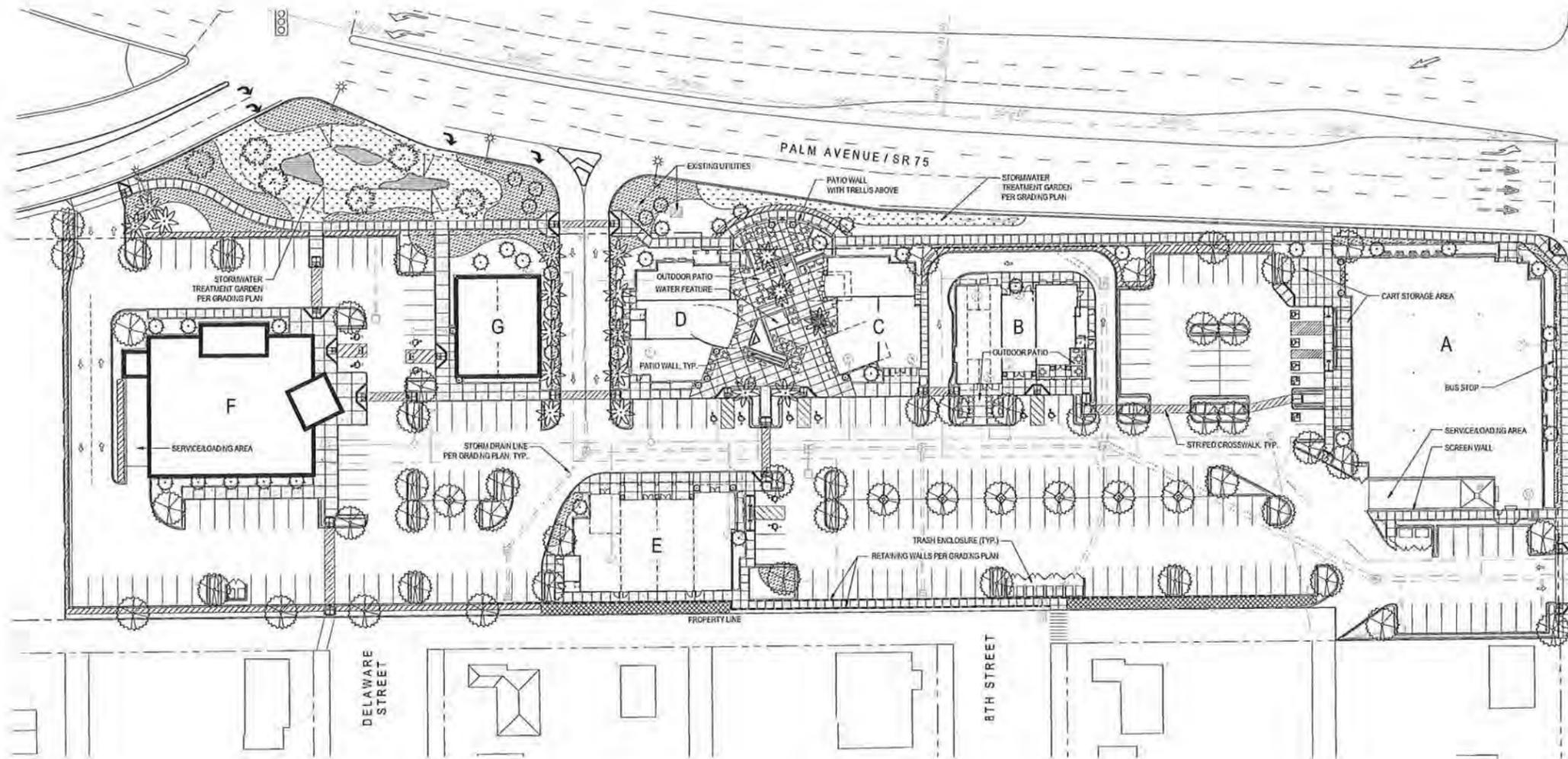
CIVIL ENGINEER:

PROJECT DESIGN CONSULTANTS
701 B STREET, SUITE 800
SAN DIEGO, CA 92101
(619) 235-6471 T
(619) 234-0349 F
MARINA WURST

Cover Sheet

September 2, 2014

C-S



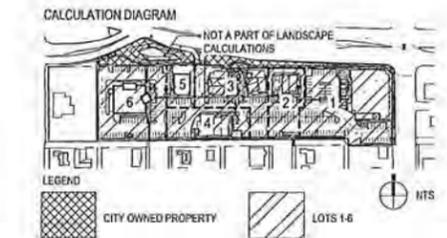
Landscape Notes

- MINIMUM TREE SEPARATION DISTANCE:
TRAFFIC SIGNAL, STOP SIGN: 20 FEET
UNDERGROUND UTILITY LINES (EXCEPT SEWER): 5 FEET
SEWER LINES: 10 FEET
ABOVE GROUND UTILITY STRUCTURES: 10 FEET
DRIVEWAYS: 10 FEET
INTERSECTIONS (INTERSECTING CURB LINES OF TWO STREETS): 25 FEET
- ALL TREES WITHIN PARKING AREAS SHALL BE A MINIMUM OF THREE-FOOT BY FIFTEEN-FOOT, OR 45 SQUARE FOOT (NOT INCLUDING PERIMETER AREA LANDSCAPING) FOR EVERY THREE PARKING SPACES OR THE EQUIVALENT.
- TREE ROOT BARRIERS SHALL BE INSTALLED WHERE TREES ARE PLACED WITHIN 5 FEET OF PUBLIC IMPROVEMENTS INCLUDING WALKS, CURBS, OR STREET PAVEMENT OR WHERE NEW PUBLIC IMPROVEMENTS ARE PLACED ADJACENT TO EXISTING TREES. ROOT BARRIERS WILL NOT BE WRAPPED AROUND THE ROOTBALL.
ROOT BARRIERS SHALL BE BLACK, INJECTION MOLDED PANELS, OF 0.085" WALL THICKNESS IN MODULES 24 INCHES LONG BY 24 INCHES DEEP, MANUFACTURED WITH A MINIMUM 50% POST-CONSUMER RECYCLED POLYPROPYLENE PLASTIC WITH ADDED ULTRAVIOLET INHIBITORS; RECYCLABLE.
- ALL LANDSCAPE AND IRRIGATION SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH THE CITY OF IMPERIAL BEACH LANDSCAPE REGULATIONS.
- ALL REQUIRED LANDSCAPE AREAS SHALL BE MAINTAINED BY THE OWNER. THE LANDSCAPE AREAS SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION.
- TREES SHALL BE MAINTAINED SO THAT ALL BRANCHES OVER PEDESTRIAN WALKWAYS ARE 6 FEET ABOVE THE WALKWAY GRADE AND SO ALL BRANCHES OVER VEHICULAR TRAVEL-WAYS ARE 14 FEET ABOVE THE GRADE OF THE TRAVEL-WAY.
- ALL GRADED, DISTURBED OR ERODED AREAS THAT WILL NOT BE PERMANENTLY PAVED OR COVERED BY STRUCTURES SHALL BE VEGETATED AND IRRIGATED.
- ALL SLOPE AREAS 3:1 OR GREATER SHALL BE REINFORCED WITH STRAW MATS (SC 150 MANUFACTURED BY NORTH AMERICAN GREEN 1-800-473-1965 OR CITY APPROVED EQUAL).
- ALL LANDSCAPED AREAS WILL BE WATERED WITH A PERMANENT BELOW GRADE, FULLY AUTOMATIC IRRIGATION SYSTEM. THIS SYSTEM WILL BE CONTROLLED BY A DUAL PROGRAM ELECTRONIC TIME CLOCK AND REMOTE CONTROL VALVES. POP-UP TYPE HEADS WILL BE USED IN SHRUB AND GROUNDCOVER AREAS AND WILL BE HELD 2" AWAY FROM IMPERMEABLE SURFACES. SUBSURFACE DRIP IRRIGATION WILL BE USED FOR LANDSCAPED AREAS LESS THAN 8" WIDE. THE SYSTEM WILL BE INSTALLED AS SOON AS POSSIBLE AFTER CONSTRUCTION AND PRIOR TO PLACEMENT OF PLANT MATERIALS. THE SYSTEM SHALL BE IN CONFORMANCE WITH CALIFORNIA'S AB 1881 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE.

Landscape Calculations

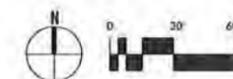
REQUIREMENTS:
NOT LESS THAN 15% OF THE TOTAL SITE SHALL BE LANDSCAPED.

TOTAL SITE AREA (LOTS 1-6)	206,910 SF; 4.76 ACRES
CITY OWNED PROPERTY AREA (LESS STORMWATER AREA)	13,965 SF; 0.32 ACRES
TOTAL COMBINED AREA (LOTS 1-6 + CITY OWNED PROPERTY, LESS STORMWATER AREA)	220,875 SF; 5.07 ACRES
15% OF TOTAL SITE (LOTS 1-6 + CITY OWNED PROPERTY, LESS STORMWATER AREA)	33,131 SF; 0.76 ACRES
TOTAL LANDSCAPE AREA PROVIDED WITHIN LOTS 1-6	21,255 SF; 0.48 ACRES
TOTAL LANDSCAPE AREA PROVIDED WITHIN CITY OWNED PROPERTY:	13,965 SF; 0.32 ACRES
TOTAL LANDSCAPE AREA PROVIDED (LOTS 1-6 + CITY OWNED PROPERTY)	35,190 SF; 0.81 ACRES
PERCENTAGE OF LANDSCAPE TO TOTAL SITE AREA (LOTS 1-6 + CITY OWNED PROPERTY)	15.55%



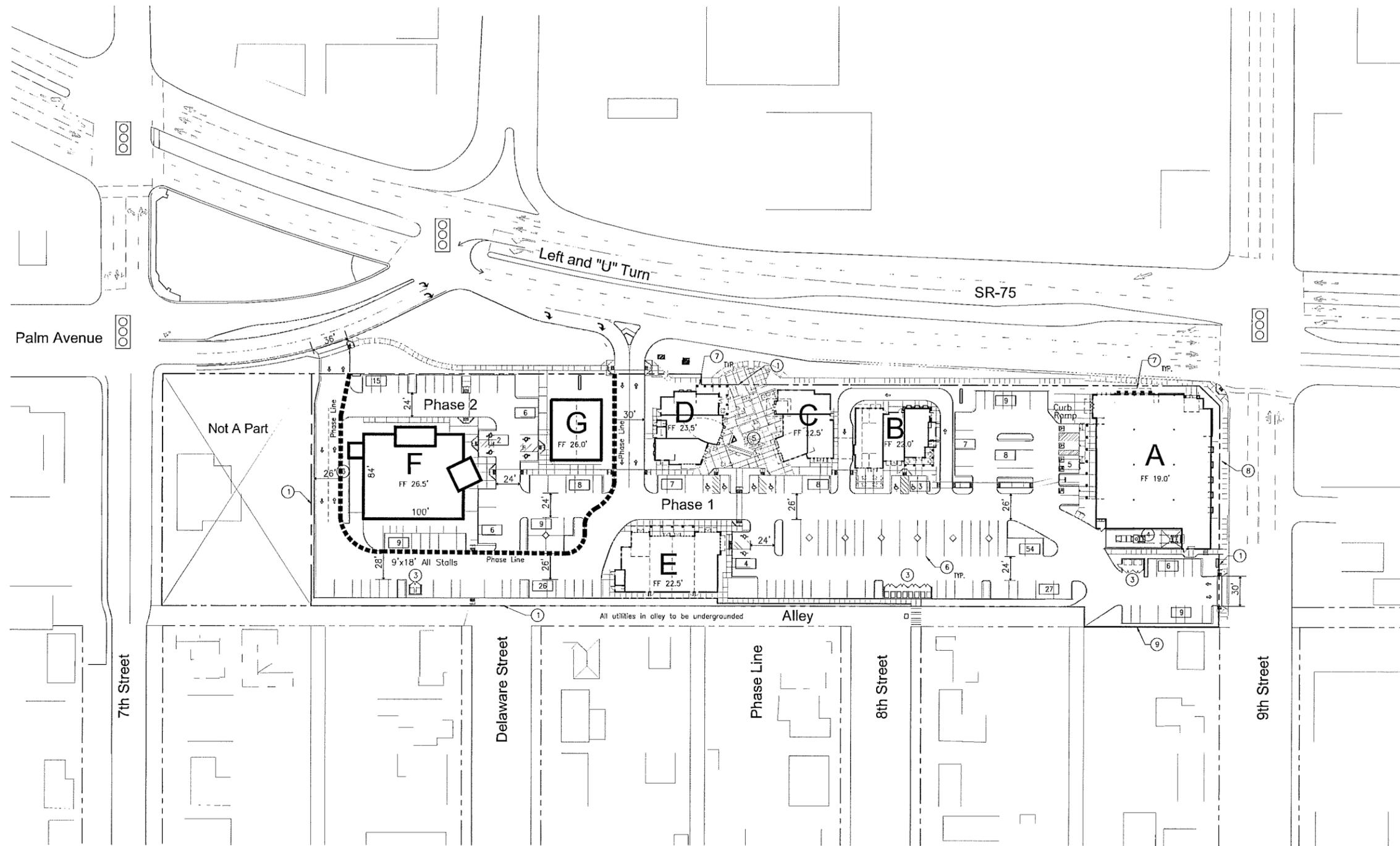
Plant Material Legend

- Palm Trees - such as:**
 - 20' BTH - PHOENIX DACTYLIFERA / MEDJOL / MEDJOL DATE PALM
 - 20' BTH - ARCHONTOPHOENIX CUMINGII / MANANA / KING PALM
 - 20' BTH - SYAGRUS ROMANZOFFIANUM / QUEEN PALM
- Evergreen Canopy Trees - 24" box size, such as:**
 - ARBUTUS MARINA / MADRONIE
 - METRODEROS EXCELSA / NEW ZEALAND CHRISTMAS TREE
 - EUCALYPTUS FICIFOLIA / RED FLOWERING GUM
- Small Accent Trees - 24" box size, such as:**
 - ARBUTUS UNEDO / STRAWBERRY TREE
 - STRELITZIA NICOLAI / TREE BIRD OF PARADISE
 - ERIOBOTRYA DEFLEXA / IRONZE LOQUAT
 - DRACAENA DRACO / DRAGON TREE
 - LEPTOSPERMUM R. MANNING'S CHOICE / MANNING'S CHOICE TEA TREE
 - MELALEUCA NESOPHILA / PINK MELALEUCA
- Medium Height Shrubs - 5 gallon size, such as:**
 - CARISSA MACROCARPA 'BOXWOOD BEAUTY' / BOXWOOD BEAUTY NATAL PLUM
 - RHAPHOLEPSIS UMBELLATA 'MINOR' / INDIAN HAWTHORN
 - AGAVE ATTENUATA / FOXTAIL AGAVE
 - PHORMIUM SPP. / NEW ZEALAND FLAX
 - ROSA SPP. / ROSE
 - STRELITZIA REGINAE / BIRD OF PARADISE
 - CISTUS SPP. / ROCKROSE
 - ESCALLONIA 'COMPACTA' / ESCALLONIA
 - ROSMARINUS O. 'TUSCAN BLUE' / TUSCAN BLUE ROSEMARY
- Medium Height Screening Hedge - 5 gallon size, such as:**
 - LEPTOSPERMUM 'APPLE BLOSSOM' / APPLE BLOSSOM LEPTOSPERMUM
 - LIGUSTRUM 'TEXANUM' / TEXAS PRIVET
 - CARISSA MACROCARPA 'BOXWOOD BEAUTY' / BOXWOOD BEAUTY NATAL PLUM
 - RHAPHOLEPSIS UMBELLATA 'MINOR' / INDIAN HAWTHORN
- Tall Screening Hedge - 5 gallon size, such as:**
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 - PODOCARPUS HEUNKELI / LONG LEAFED YELLOW WOOD
 - PITOSPORUM T. 'OLIVER TWIST' / OLIVER TWIST PITOSPORUM
 - ELAEOCARPUS DECIPENS / JAPANESE BLUEBERRY
 - FRUNUS CAROLINIA 'BRIGHT & TIGHT' / CAROLINA LAUREL
- Grasses & Grass-like Plants & Groundcovers - 5 gallon and 1 gallon size, such as:**
 - CALANDRINA SPP. / CALANDRINA
 - CAREX PANSA / CALIFORNIA MEADOW SEDGE
 - CARISSA M. 'GREEN CARPET' / 'GREEN CARPET' NATAL PLUM
 - DIETES V. 'ORANGE DROPS' / 'ORANGE DROPS' FORTNIGHT Lily
 - HELICTOTRICHON SEMPERVIRENS / BLUE OAT GRASS
 - LANTANA SPP. / LANTANA
 - LOMANDRA LONGIFOLIA / DWARF MAT RUSH
 - PENNisetum M. 'RED BUNNY TAILS' / BUNNY TAILS FOUNTAIN GRASS
 - SENECIO MANOHALISCAE / BLUE CHALKSTICKS
- Stonewater Treatment Garden Plantings**
 - Canopy Tree - 24" box size, such as:**
 - POPULUS FREMONTHI / WESTERN COTTONWOOD
 - PLATANUS RACEMOSA / CALIFORNIA SYCAMORE
 - Grasses & Grass-like Plants - See plant sizes below.**
 - CAREX BISSA / SAWGRASS SEDGE - PLUGS AT 9" O.C.
 - CAREX PANSA / CALIFORNIA MEADOW SEDGE - PLUGS AT 9" O.C.
 - HEHEMOCALLIS HYBRID (EVERGREEN) / HYBRID DAYLILY - 1 Gal. size
- Potted Plants:**
ACCENT PLANTINGS IN FREE-STANDING CONTAINERS, QUANTITY, SIZE, AND MATERIAL TO BE DETERMINED.



August 14, 2014

L-1



LEGEND	
---	EXISTING PROPERTY LINE
---	ACCESSIBLE PATH OF TRAVEL
♿	ACCESSIBLE PARKING SPALL
---	PAINTED DIRECTIONAL TRAFFIC MARKINGS
+++++	FIRE LANE

KEYNOTES	
1.	EXISTING PROPERTY LINE
2.	NOT USED
3.	REFUSE & RECYCLING
4.	LOADING & UNLOADING AREA
5.	PLAZA
6.	PARKING STRIPING
7.	CANOPY OR AWNING ABOVE
8.	BUS STOP TO MEET CITY REQUIREMENTS
9.	CMU WALL, 4' HIGH FOR 1ST 20' FROM PROPERTY LINE, THEN 6' HIGH

NOTES	
1.	THE PROJECT WILL EXCEED THE PARKING RATIO OF 1/250 SF OF NET FLOOR AREA, PLUS ONE SPACE PER 2 EMPLOYEES.
2.	SIDEWALKS SHALL NOT EXCEED 5% IN THE DIRECTION OF TRAVEL AND 2% CROSS SLOPE.

Project Summary

Site Area 4.75 acres
 Building Area 44,684 sf
 Parking 228 Stalls (5.1/1,000 sf)

Building Summary

A) Market	16,000 sf
B) Shops	4,114 sf
C) Shops	3,564 sf
D) Shops	4,106 sf
E) Shops	5,500 sf
F) Retail	8,400 sf
G) Retail / Financial	3,000 sf

* Shops buildings can be demised

LIGHTING DESCRIPTION:

LIGHTING THROUGHOUT THE BREAKWATER PROJECT AREA SHALL BE DESIGNED TO ENHANCE THE ARCHITECTURAL CONCEPTS AND MATERIALS OF THE BUILDINGS, AND PROVIDE A COMFORTABLE ENVIRONMENT FOR WALKING, AND SHOPPING.

LIGHTING DESIGN CRITERIA

- LIGHTING SHALL CONFORM TO ALL CITY OF IMPERIAL BEACH LIGHTING REQUIREMENTS AND CALIFORNIA STATE CALGREEN ENERGY EFFICIENCY STANDARDS. EFFECTIVE EXTERIOR LIGHTING SHALL BE ARCHITECTURALLY INTEGRATED WITH THE CHARACTER OF THE ADJACENT STRUCTURES. THE CITY OF IMPERIAL BEACH STREET DESIGN CRITERIA, BE APPROPRIATE IN SCALE, HEIGHT, AND INTENSITY FOR THE USE, AND BE MOUNTED DOWNWARD SO AS TO MINIMIZE GLARE.
- EXTERIOR LIGHTING SHALL NOT BLINK, FLASH, OR BE OF UNUSUALLY HIGH INTENSITY OR BRIGHTNESS.
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EXTERIOR LIGHTING CONCEPTS

LIGHTING DESIGN CRITERIA

- AFTER HOURS LIGHTING SHALL REMAIN IN OPERATION AS AGREED TO BY THE PROJECT MANAGEMENT AND THE CITY OF IMPERIAL BEACH.

LANDSCAPE ACCENT LIGHTING

- LANDSCAPE LIGHTING SHALL BE LOCATED SUCH THAT GLARE IS MINIMIZED FOR VEHICULAR TRAFFIC AND PEDESTRIAN ACTIVITIES.
- LANDSCAPE LIGHTING SHALL ENHANCE THE LANDSCAPE AREAS AS APPROPRIATE FOR THE PROJECT.
- FIXTURES MAY BE MOUNTED RECESSED IN-GROUND, ABOVE-GRADE, OR ON TREES.

PARKING LOT LIGHTING

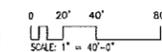
- UNIFORMITY OF LIGHTING IN PARKING AREAS WILL BE PROVIDED TO ENHANCE USE AND VISIBILITY.
- PARKING AREA FIXTURES AND STANDARDS SHALL BE CONSISTENT THROUGHOUT THE PROJECT.
- LIGHTING LEVELS, LIGHTING SOURCES, AND LAMP COLOR-RENDERING-INDEXES (CRI) SHALL BE SUITABLE FOR HIGH ACTIVITY RETAIL ENVIRONMENTS AND WILL BE CONSISTENT WITH CITY OF IMPERIAL BEACH STANDARDS.

PUBLIC SPACE LIGHTING

- PEDESTRIAN AREAS SHALL BE ILLUMINATED SUCH THAT PATHWAYS ARE HIGHLIGHTED.
- PLAZA AND GATHERING AREAS SHALL BE ILLUMINATED SUCH THAT THE AREA FUNCTION IS SERVED WITHOUT ADVERSELY IMPACTING SURROUNDING AREAS OR USE'S.
- PUBLIC SPACE LIGHTING SHALL PROMOTE PEDESTRIAN USE.
- PUBLIC SPACE LIGHTING MAY BE ACCOMPLISHED THROUGH A VARIETY OF FIXTURE TYPES.

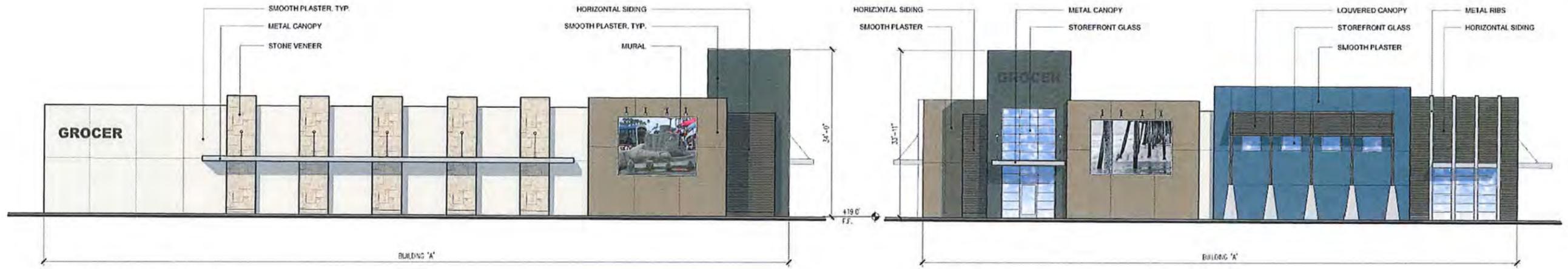
Site Plan

Breakwater
 Imperial Beach, California



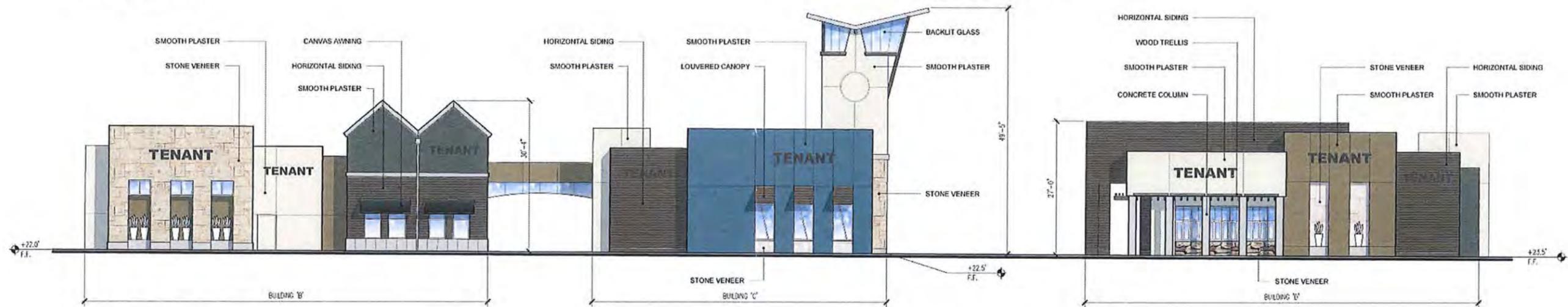
August 14, 2014

A-1



East Elevation

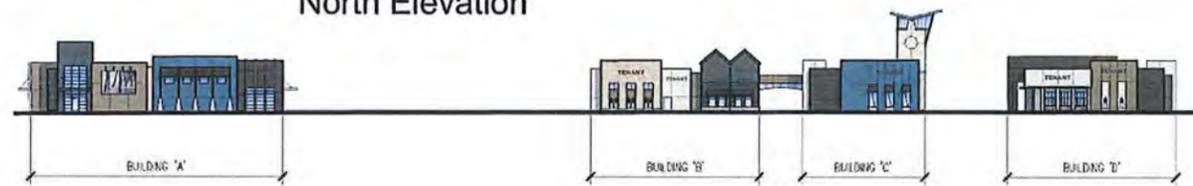
North Elevation



North Elevation



North Elevation



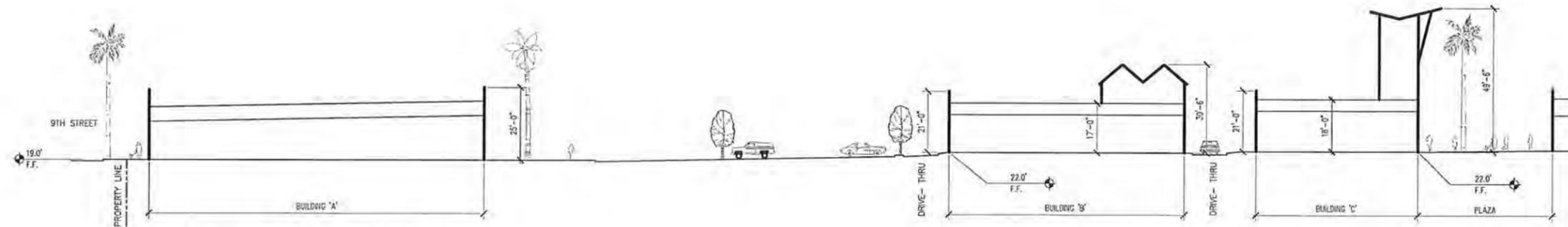
Site - North Elevation



SCALE: 1/8" = 1'-0"

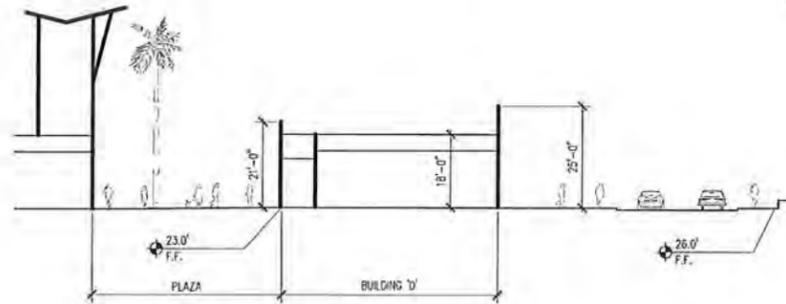
September 2, 2014

A-5

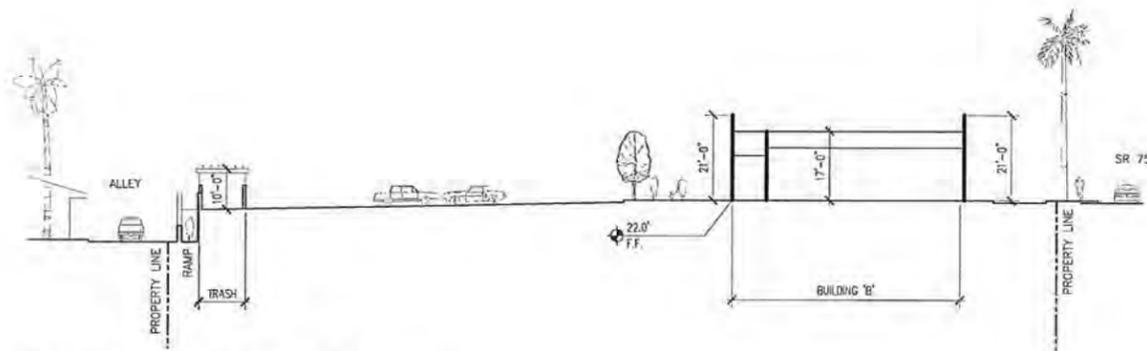


Section A West - East

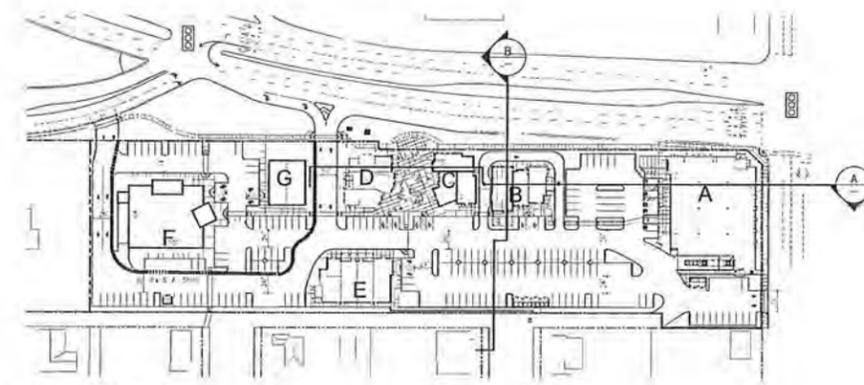
NOTE: SCREENED MECHANICAL ROOFTOP EQUIPMENT, TYP.



Section A continuation West - East

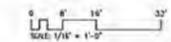


Section B North - South



Key Plan

Aug. 28, 2014 - 11:00am
 5445 Marquardt Drive, Suite 200
 San Diego, California 92121
 T: 619-220-7224 F: 619-546-3993



August 14, 2014

A-6



Alberti+Alberti

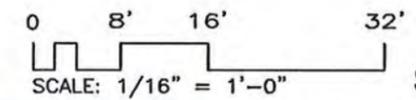
Building 'B'

Building 'C'

Building 'D'

Palm Avenue Elevation

Sep 04, 2014 - 1:30pm
 C:\Users\Alberti\My Documents\B\New Development\Permit\DW elevations rendering.dwg



September 2, 2014

Andrew Hull Stevenson Architects

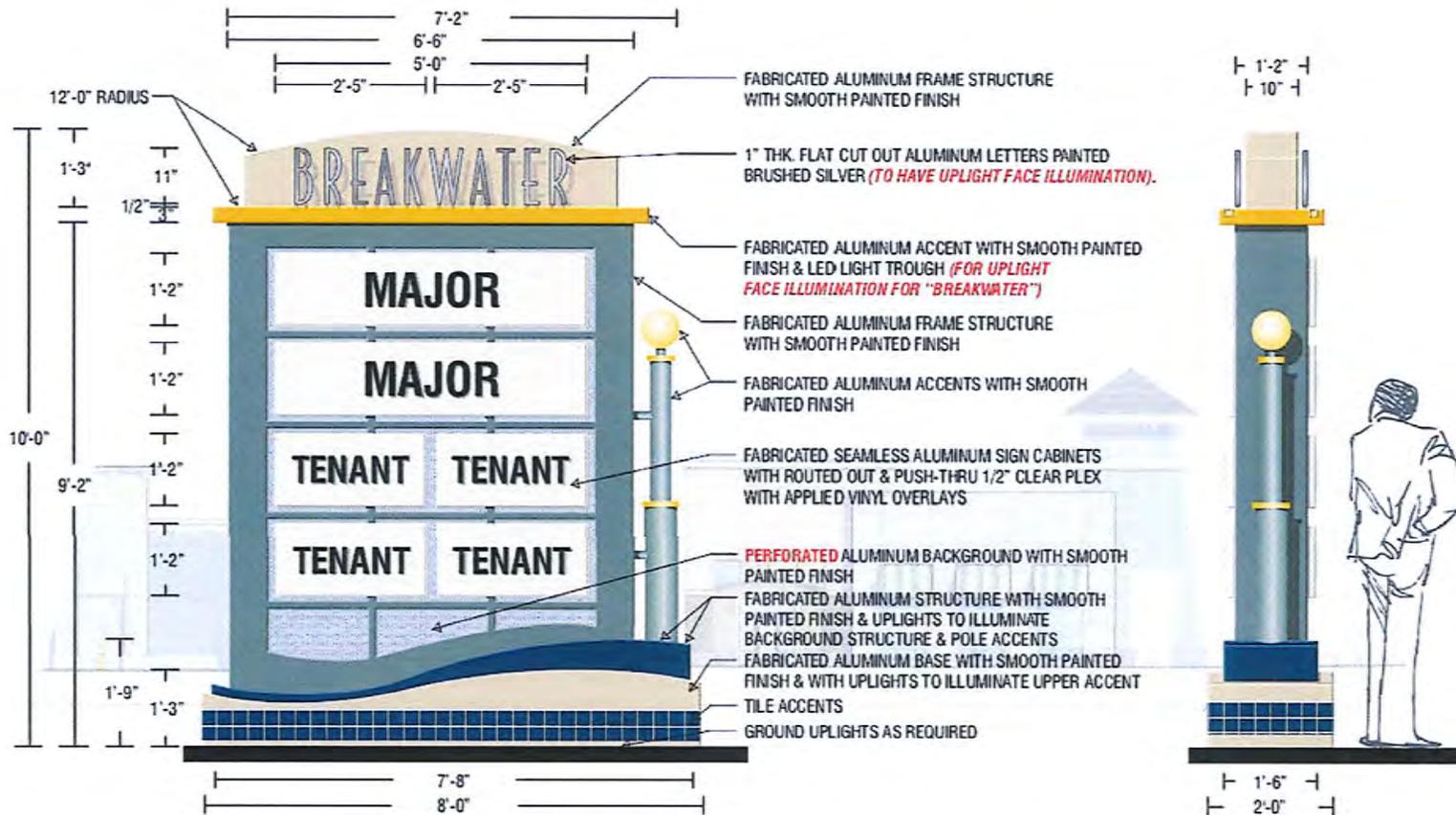
5465 Morehouse Drive Suite 260
 San Diego, California 92121
 T 858.220.7224 F 858.546.3009

Elevations

Breakwater, Imperial Beach, California



Attachment 3 - 2011 Sign Plans

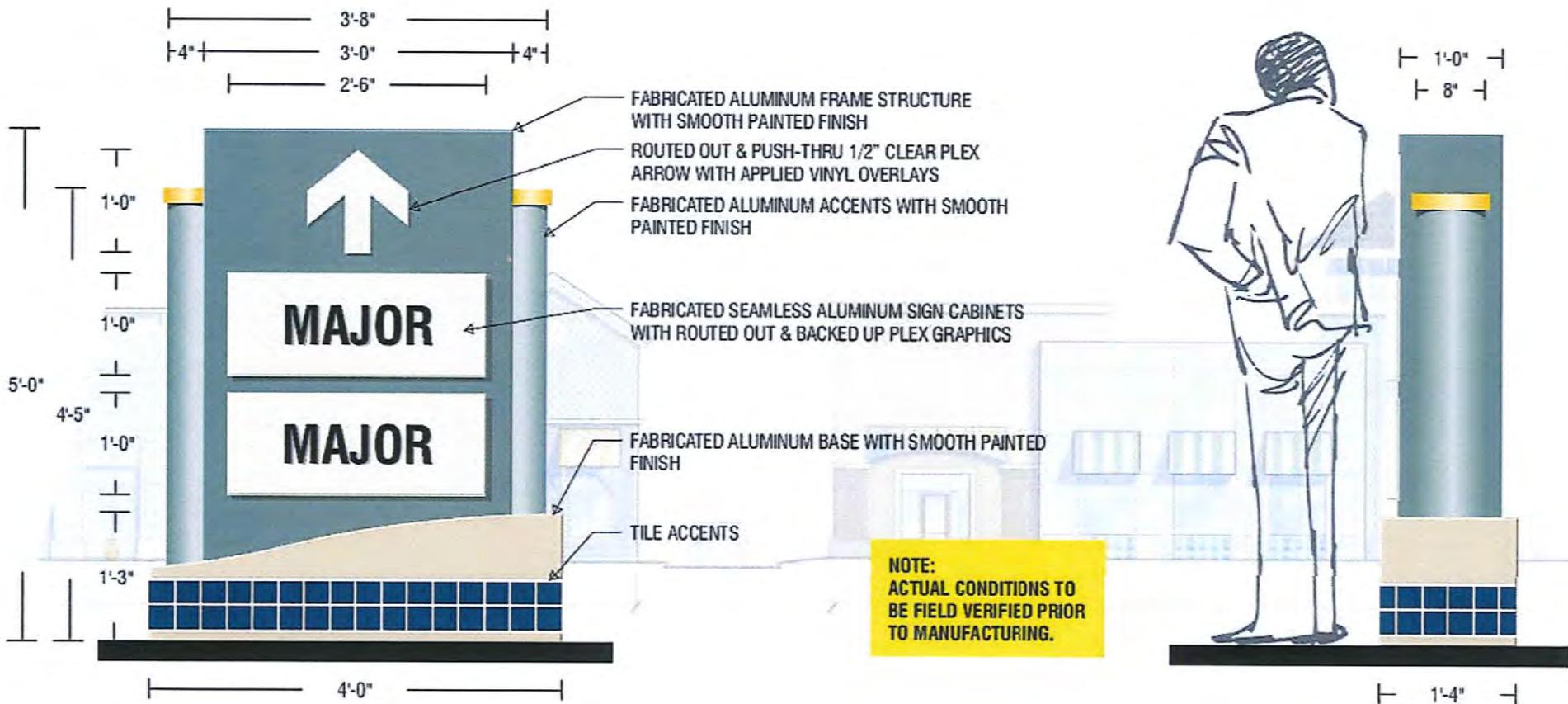


NIGHT TIME VIEW SCALE: 1/4" = 1'-0"

USE STANDARD ALUMINUM CONSTRUCTION WITH ANGLE FRAME AND STEEL PIPE SUPPORT INTO CEMENT FOOTING.
 PAINT CABINET, ACCENT TRIM AND BASE STRUCTURE MATTHEWS ACRYLIC POLYURETHANE COLORS.
 SEE ENGINEERS SPECS FOR STRUCTURAL CALCULATIONS,
 VERIFY EXACT LOCATION OF SIGN PRIOR TO INSTALLATION.

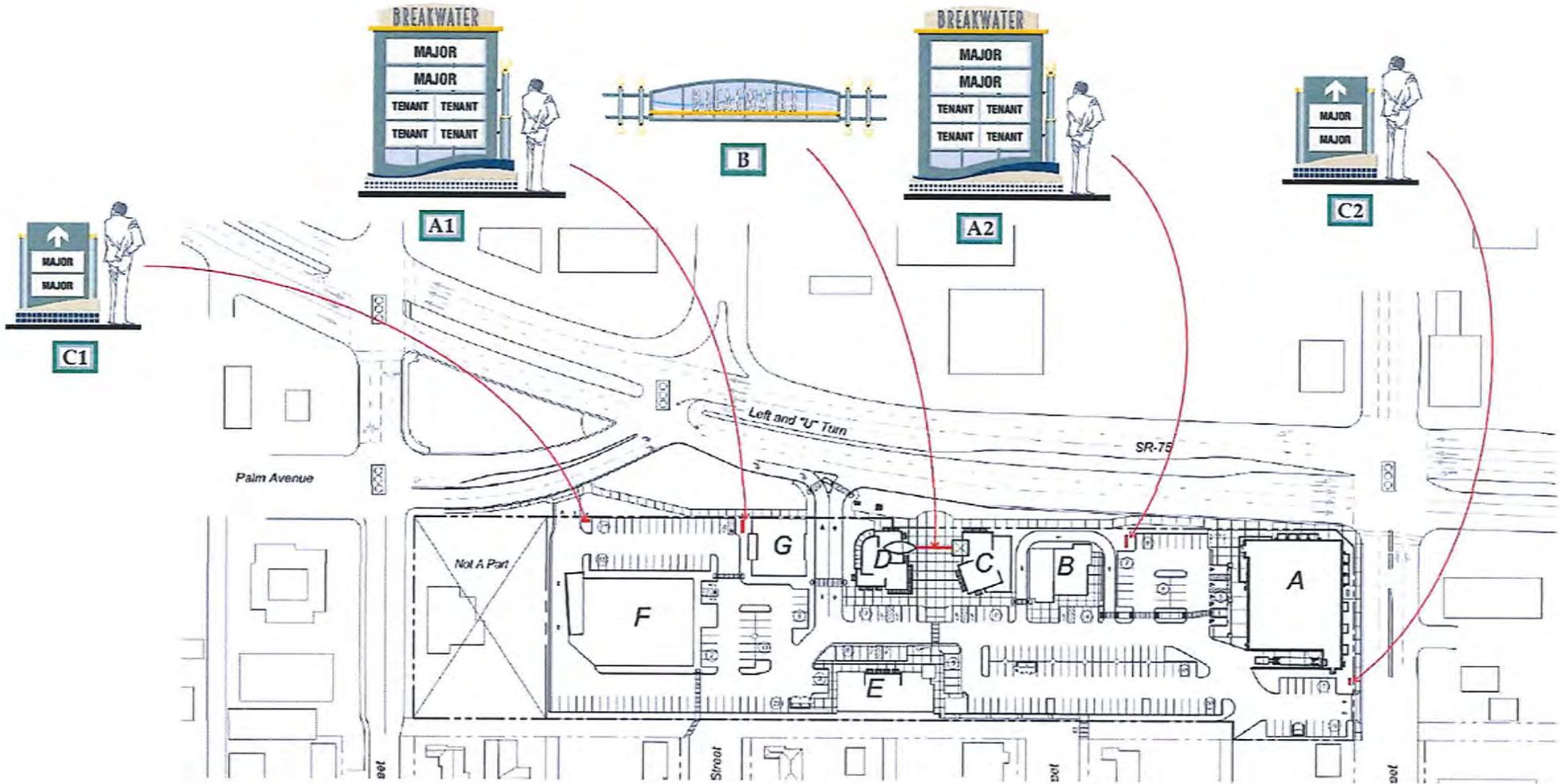
A1 A2 NEW DOUBLE FACED PYLON SIGN

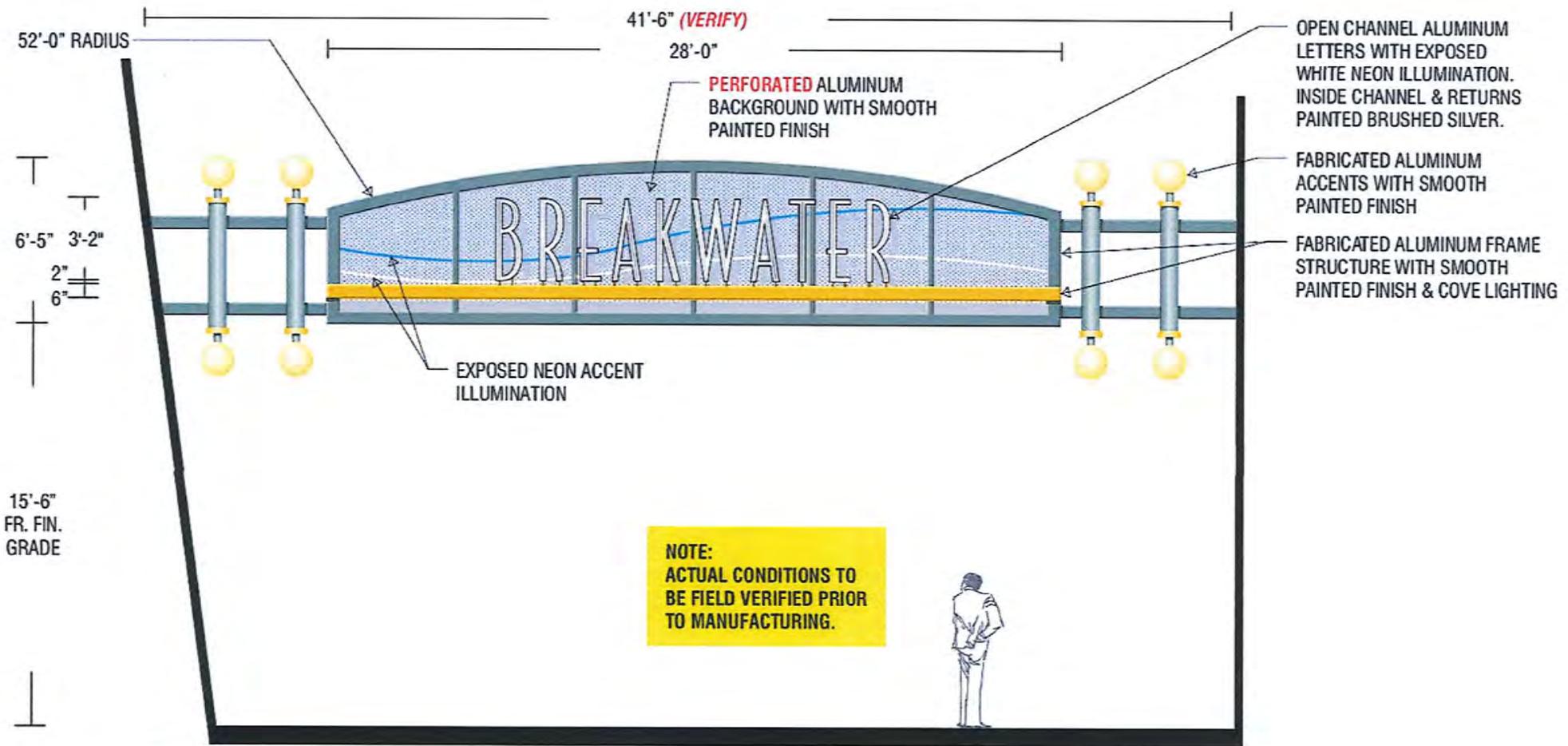
SCALE: 1/2" = 1'-0"



C1 C2 NEW SINGLE FACED DIRECTIONAL SIGN

SCALE: 3/4" = 1'-0"

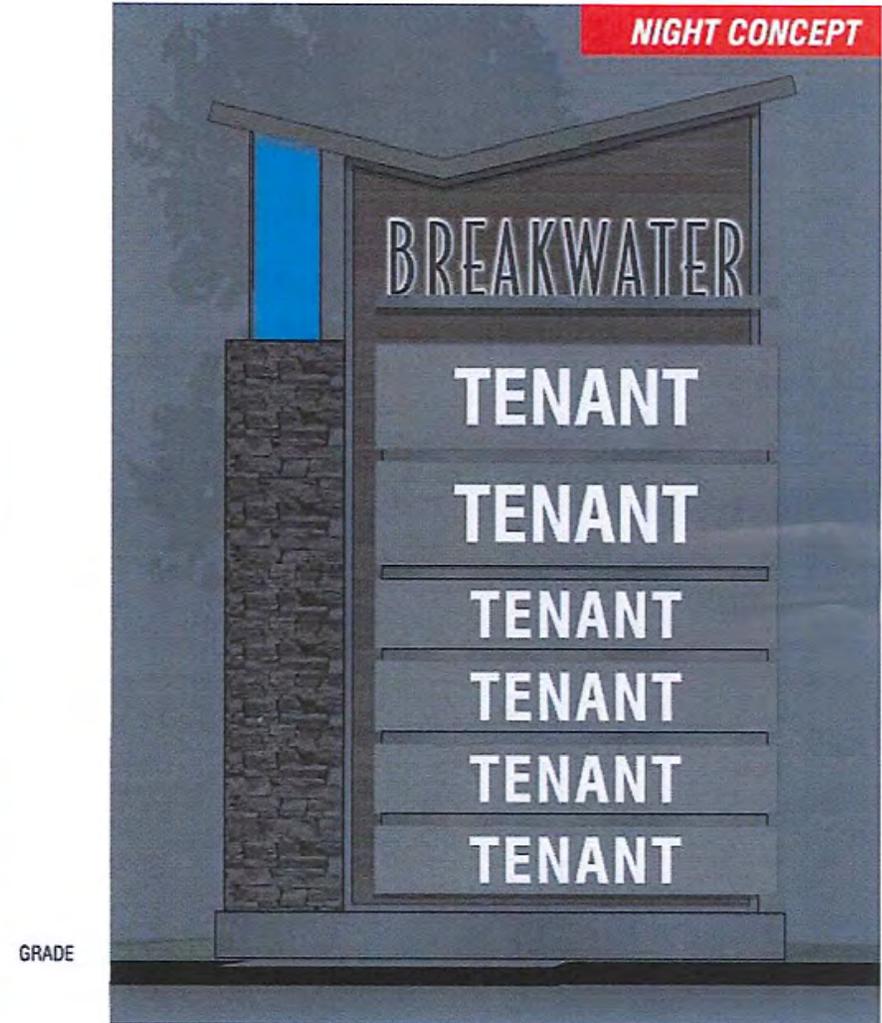
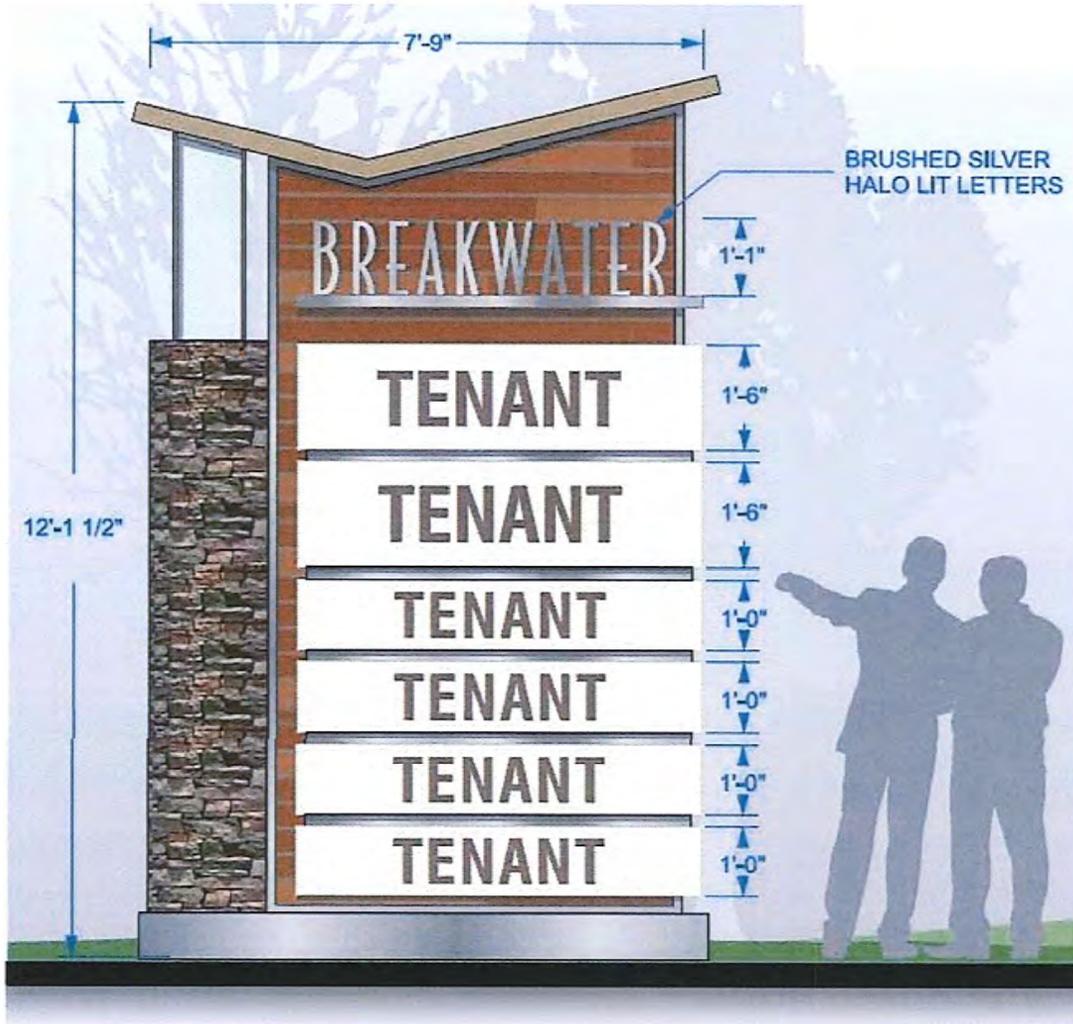


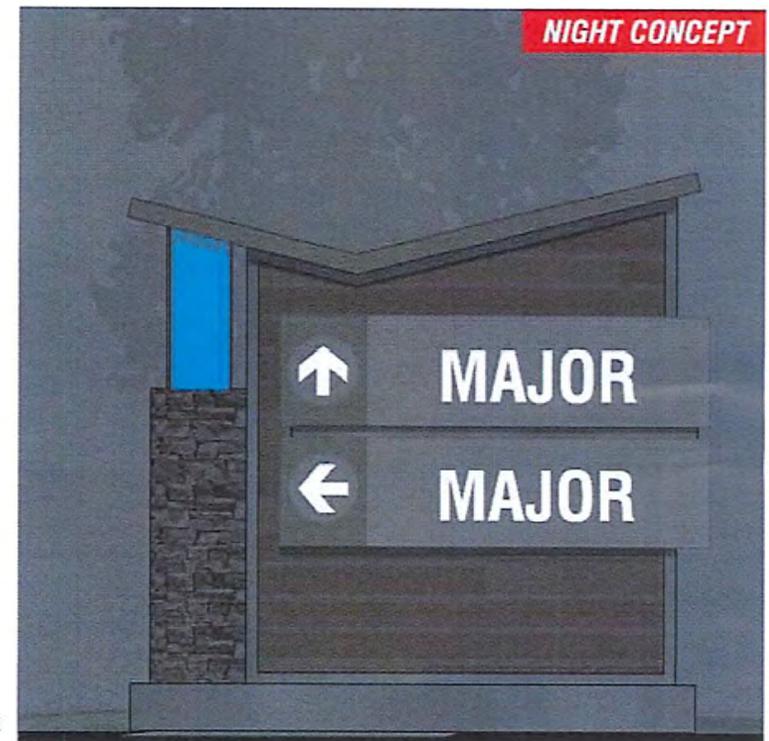
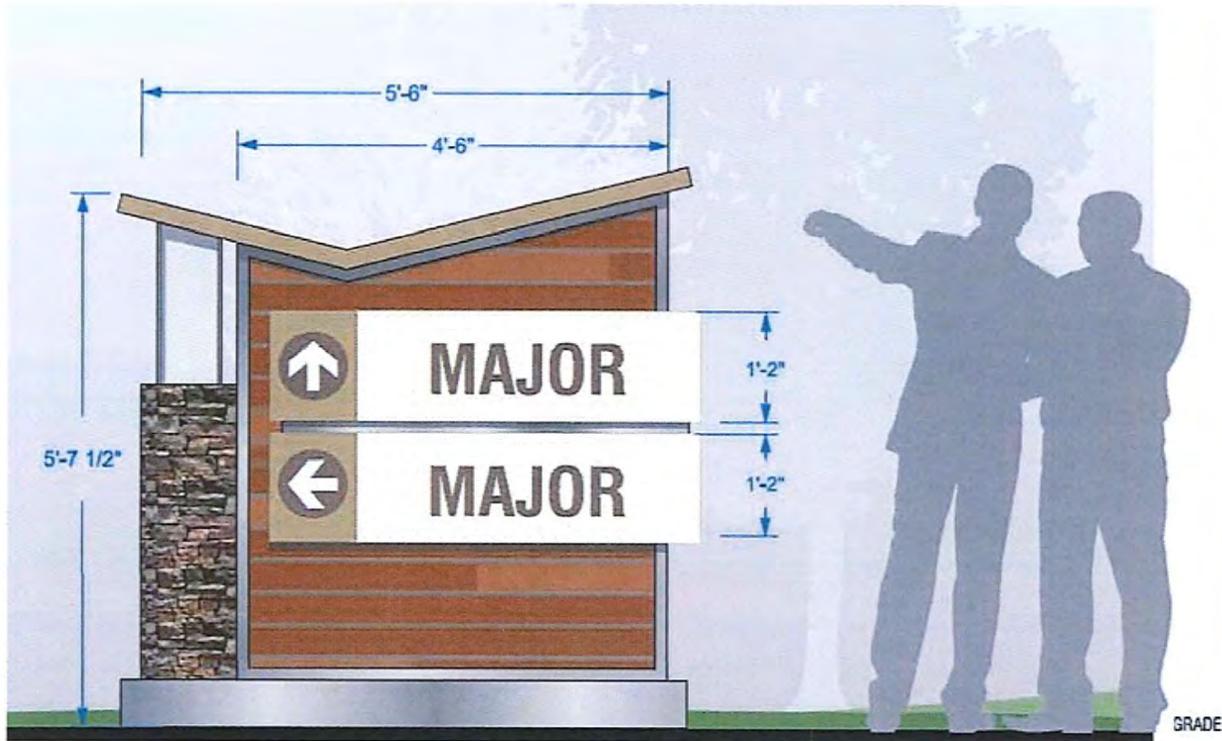


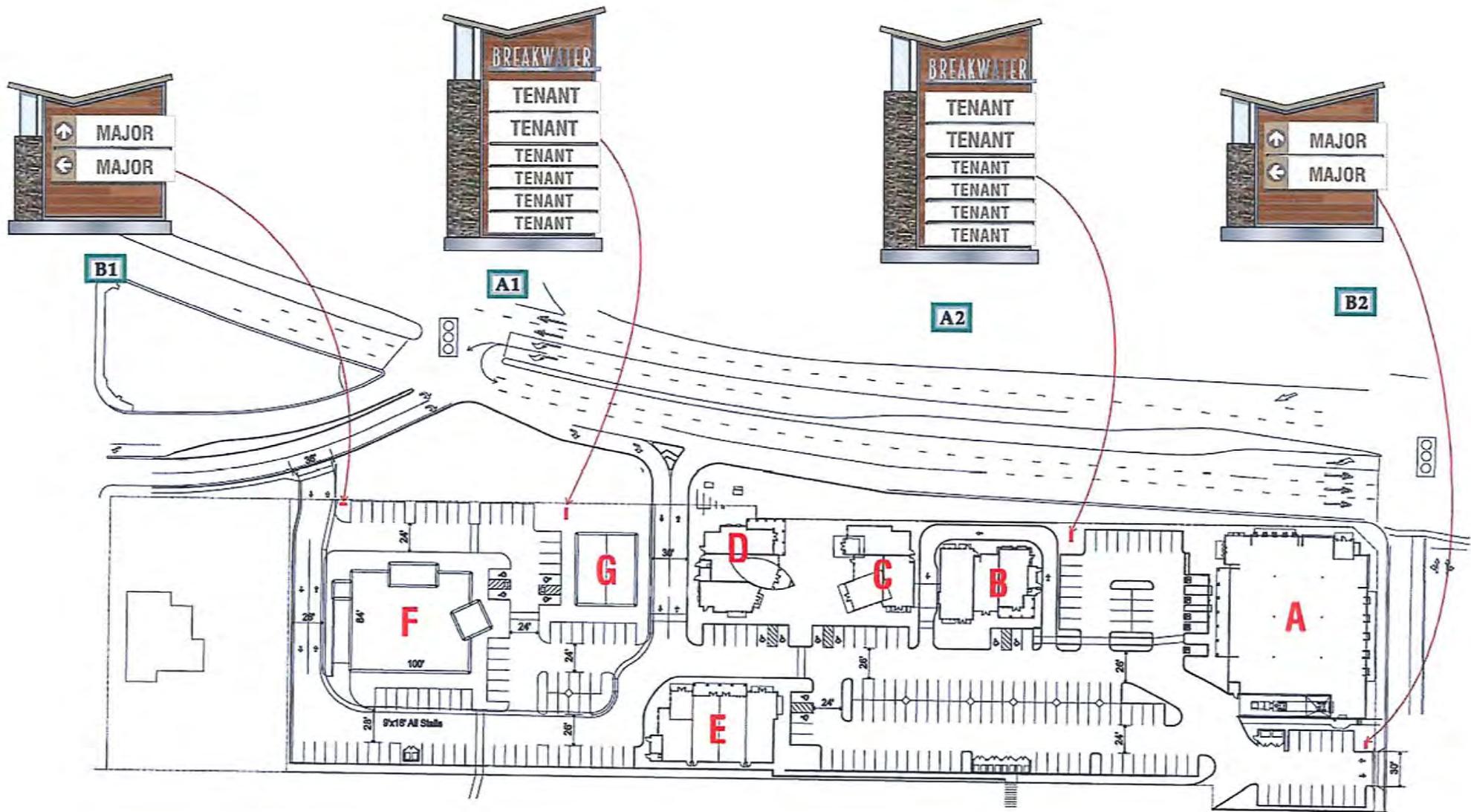
B

GATEWAY SIGN DISPLAY

Attachment 4 - 2014 Sign Plans







Paint Colors



SHERWIN-WILLIAMS
SW 7014
EIDER WHITE



SHERWIN-WILLIAMS
SW 6125
CRAFT PAPER



SHERWIN-WILLIAMS
SW 7062
ROCK BOTTOM



SHERWIN-WILLIAMS
SW 7604
SMOKY BLUE

Storefronts



ALUMINUM
CLEAR
ANODIZED

Wood Stains

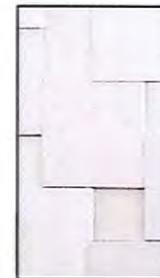


OLYMPIC
CHOCOLAT

Veneers



CORONADO
CARIBBEAN
CORAL STONE



CORONADO
ASTORIA WHITE
LENNOX STONE



CORONADO
VILLA VOLTERA
ITALIAN STONE

Awnings



SUNBRELLA
PEARL BLACK
MARINE GRADE

Siding



GEOLAM
HORIZONTAL
EBONY

Color and Materials

September 2, 2014

8/19/2014 11:03am C:\Users\jerry\Documents\New Development\Permit\04 color and materials.dwg

RESOLUTION NO. 2011-7131

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF IMPERIAL BEACH, CALIFORNIA, APPROVING ADMINISTRATIVE COASTAL PERMIT (ACP 110024), DESIGN REVIEW CASE (DRC 110025), SITE PLAN REVIEW (SPR 110026), TENTATIVE MAP (TM 110027), AND MITIGATED NEGATIVE DECLARATION (EIA 110028 AND SCH #2011111018) TO CONSTRUCT A NEW COMMERCIAL CENTER (BREAKWATER) ON THE SOUTH SIDE OF PALM AVENUE/SR75 BETWEEN 9TH & 7TH STREETS IN THE C-1 (GENERAL COMMERCIAL) ZONE. MF 1062.

DEVELOPER: SUDBERRY-PALM, LLC

WHEREAS, on December 14, 2011, the City Council of the City of Imperial Beach held a duly advertised and noticed public hearing to consider the merits of approving or denying an application for an Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028) for the redevelopment of a new commercial/retail center (Breakwater) that would replace the former 68,585 square foot Miracle Shopping Center located on a 4.75-acre project site on the south side of State Route 75/Palm Avenue between 9th & 7th Streets (APNs 626-250-03-00, 626-250-04-00, 626-250-05-00, and 626-250-06-00). The redevelopment would include the construction of seven new commercial buildings with approximately 46,200 square feet of commercial space composed of a market, retail shops, and a financial institution. The project would include approximately 238 surface parking spaces, on-site improvements, landscaping, signage, lighting, and utilities. In addition to the on-site development with its associated internal roads and utilities, the project would include vacation of portions of Delaware Street and 8th Street traversing the project site, as well as a portion of the alley south of the project site at 9th Street. The project also involves off-site improvements to Palm Avenue between 7th and Delaware Streets immediately south of SR-75, to include a reconfigured design of the Palm Avenue/SR-75 intersection to improve traffic flow, closure of Delaware Street at Palm Avenue, modification of existing traffic signals along SR-75 and Palm Avenue. The property is designated C-1 (General Commercial) on the Zoning Map on a site legally described as follows:

Parcel A: APN 626-250-03

That portion of the northwest quarter of the northwest quarter of section 29, township 18 south, range 2 west, San Bernardino meridian, in the City of Imperial Beach, County of San Diego, State of California, according to official plat thereof lying northerly of the northerly line of south Coronado Manor, according to Map thereof No. 2450 filed in the Office of the County Recorder of San Diego County January 20, 1948 and lying west of the center line of Delaware Street, formerly 13th Street as shown on Map of R. Merideath Jones addition to south San Diego being Map No. 1145, filed in the Office of the County Recorder of San Diego County July 29, 1908.

Excepting that portion thereof which lies westerly of the location and northerly prolongation of the center line of the alley in Block 3 of said Merideath Jones addition, as shown on said Map No. 1145.

Said land is also shown as a portion of Block 3 of Map No. 1145, filed in the Office of the County Recorder of San Diego County, July 29, 1908 and vacated March 22, 1923, by decree in Superior Court Action No. 28686.

Parcel B: APNs 626-250-04 thru 06

That portion of the northwest quarter of the northwest quarter of section 29, Township 18 south, range 2 west, San Bernardino meridian, in the City of Imperial Beach, County of San Diego, State of California, according to United States Government survey approved February 25, 1870, lying northerly of the northerly line of South Coronado Manor as shown on Map thereof No. 2450, filed in the Office of the County Recorder of San Diego County, January 20, 1948, and lying east of the center line of Delaware Street, formerly 13th Street, and west of the center line of 8th Street, formerly 12th Street, and that portion lying west of the west line of 9th Street formerly 11th Street and east of the east line of 8th Street, formerly 12th Street, as said streets are shown on Map of R. Merideath Jones' addition to South San Diego, being Map No. 1145, filed in the Office of the County Recorder of San Diego County, July 29, 1908.

Excepting therefrom that portion lying within the north 50.00 feet of the east 550.50 feet of said northwest quarter of the northwest quarter of section 29 as described in the deed to the State of California, recorded August 24, 1943 in Book 1526, Page 405 of Official Records.

Also excepting therefrom that portion described in deed to the State of California recorded June 20, 1965 as file No. 79513 in Book 5685, Page 513 of Official records, as follows:

That portion of the northwest quarter of the northwest quarter of section 29, township 18 south, range 2 west, San Bernardino base and meridian, in the City of Imperial Beach, County of San Diego, State of California, according to United States Government survey approved February 25, 1870, described as follows: Beginning at the intersection of the west line of 9th Street (shown as 11th Street on Map 1145 of R. Merideath Jones' addition to South San Diego) with the southerly line of the north 50.00 feet of said northwest quarter of the northwest quarter of said section 29; thence along said southerly line westerly 20.00 feet; thence in a straight line southeasterly to a point on the said westerly line southerly 20.00 feet from said point beginning; thence northerly 20.00 feet to the point of beginning.

Said land is also shown as Lots 1 to 10 inclusive and 31 to 39 inclusive and a portion of lot 40 in Block 2, Lots 2 to 10 inclusive and Lots 31 to 39 inclusive and a portion of Lots 1 and 40, in Block 1 of Merideath Jones' addition to South San Diego, being Map No. 1145, filed in the office of the County Recorder of San Diego County, July 29, 1908 and vacated March 22, 1923 by decree in Superior Court Action 38686.

WHEREAS, on November 17, 2011, the Design Review Board recommended approval of the project with the following recommendations: 1st recommendation: Soften the elevation of the northeast corner of Building A, address public space usage and architectural statement and examine pedestrian usage. 2nd recommendation: Pedestrian walkway between Buildings D & G should be moved south so that the pathway is continuous. 3rd recommendation: Look at alternative pedestrian entry way at southeast entrance, formerly alley access. 4th recommendation: Soften west vehicular entrance; and

WHEREAS, the project design of a new commercial center that would provide seven commercial buildings that would locate on the south side of State Route 75/Palm Avenue between 9th & 7th Streets (APNs 626-250-03-00, 626-250-04-00, 626-250-05-00, and 626-250-06-00), in the C-1 (General Commercial) Zone, is compatible with other developments in the

vicinity, would be consistent with Policy D-8 (Project Design) of the Design Element of the General Plan; and,

WHEREAS, the City Council finds that the project is in substantial compliance with Policy L-4d of the Land Use Element of the General Plan, which promotes a mix of pedestrian and automobile oriented uses for Highway 75 Commercial Areas (C-1); and,

WHEREAS, this project complies with the requirements of the California Environmental Quality (CEQA) as a Mitigated Negative Declaration (MND) had been prepared for this project and was routed for public review from November 7, 2011 to December 7, 2011 and submitted to the State Clearinghouse (SCH #2011111018) for agency review; and

WHEREAS, a Mitigation Monitoring and Reporting Program has been prepared that establishes mitigation measures that will avoid or reduce all potentially significant environmental impacts identified in the MND to below a level of significance; and

WHEREAS, the City Council further offers the following findings in support of its decision to conditionally approve the project:

ADMINISTRATIVE COASTAL PERMIT:

- 1. The proposed development conforms to the certified local coastal plan including coastal land use policies.**

The General Plan/Local Coastal Plan designates the site as General Commercial (C-1 Zone), providing for the development of businesses to meet the local demand for commercial goods and services. The project site is located in a non-appealable coastal zone and complies with the land use designation of the General Plan/Local Coastal Plan.

- 2. The proposed development meets the minimum criteria set forth in the City of Imperial Beach Zoning Ordinance, the City's Minimum Landscape Planting and Irrigation Standards, and the City's Design Guidelines, as applicable.**

The project complies with the setback requirements, landscaping requirements and building height limitation specified in the Zoning Ordinance.

- 3. This project complies with the California Environmental Quality Act.**

This project complies with the requirements of the California Environmental Quality (CEQA) as a Mitigated Negative Declaration (MND) had been prepared for this project and was routed for public review from November 7, 2011 to December 7, 2011 and submitted to the State Clearinghouse (SCH #2011111018) for agency review. The City conducted an Environmental Initial Study (IS) that determined the proposed project could have a potentially significant environmental effect in the following areas: Aesthetics, Biological Resources, Cultural Resources, Geology and Soils, Hazards and Hazardous Materials, Hydrology /Water Quality, Noise, and Transportation/Traffic. The MND identifies mitigation measures that will avoid or reduce all potentially significant environmental effects to below a level of significance.

- 4. Public Notice requirements, pursuant to Zoning Ordinance Section 19.87.100, of the Coastal Development Project have been satisfied.**

The project description and the date of the City Council public hearing were sent to property owners within 300 feet and occupants within 100 feet of the subject site on

November 30, 2011, and a public hearing notice was published in the San Diego union Tribune newspaper on November 30, 2011.

DESIGN REVIEW/SITE PLAN REVIEW:

5. **The proposed use does not have any detrimental effect upon the general health, safety and convenience of persons residing or working in the neighborhood, or is not detrimental or injurious to the value of the property and improvements in the neighborhood.**

The project will provide a commercial center that would provide services to residents of the City of Imperial Beach. The project would not have a detrimental effect on the general health, welfare, safety and convenience of persons residing or working in the neighborhood because it is consistent with the development standards and zoning designation of the site. The development would not be injurious to the value of the property and improvements in the neighborhood because the commercial center represents an improvement of the existing conditions of the property. This could improve property values and stimulate growth in the area.

6. **The proposed use does not adversely affect the General Plan or the Local Coastal Plan.**

The General Plan/Local Coastal Plan designates the site as General Commercial (C-1 Zone), providing for the development of businesses to meet the local demand for commercial goods and services. The project site is located in a non-appealable coastal zone and complies with the land use designation of the General Plan/Local Coastal Plan.

7. **The proposed use is compatible with other existing and proposed uses in the neighborhood.**

The site is bordered by commercially zoned properties to the north, east, and west. Residences are located to the south and the commercial center would provide commercial goods and services for the community.

8. **The location, site layout and design of the proposed use orients the proposed structures to streets, driveways, sunlight, wind and other adjacent structures and uses in a harmonious manner.**

The proposed buildings would locate in a manner that would provide harmonious access throughout the site and be compatible with the surrounding structures and uses in the neighborhood.

9. **The combination and relationship of one proposed use to another on the site is properly integrated.**

The commercial center proposes commercial uses that are properly integrated. The design style and the choice of building materials properly integrate the buildings.

10. **Access to and parking for the proposed use does not create any undue traffic problem.**

Vehicular ingress and egress for the project site will occur via Palm Avenue, highway 75, 9th Street, and the alley. The ingress and egress for the project should not create undue traffic problems.

11. **All other applicable provisions of the Zoning Code are complied with.**

The project is consistent with the General Plan and the Zoning development standards for the site.

- 12. Any other considerations as the Community Development Department deem necessary to preserve the health, safety and convenience of the City in general.**

Standard and applicable conditions of approval have been included with the Resolution to further ensure that the health, safety, welfare, and convenience of the City in general is preserved.

- 13. Public Notice requirements, pursuant to Zoning Ordinance Section 19.87.100, have been satisfied.**

The project description and the date of the City Council public hearing were sent to property owners within 300 feet and occupants within 100 feet of the subject site on November 30, 2011, and a public hearing notice was published in the San Diego union Tribune newspaper on November 30, 2011.

ADMINISTRATIVE SIGN PERMIT:

- 14. The proposed project is consistent with the Design Element of the General Plan.**

The Developer proposes a Comprehensive Signage Plan for the commercial center with design and construction requirements that are consistent with the Design Element and General Plan.

- 15. The proposed project is consistent with Chapter 19.52 of the City of Imperial Beach Municipal Code, entitled "Signs".**

The purpose of the sign regulations is to encourage the effective use of signs as a means of communication in the City and to maintain and enhance the aesthetic environment and the City's ability to attract sources of economic development and growth. The project will receive a 25 percent increase in the maximum total sign area because the project provides a Comprehensive Signage Plan. Any signs would comply with the Comprehensive Sign Plan and would be consistent with Chapter 19.52 of the City of Imperial Beach Municipal Code.

- 16. The proposed project will not have a detrimental effect upon the general health, welfare, safety or convenience of persons residing or working in the neighborhood, and will not be detrimental or injurious to the value of property and improvements in the neighborhood.**

The proposed signs will not adversely impact adjacent businesses because the signage does not exceed the allowance limits provided by Chapter 19.52 of the City of Imperial Beach Municipal Code.

TENTATIVE MAP FINDINGS:

- 17. The proposed tentative tract map is consistent with the General Plan/Local Coastal Plan.**

The proposed subdivision for six lots is consistent with the General Plan and zoning designation (C-1 General Commercial Zone) because the new lots will meet or exceed the minimum lot size and frontage requirements.

- 18. The design or improvement of the proposed subdivision is consistent with the General Plan/Local Coastal Plan.**

The General Plan/Local Coastal Plan designates the site as General Commercial (C-1 Zone), providing for the development of businesses to meet the local demand for commercial goods and services. The project site is located in a non-appealable coastal zone and complies with the land use designation of the General Plan/Local Coastal Plan.

19. The site is physically suitable for the type of development.

Grading and drainage for the development of the commercial center has been reviewed so that no adverse impacts would occur.

20. The design of the subdivision will not cause substantial environmental damage or substantial and avoidable injury to fish or wildlife, or their habitat.

The project involves new construction in a developed urban area that will not affect fish or wildlife habitat.

21. The design of the subdivision will not cause serious public health problems.

The development will be served by municipal water and sewer service and would not result in public health problems.

22. The design of the subdivision will not conflict with any easement of record.

A design of the subdivision will not conflict with any easement of record.

23. All requirements of the California Environmental Quality Act (CEQA) have been fulfilled.

This project complies with the requirements of the California Environmental Quality (CEQA) as a Mitigated Negative Declaration (MND) had been prepared for this project and was routed for public review from November 7, 2011 to December 7, 2011 and submitted to the State Clearinghouse (SCH #2011111018) for agency review. The City conducted an Environmental Initial Study (IS) that determined the proposed project could have a potentially significant environmental effect in the following areas: Aesthetics, Biological Resources, Cultural Resources, Geology and Soils, Hazards and Hazardous Materials, Hydrology /Water Quality, Noise, and Transportation/Traffic. The MND identifies mitigation measures that will avoid or reduce all potentially significant environmental effects to below a level of significance.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Imperial Beach that the above-listed findings and recitals are true and correct and are incorporated by reference; and

BE IT FURTHER RESOLVED by the City Council of the City of Imperial Beach that Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028 and SCH #2011111018) and Mitigation Monitoring and Reporting Program for the construction of a new commercial center that would provide seven commercial buildings that would locate on the south side of State Route 75/Palm Avenue between 9th & 7th Streets (APNs 626-250-03-00, 626-250-04-00, 626-250-05-00, and 626-250-06-00) in the C-1 (General Commercial) Zone to construct a new commercial center that would provide seven commercial buildings that would locate on the south side of State Route 75/Palm Avenue between 9th & 7th Streets (APNs 626-250-03-00, 626-250-04-00, 626-250-05-00, and 626-250-06-00) in the C-1 (General Commercial) Zone, are hereby approved subject to the following:

CONDITIONS OF APPROVAL:**A. PLANNING:**

1. **Approval** of the Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), Tentative Map (TM 110027), and Mitigated Negative Declaration (EIA 110028) is valid for three years from the date of final action by the City Council **to expire December 14, 2014**, unless vested with substantial construction pursuant to an approved building permit. Approvals of the Administrative Coastal Permit (ACP 110024), Design Review Case (DRC 110025), Site Plan Review (SPR 110026), and Mitigated Negative Declaration (EIA 110028) shall run coterminous with the tentative map.
2. The site shall be developed in substantial compliance with the plans dated December 5, 2011, on file at the Community Development Department, or as otherwise amended and approved, and the conditions contained herein.
3. The design of the corner element of the building at the southwest corner of 9th Street and Palm Avenue shall be modified to provide a more distinctive and attractive focal point of the project. Such design modification may include vertical and horizontal articulation, the use of glass or other materials, a varied façade and/or roofline including the use of a cupola or tower element, a curved façade or a public entrance.
4. All building façades either fronting or facing a public street shall be designed to be pedestrian-friendly and shall incorporate clear glass, awnings, display cases, and other architectural treatment that provides visual interest. Blank walls shall be avoided and, where necessary, shall be enhanced with architectural detail and/or landscaping to either screen or add visual interest to the façade.
5. The appearance of any blank walls shall be avoided on all building façades and, where present, shall be enhanced with architectural details and/or landscaping to either screen or add visual interest to such façades.
6. The Developer shall pursue the design and installation of a median refuge in the western crosswalk within Palm Avenue/State Route 75 at 9th Street.
7. The pedestrian crosswalk at the main vehicular access to and from Palm Avenue/State Route 75 shall be relocated to the south to provide a straight and continuous path of travel along the northern frontage of the subject property and proposed buildings. Additional traffic calming measures such as stop signs, other signage and road markers shall also be incorporated to provide for the safety of pedestrians utilizing the crosswalk.
8. The Developer shall consider the addition of sidewalks adjacent to the vehicular drive aisle from Palm Avenue/State Route 75 which would connect the sidewalk along the northern property line to the proposed crosswalk internal to the site.
9. The Developer shall consider and/or incorporate a pedestrian access/sidewalk to allow for safe pedestrian access at the southeast corner of the site.
10. Prior to issuance of any building permits for the project, the Developer shall prepare a landscape/streetscape plan for the proposed stormwater demonstration area located within the City's public right-of-way for submittal to and approval by the City.

11. Prior to issuance of any building permits for the project, the Developer shall prepare a streetscape/landscape plan for the alley along the southern portion of the site for submittal to and approval by the City. This plan shall include proposed hardscape, landscaping, and street lighting and shall be designed to be suitable for both pedestrian and vehicular use.
12. All drainage shall be in compliance with approved Drainage Report and Water Quality Technical Reports dated December 5, 2011, on file at the Community Development Department, or as otherwise amended and approved.
13. Separate design and zoning review by the Community Development Department for all signage will be necessary. Signage shall be in substantial compliance with the Comprehensive Signage Plan dated December 5, 2011, subject to the following design and construction requirements:
 - No exposed conduits or raceways will be allowed.
 - Signs shall be made of durable rust-inhibiting materials that are appropriate and complimentary to the building.
 - Joining of materials shall be finished in a way as to be unnoticeable.
 - Finished surfaces of metal shall be free from oil canning and warping. All sign finishes shall be free from dust, orange peel, drips, and runs and shall have a uniform surface conforming to the highest standards of the industry.
 - Manufacturer's labels shall not be visible from the street from normal viewing angles.
 - Exposed raceways are not permitted unless they are incorporated into the overall sign design.
 - Exposed junction boxes, lamps, tubing or neon crossovers of any type are not permitted.

Because the tenants for the commercial center are unknown, sign criteria will be provided to encourage creativity and individuality of each tenant while maintaining compatibility with the center. The following types of signage would be allowed:

- Acrylic face channel letters.
- Through face and halo channel letters.
- Reverse pan channel letters.
- Open pan channel letters (in an artistic letter style/font).
- Push thru letters and logos in aluminum cabinets.
- Flat cut out dimensional shapes and accents.
- Exposed skeleton neon accents.
- Logo modules to be clouded.

Elevations facing residential areas shall not be illuminated, and will only allow the following sign construction:

- Acrylic face channel letters.
- Reverse pan channel letters.
- Push thru letters and logos in aluminum cabinets.
- Flat cut out dimensional letter shapes and accents.

In addition to signs prohibited in Municipal Code Section 19.52, the following signs would be prohibited:

- Signs constituting a traffic hazard.
 - Signs in proximity to utility lines.
 - Signs painted directly on a building surface.
 - Wall signs projecting above the top of a parapet, the roof line at the wall, or roof line.
 - Flashing, moving, or audible signs.
 - Vehicle signs.
 - Light bulb strings.
 - Banners, pennants, and balloons for advertising purposes.
 - Billboard signs.
 - Permanent "sale" signs.
 - Standard plex-faced cabinet signs.
14. All owners, future owners, and/or tenants shall sign a Reciprocal Easement Agreement that will provide for access through and among all subdivided lots and shall provide for pedestrian and vehicular access to all common and parking areas. This legal agreement shall be recorded with the County Recorder and is subject to prior approval by the City.
15. The Developer shall defend, indemnify, and hold harmless the City, its agents, officers, and employees from any and all claims, actions, proceedings, damages, judgments, or costs, including attorney's fees and costs incurred by the City and any claim for private attorney general fees and costs claimed by or awarded to any party, against the City or its agents, officers, or employees, relating to the approval of the MF 1062 9th & Palm project including, but not limited to, any action to attack, set aside, void, challenge, or annul the development approvals (including the Administrative Coastal Permit, Design Review Case, Site Plan Review, and Tentative Map) and/or certification of the Final Mitigated Negative Declaration (SCH #2011111018) and any related environmental document or decision (collectively "Development Approvals"). The City will promptly notify Developer of any claim, action or proceeding concerning the Development Approvals. The City may elect to conduct its own defense, participate in its own defense, or obtain independent legal counsel in defense of any claim related to this indemnification. In the event of such election, Developer shall pay all of the costs related thereto, including without limitation reasonable attorney's fees and costs incurred by the City. In the event of a disagreement between the City and Developer regarding litigation issues, the City shall have the authority to control the litigation and make litigation-related decisions, including, but not limited to, settlement or other disposition of the matter. However, the Developer shall not be required to pay or perform a settlement unless such settlement is approved by Developer.
16. The City of Imperial Beach currently holds title to the project site, and it seeks to quiet title in its favor for the east one-half of 8th Street, the west one-half of 9th Street, and an easement appurtenant to the 8th Street property along the northeast 40 feet portion of the property against Davies Motors, Inc. in San Diego Superior Court Case No. 37-2011-00079079-CU-OR-SC. No final map may be recorded until such quiet title action is complete.
17. Developer shall provide an updated Title Report dated within 60 days of the Final Map submittal.

18. Prior to issuance of the first building permit for any development on the site, the Developer shall obtain approval from the City and record the Reciprocal Easement Agreement required under condition number 14 above that allows for shared vehicular and pedestrian access of the entire site.
 19. All building permits required for the project shall be obtained from the Imperial Beach Building Department.
 20. Per Section 19.48.150 of the IBMC, parking areas that abut properties zoned for residential uses shall be separated from such property by a solid fence, wall or building six feet in height; provided, that in the required front yard the fence or wall shall not exceed four feet in height.
 21. All landscaping and bio-filtration areas shall be maintained by the Developer, including the landscaped areas on City-owned property north of the project site and south of Palm Avenue/Highway. All landscaped areas shall be maintained in a healthy condition, free from weeds, trash, and debris.
 22. Drought tolerant landscaping shall be required and shall be subject to staff approval.
 23. A permanent irrigation system shall be installed to serve all landscaped areas.
 24. The courtyard area extending into the public right-of-way between buildings C and D shall be maintained by the Developer.
 25. Public improvements required for the project and/or adjacent to the entire project site shall be designed and installed by the Developer. All improvements proposed within the public right-of-way including paving, street trees, irrigation shall be subject to submittal of public improvement drawings and a Temporary Encroachment Permit (TEP) application to the City of Imperial Beach for review and approval by the Community Development Department and Public Works Department.
 26. An encroachment permit from the City and/or CALTRANS shall be required for all improvements within Caltrans right-of-way.
 27. The Developer or Developer's representative shall read, understand, and accept the conditions listed herein and shall, within 30 days, return a signed statement to the Community Development Department accepting said conditions.
 28. The Developer shall pay off any deficits in his project account (110024) prior to building permit issuance and prior to final inspection.
- B. BUILDING:**
29. A registered design professional licensed in the State of California will be required at time of construction drawings, to prepare plans for proposed improvements per the Business and Professions Code.
 30. BUILDING CODES – Improvements (new framing, electrical, mechanical, plumbing) Obtain all required building permits (Building, Plumbing, Mechanical, Grading etc.) for this project. All construction and design shall comply with the most recent adopted City and State Building Codes. January 1, 2011 the new California Building Code became effective, along with City amendments:
 - a. 2010 California Building Code
 - b. 2010 California Green Building Code
 - c. 2010 California Electrical Code
 - d. 2010 California Mechanical Code

- e. 2010 California Plumbing Code
- f. California Energy Code
- g. 2010 California Fire Code

31. No Building Structure, plumbing, drainage system, building sewer, or electric service shall be located in any lot other than the lot which is the site of the building, structure or premises served by such facilities. No piping, fixtures or equipment shall be so located as to interfere with the normal safe use thereof or with normal operation. Show all utility and service runs serving buildings or facilities. If alterations are to be made, separate permits and plans will be required to re-locate any utility services. Show all existing and new easements if required to comply with State Building Codes.

- **Cross easement agreement will be needed for drainage, sewer, water, parking etc.**

DURING DESIGN (at construction plan submittal):

32. The Title Sheet of the plans shall include:
- a. Occupancy group
 - b. Occupant Load
 - c. Description of use
 - d. Type of construction
 - e. Height of the building
 - f. Floor area of building(s)
33. SOILS REPORT- shall be submitted at time of Grading permit and/or Building permit application for building plan submittal.
34. FOUNDATIONS - Structural foundation design plans shall be reviewed by the soils Engineer of record for conformance to recommendations contained within the project soils report approved by the Building Official. A written statement approving the design will be provided to the Building Official at the time of construction permit application.
35. SPECIAL INSPECTIONS – As indicated by CBC Sec. 1704, the owner or the engineer or architect of record acting as the owner's agent shall employ one or more special inspectors who shall provide special inspections as required by CBC section 1704. Please contact Building Division at time of plan submittal to obtain application for special inspections.
36. Title 24 ACCESSIBILITY/COMMERCIAL - Comply with all applicable provisions of the California State Building Code (Title 24) Part 2 Chapter 11B Division for Disabled Access requirements. Plans shall show compliance in sufficient information and detail to determine compliance.
37. FOOD SERVICE FACILITIES AND PUBLIC POOLS AND SPAS - All projects including food service or public pools or spas shall be checked and approved by the County Health Department prior to submittal for a building permit.
38. PRE-CONSTRUCTION CONFERENCE – prior to issuance of a building permit or commencement of any site work a pre-construction conference shall be held at a time and location agreed upon by the City and Developer for the purpose of review and approval of construction procedures. The City will be represented by departments having conditions of approval on the project. Attendance shall be mandatory for the building owner and/or developer, designer of record and construction contractors:

39. **PRE-CONSTRUCTION CONFERENCE** – prior to issuance of a building permit or commencement of any site work a pre-construction conference shall be held at a time and location agreed upon by the City and Developer for the purpose of review and approval of construction procedures. The City will be represented by departments having conditions of approval on the project. Attendance shall be mandatory for the building owner and/or developer, designer of record and construction contractors:

Please contact the Building Official at (619) 628-1357 to schedule an appointment minimum two weeks ahead of proposed meeting date. Pre-construction meeting is required a maximum of two weeks after permit issuance or two weeks before issuance.

DURING CONSTRUCTION:

40. A licensed surveyor/engineer shall verify pad elevations and building corners and setbacks.
41. **HOURS OF CONSTRUCTION** - No work for which a building permit is required shall be performed within the hours of 7:00 P.M. - 7:00 A.M. Monday - Friday, nor prior to 8:00 A.M. or after 5:00 P.M. on Saturday. Construction work on Sundays shall be approved by the City.

Pre-Final Inspection

42. Call for a pre-final inspection seven working days prior to final inspection so project can be routed to all City Departments for review and approval.
43. Prior to issuance of the Certificate of Occupancy, the project designers including civil, structural, and geotechnical engineers shall provide statements of compliance attesting that they have reviewed the completed project and that it was constructed in conformance with their recommendations and plans.
44. Prior to final inspection of the project by the City, the Developer shall provide the City with a copy of the marked plans for review. After the City review has approved the marked plans, the Developer shall submit a final "wet stamp" copy to the City. The plans shall be identified by an "as built" or "record drawing" stamp.
45. Provide all Special Inspection and testing reports prior to final inspection.
46. Provide Title 24 Certificate of Acceptance Forms and Worksheets prior to final inspection.
47. Energy Compliance Forms shall be provided at site prior to final inspection.
48. Provide any required manufacture warranty and operation manuals on site prior to final inspection.
49. Provide a complete copy of the City inspection record log prior to final inspection.

POST CONSTRUCTION:

50. Building structures and facilities serving the project/buildings (this will include parking areas, drainage BMPs and landscaping) shall be maintained as originally designed and approved by the City. Alterations to structures and facilities serving this project shall be submitted for approval by the City prior making alterations. A Note indicating this requirement shall be included on design drawings.

C. PUBLIC WORKS:

51. Ensure that the hot water tank P.T. discharge pipe is piped to discharge to the sanitary sewer system or the landscape area. A design that has the water discharge directly into the storm drain conveyance system (onto an impervious surface that flows to the street) is in violation of the Municipal Storm Water Permit - Order R9-2007-0001.
52. The project shall comply with the approved Drainage Report and Water Quality Technical Report dated December 5, 2011, on file at the Community Development Department, or as otherwise amended and approved. The project may not violate Municipal Storm Water Permit - Order R9-2007-0001.
53. Landscape irrigation system shall be provided to tree planters and all landscaping in the public right-of-way per I.B.M.C. 19.50.040.F.
54. Require the building foundation elevation be at least 1 foot above gutter line to minimize flooding during storm conditions. An alternate methods and design request may be applied for demonstrating adequate drainage flow away from the building and adequately conveying water from the site.
55. 9th Street between Palm Avenue and Donax Avenue shall be resurfaced per the plans provided by the City Engineer/City of Imperial Beach. City will contribute to the resurfacing costs. Infrastructure improvements (sewer / storm drain) required as part of the development on 9th Street will be borne by the Developer.
56. Street lights shown to be removed from S.R. 75 and Palm Avenue shall be considered for relocation and reuse. If these are not to be reused, they shall be delivered to the City for reuse.
57. Alleyway shall be designed and reconstructed to the standards of the City Engineer and subject to approval by the City.
58. The pedestrian access ramps located along Palm Ave shall be constructed in compliance with San Diego Regional Standard Drawing G-27A. All driveway approaches shall be constructed per San Diego Regional Standard Drawing G-14A.
59. For alley, sidewalk or curb & gutter replacement ensure compliance with San Diego Regional Standard Drawing G-11 in that, the "Area to be removed [shall be] 5' or from joint to joint in panel, whichever is less." The distance between joints or score marks shall be a minimum of 5-feet. Where the distance from "Area to be removed", to existing joint, edge or score mark is less than the minimum shown, "Area to be removed" shall be extended to that joint, edge or score mark.
60. If it is necessary to cut into the alley pavement as part of this project, all concrete cuts in the alley shall be replaced with #4 rebar dowels positioned every 1 foot on center. Concrete specification shall be 560-C-3250. Concrete cuts shall also comply with item 9 above and cuts parallel to the alley drainage shall be at least 1-foot from the alley drain line.
61. For any work to be performed in the street or alley, a traffic control plan shall be submitted to the City for approval by Public Works Director a minimum of 5 working days in advance of street work. Traffic control plan is to be per Regional Standard Drawings or CALTRANS Traffic Control Manual.

62. For any project that proposes work within the public right-of-way (i.e., driveway removal/construction, sidewalk removal/construction, street or alley demolition/reconstruction, landscaping and irrigation, fences, walls within the public right-of-way, etc.), a Temporary Encroachment Permit (TEP) shall be applied for and approved either prior to or concurrent with issuance of the building permit required for the project. Application for a Temporary Encroachment Permit shall be made on forms available at the Community Development Department Counter.
63. All street work construction requires a Class A contractor to perform the work. All pavement transitions shall be free of tripping hazards.
64. Construct trash or refuse enclosures and recycling enclosures to comply with IBMC 19.74.090. Trash and recycling enclosures are to be enclosed by a six-foot high masonry wall and locking gate. The minimum size refuse enclosure shall be 6'-0" x 9'-0" and the minimum recycling enclosure shall be 4'-0" x 8'-0". **The trash bins shall be accessible to the refuse hauler without restriction.**
65. Any disposal/transportation of solid waste / construction waste in roll off containers shall be contracted through the City's waste management provider unless the hauling capability exists integral to the prime contractor performing the work.
66. The existing parcel impervious surfaces shall not increase beyond the current impervious services as a post-conversion condition in order to maximize the water runoff infiltration area on the parcel in compliance with Municipal Storm Water Permit – Order 2001-01.
67. All landscape areas, including grass and mulch areas, shall be improved to consist of at least 12-inches of loamy soil in order to maximize the water absorption during wet weather conditions and minimize irrigation runoff.
68. Survey monuments shall be installed on the southeast, southwest and northeast property lines in or adjacent to the sidewalk. Record same with County Office of Records.
69. In accordance with I.B.M.C. 12.32.120, Developer shall place and maintain warning lights and barriers at each end of the work, and at no more than 50 feet apart along the side thereof from sunset of each day until sunrise of the following day, until the work is entirely completed. Barriers shall be placed and maintained not less than three feet high.
70. All BMP's planned for the project shall comply with the approved Drainage Report and Water Quality Technical Report dated December 5, 2011, on file at the Community Development Department, or as otherwise amended and approved. Maximum use of Low Impact Development (LID) elements and filter inserts are necessary to reduce pollutant discharge to the maximum extent practicable.
71. Require Developer to provide verification of post construction Best Management Practice (BMP) maintenance provisions through a legal agreement, covenant, CEQA mitigation requirement, and / or Coastal Development Permit. Agreement is provided through the Community Development Department.

72. Developer or property owner shall institute "Best Management Practices" to prevent contamination of storm drains, ground water and receiving waters during both construction and post construction. The BMP practices shall include but are not limited to:
- Contain all construction water used in conjunction with the construction. Contained construction water is to be properly disposed in accordance with Federal, State, and City statutes, regulations and ordinances.
 - All recyclable construction waste shall be properly recycled and not disposed in the landfill.
 - Water used on site shall be prevented from entering the storm drain conveyance system (i.e. streets, gutters, alley, storm drain ditches, storm drain pipes).
 - All wastewater resulting from cleaning construction tools and equipment shall be contained on site and properly disposed in accordance with Federal, State, and City statutes, regulations, and ordinances.
 - Erosion control - All sediment on the construction site shall be contained on the construction site and not permitted to enter the storm drain conveyance system. Developer is to cover disturbed and exposed soil areas of the project with plastic-like material (or equivalent product) to prevent sediment removal into the storm drain system.
73. Developer shall agree to underground all utilities in accordance with I.B.M.C. 13.08.060.
74. Developer shall underground the overhead utilities in the adjacent south alley and install applicable alley lighting as approved by the City.

D. PUBLIC SAFETY

75. Submit applicable sheet(s) or provide in a separate submittal(s): Complete construction documents for each structure on the proposed site including but not limited to the following information:
- Construction type.
 - Hour ratings.
 - Occupancy classification/ Building use(s).
 - Building height and area.
76. Separate fire and life safety submittals shall include:
- Fire sprinkler system drawings and hydraulic calculations.
 - Fire alarm system drawings and load calculations.
77. All submitted construction documents shall be designed in compliance with currently adopted codes and approved standards including but not limited to:
- 2010 California Fire Code
 - 2010 California Building Code
 - 2010 California Mechanical Code
 - NFPA 13 2010 Edition
 - NFPA 72 2010 Edition
78. The designated fire sprinkler contractor and the builder shall attend a mandatory pre-construction meeting with the Imperial Beach Fire Department, Fire Code Official to establish inspection procedures and timelines.

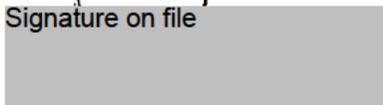
- 79. The designated fire alarm contractor and the builder shall attend a mandatory pre-construction meeting with the Imperial Beach Fire Department, Fire Code Official to establish inspection procedures and timelines.
- 80. It is recommended that the water supply piping serving the project be continued to the western most property line to better serve the fire protection needs of the additional phase.

Appeal Process under the California Code of Civil Procedure (CCP): The time within which judicial review of a City Council decision must be sought is governed by Section 1094.6 of the CCP. A right to appeal a City Council decision is governed by CCP Section 1094.5 and Chapter 1.18 of the Imperial Beach Municipal Code.

PROTEST PROVISION: The 90-day period in which any party may file a protest, pursuant to Government Code Section 66020, of the fees, dedications or exactions imposed on this development project begins on the date of the final decision.

PASSED, APPROVED, AND ADOPTED by the City Council of the City of Imperial Beach at its meeting held on the 14th day of December 2011, by the following vote:

AYES:	COUNCILMEMBERS:	KING, BRAGG, SPRIGGS, BILBRAY, JANNEY
NOES:	COUNCILMEMBERS:	NONE
ABSENT:	COUNCILMEMBERS:	NONE

Signature on file


JAMES C. JANNEY, MAYOR

ATTEST:

Signature on file


JACQUELINE M. HALD, MMC
CITY CLERK

